

DOWNTOWN BISMARCK

SUBAREA PLAN

DECEMBER 2013

**CITY OF BISMARCK
BISMARCK-MANDAN MPO**

ACKNOWLEDGEMENTS

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EXECUTIVE SUMMARY

OVERVIEW

The Bismarck Subarea Plan is the product of a yearlong study to revitalize the Downtown. The Subarea Plan identifies urban design and complete streets frameworks that will guide the development of the public and private realm over the coming years. The plan has an emphasis on implementation and identifies game-changing public projects that will stimulate private investment and fundamentally change the public's perception of downtown.

STUDY PURPOSE

The Bismarck-Mandan Metropolitan Planning Organization (MPO) and the City of Bismarck initiated the Bismarck Subarea Study in an effort to transform the downtown area into a vibrant place where people want to live, work and be entertained. The Study addresses:

- Parking, traffic, quiet rail, transit, walking and biking
- Opportunities for new public gathering spaces, 'greening' downtown streets, and a transit hub
- Opportunities to live downtown
- The integration of transportation, land use and urban design into a coordinated vision
- A realistic implementation strategy that identifies the necessary public investments to inspire investor confidence and stimulate significant private investments within the first five years of Subarea Plan adoption

PUBLIC INVOLVEMENT

The Subarea Plan reflects the community's vision for the revitalization of downtown. The public involvement process included:

Public Outreach

Public outreach consisted of radio and newspaper announcements, interviews with local media, direct email, a Downtown Bismarck Subarea Study website, Facebook page, and Twitter account that provided direct public access to all Study materials and presentations throughout the extent of the Study effort.

Committee, Stakeholder, and Commission Meetings

A Subarea steering committee, technical advisory committee, and key community stakeholders provided input and direction for the development of all plan

concepts and implementing actions. Monthly Study updates with the MPO Policy Board and MPO TAC were supplemented with presentations to the Planning and Zoning Commission, Renaissance Zone Authority, and the Board of City Commissioners.

Public Workshops

A series of three public engagement sessions were facilitated at key project milestones beginning in March of 2013 and concluding in August of 2013. During public workshops, local residents provided comments and preferences for plan concepts and proposals. The process concluded with a Subarea Plan public hearing to the City Commission and presentation for adoption to the MPO Policy Board and Technical Advisory Committee.

STUDY CRITERIA

During the first public engagement session the community identified issues that the Subarea Study needed to address. Those issues are listed below.

Urban Design

- Public Gathering Spaces
- Parking Strategy for Ramps
- Shopping & Entertainment Opportunities
- Housing Opportunities
- Preserve/Enhance Historic Buildings
- Multi-Modal Transit Hub
- Downtown Core, Civic Center, Kirkwood Mall Linkage

Complete Streets

- Pedestrian-friendly, Walkable Environment
- Greener Downtown Streets
- Biking Opportunities
- Traffic Access To & Within Downtown
- Chancellor Square Traffic Circulation
- Public Art
- Trolley

SUBAREA STUDY
BOUNDARY



FUNDAMENTAL CONCEPT

The Downtown Bismarck Subarea Plan's fundamental concept calls for public investment to stimulate substantial private investment. Public investments include improvements to make:

- Main Avenue pedestrian and retail friendly
- Fifth Street a shopping, entertainment, and dining destination
- A seamless pedestrian rail underpass connection between Main Avenue and Fifth Street
- Depot Plaza a public gathering space
- Mehus Commons a focus for cultural activities
- The Civic Center a significant regional and national convention destination

FUNDAMENTAL
CONCEPT



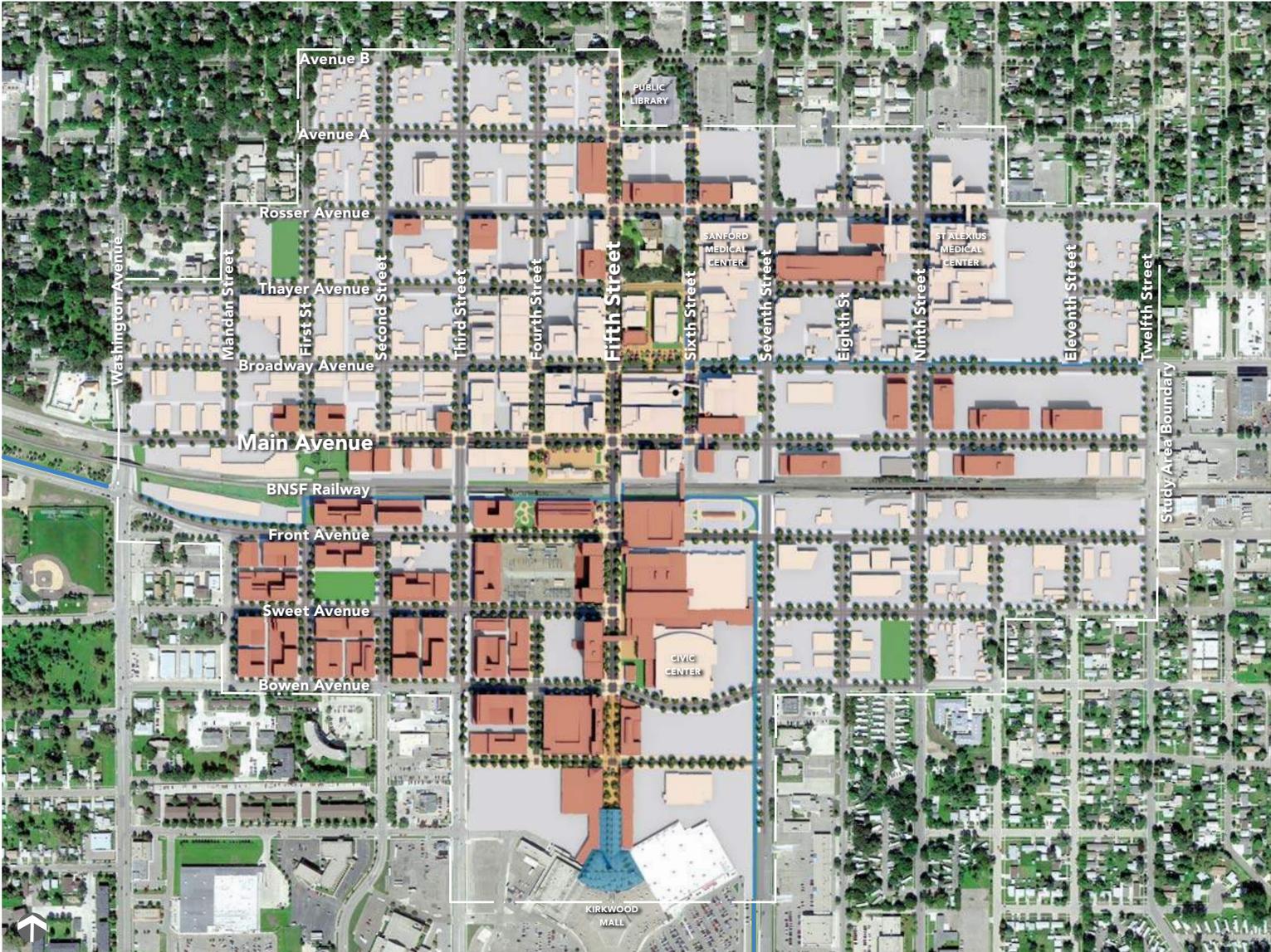
BUILD-OUT

Downtown Bismarck has the capacity to absorb a significant amount of growth within the study area. A conceptual estimate of new investment value is identified. The Build-Out diagram provides a snapshot of where new investment is likely to occur. Most new development is located on underdeveloped, vacant, and underutilized sites.

PRIVATE INVESTMENT SUMMARY		
Project Name		Projected Cost
Housing (Unit) *	1,830	\$445,950,000
Hotel (Rooms)	250	\$42,500,000
Office (SF)	1,350,000	\$392,050,000
Retail/Commercial (SF)*	364,000	\$84,300,000
PRIVATE INVESTMENT TOTAL:		\$964,800,000

* Includes on-site parking

BUILD-OUT



IMPLEMENTATION

The implementation strategy identifies a program for creating positive change and stimulating development in the downtown. Public expenditures will be required to leverage significant private investment.

The implementation strategy includes:

Game Changers— Four key projects that will fundamentally change the investment environment of the downtown.

Responsibilities— All projects identified will require coordination and leadership by the City of Bismarck. It is recommended that a single point-of-contact staff position be created to coordinate and manage these projects through implementation.

Schedule— Calls for action on all projects within the first five years to ensure that Plan momentum is established.

PUBLIC INVESTMENT SUMMARY	
Project Name	Projected Cost
Main Avenue Traffic Calming	\$3,500,000
Depot Plaza	\$8,500,000
Fifth Street Improvements	\$10,500,000
Convention Hotel	\$0
PUBLIC INVESTMENT TOTAL:	\$22,500,000

MAIN AVENUE



DEPOT PLAZA



FIFTH STREET



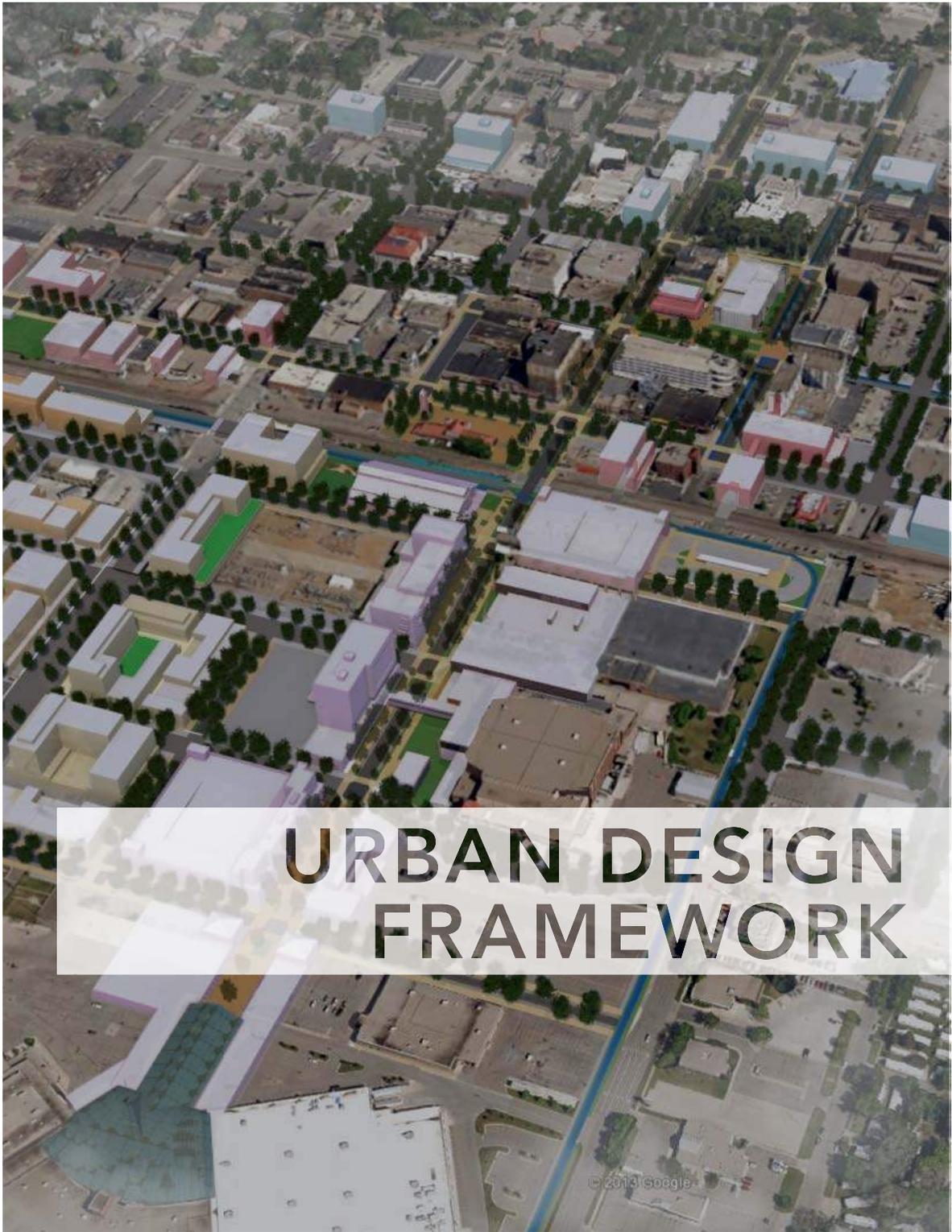
CONVENTION HOTEL SITE



GAME-CHANGERS



- 1 Main Avenue
- 2 Depot Plaza
- 3 Fifth Street
- 4 Convention Hotel Site



**URBAN DESIGN
FRAMEWORK**

OVERVIEW

The urban design framework identifies the types and locations of land uses recommended within the downtown subarea. It provides a vision for coherent downtown growth.

The urban design framework:

- Builds upon and strengthens downtown Bismarck’s greatest assets—its historic buildings, close-in neighborhoods, network of streets, and proximity to the Missouri River and State Capitol
- Recommends preferred locations for the land uses illustrated below

RETAIL



Distinct shopping and entertainment districts are established. The framework strengthens existing retail along Main Avenue and Broadway Avenue and provides opportunities for retail expansion along Fifth Street.

OPEN SPACE



Public spaces, and parks are the key organizing elements for surrounding land uses. A plaza is re-established at the historic center of downtown. It is complemented by a commons, and neighborhood parks.

CIVIC/CULTURAL



Expansion and improvements to the Civic Center, and Belle Mehus Auditorium are fostered to ensure that downtown continues to be an entertainment and special event destination.

EMPLOYMENT



Opportunities for additional office development that enhance and preserve downtown as a major employment center for medical, financial, and government sectors are identified.

HOUSING



Housing animates the downtown with residents and provides customers for downtown businesses. The housing framework identifies a mix of housing types that create a neighborhood.

TRANSIT HUB



A downtown transit hub and multi-modal transit center is located in close proximity to the Civic Center, shopping, and downtown services.

PARKING



Retail parking ramp locations are identified to serve existing and future businesses.

RETAIL

Successful retail will attract residents and visitors to downtown.

The retail framework builds upon downtown Bismarck's unique qualities, including its:

- Wealth of historic buildings
- Existing retail uses
- Resident and visitor attractors consisting of the Civic Center and Belle Mehus Auditorium
- Convenient local and regional access

The proposed retail framework is:

- Compact and walkable, extending approximately 1/4 mile in length along Main Avenue and Fifth Street
- Street-oriented and active, with continuous retail uses activating ground floors of buildings
- Served by adequate parking, with on-street parking and strategically located ramps
- Anchored with retail destinations, the Kirkwood Mall and a new Public Market

Retail Types

The retail framework creates two complementary areas for retail:

- **Main/Broadway Avenue** serve as downtown's primary retail shopping streets
- **Fifth Street** incorporates street-oriented Mall retail expansion, a new Convention Center Hotel and dining/entertainment uses that capitalize on their proximity to the Civic Center

RETAIL



MAIN/BROADWAY AVENUE RETAIL CORRIDOR

Main Avenue, from First Street to Sixth Street, is reestablished as the primary Downtown retail street. Main Avenue and adjacent streets have the potential to add 122,000 square feet of new retail uses to the existing 93,000 square feet that exists today.

Essential components of the retail corridor include:

- **Retail Anchor**— a retail development would be located on the northwest corner of Front Avenue and Fifth Street. The anchor use could include a multi-purpose Public Market (floor plate 30,000 SF +/-) incorporating a farmers market, offices and gallery spaces
- **Depot Plaza**— Depot building improvements would maintain a ground-floor retail tenant
- **Street Improvements**— a traffic-calmed three-lane Main Avenue would include widened sidewalks and intersection improvements such as curb extensions and additional landscaping
- **Parking**— a new Front Avenue parking ramp located on the northeast corner of Front Avenue and Fifth Street with ground-floor retail facing Fifth Street
- **Continuous Retail**— existing and new retail development would be uninterrupted by incompatible uses at the ground floor of buildings and incorporate inviting storefronts

PUBLIC MARKET



FRONT AVENUE RAMP



MAIN/BROADWAY
AVENUE RETAIL

- Existing/Infill Retail
- New Retail
- A Retail Anchor (Public Market)
- P New Retail Parking Ramp (Front Avenue)
- P Existing Ramp



FIFTH STREET RETAIL/ENTERTAINMENT CORRIDOR

From Front Avenue to Kirkwood Mall, a potential for 160,000 square feet of new shops, restaurant and entertainment uses is identified.

Fifth Street would be organized into two distinct zones:

Mall Zone—Kirkwood Mall to Ingals Avenue

- Mall expansion of street-oriented shops

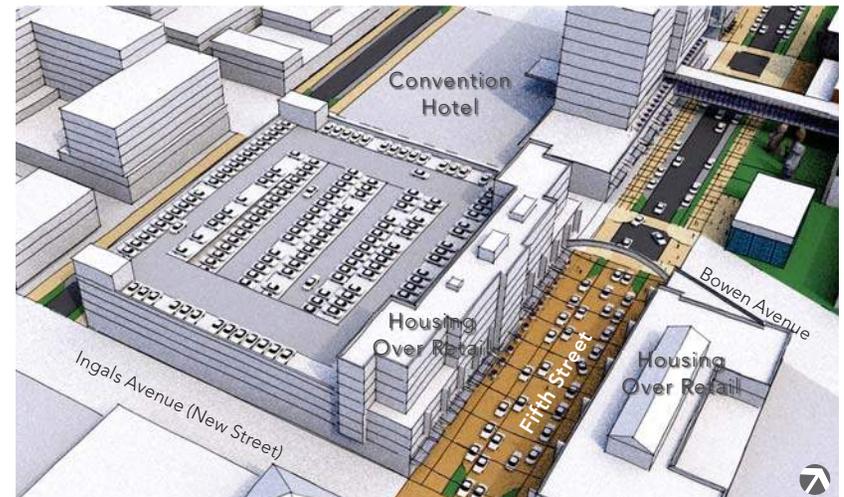
Convention Zone—Ingals Avenue to Front Avenue

- Convention hotel with storefronts on the ground floor
- Storefronts uninterrupted by incompatible uses with upper floor apartments and condominiums
- Bowen Avenue ramp and Front Avenue ramp

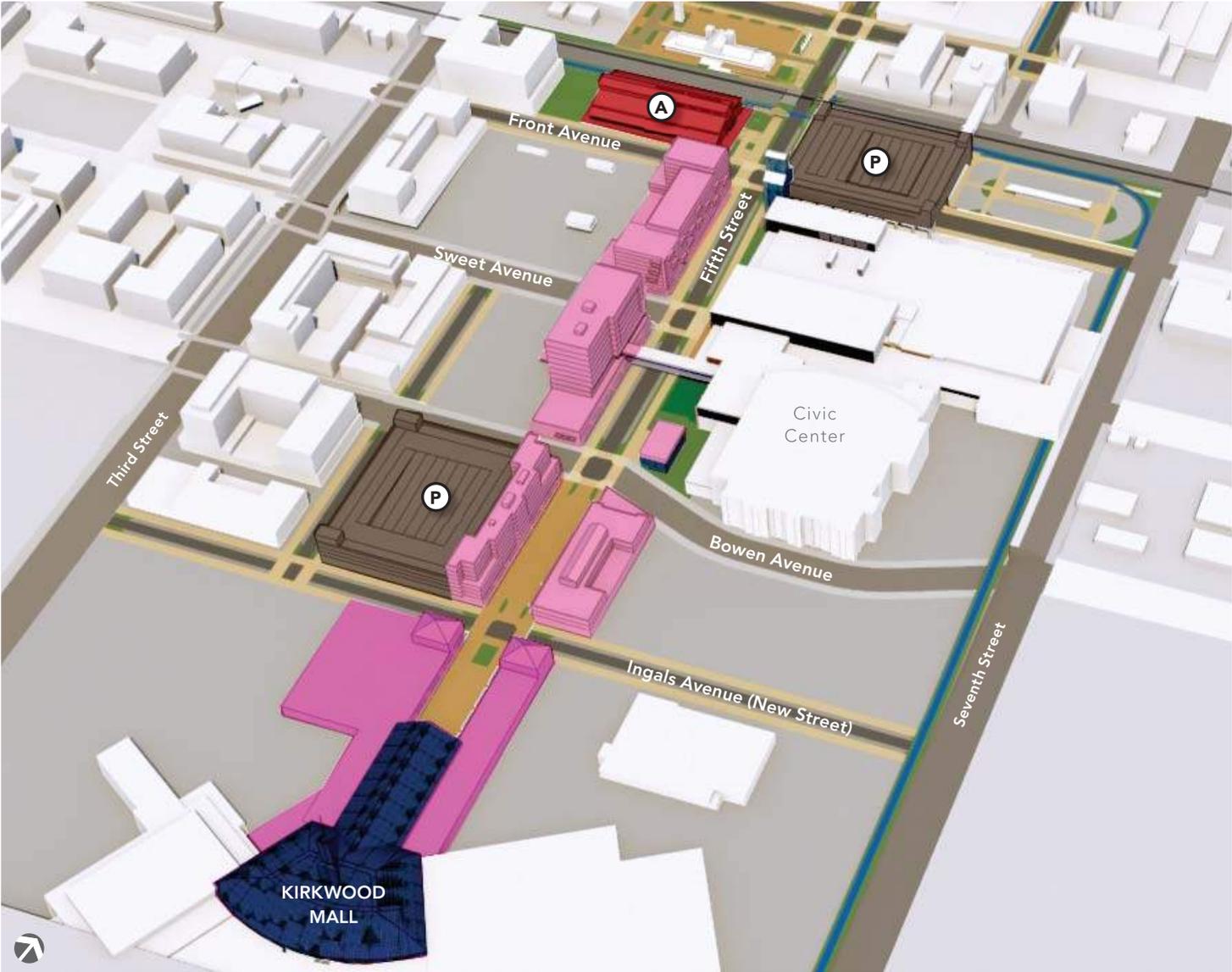
MALL EXPANSION



BOWEN AVENUE RAMP AND CONVENTION HOTEL



FIFTH STREET RETAIL/
ENTERTAINMENT



- Convention Zone
- Mall Zone
- A Retail Anchor (Public Market)
- P New Retail Parking Ramps

OPEN SPACE

New public gathering spaces attract new downtown development and improve livability.

The open space framework establishes the following public spaces:

DEPOT PLAZA



Located at the crossroads of Main Avenue and Fifth Street, Depot Plaza is envisioned as downtown's 'living room', appropriate for special events and casual daily use by workers, residents, shoppers and visitors.

MEHUS COMMONS



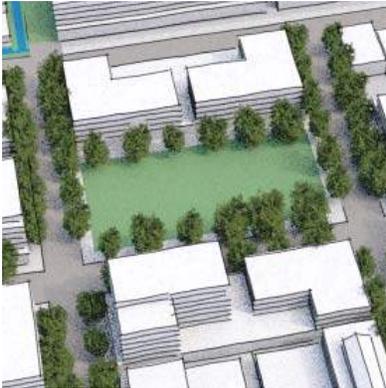
Forecourt to the Belle Mehus Auditorium, the space is integrated with the Courthouse through alley enhancements. This public space should be designed to accommodate public art. It would serve downtown's employees, shoppers, and Belle Mehus events.

MALL



The Mall expansion incorporates a pedestrian-only street that provides a front-door entry.

NEIGHBORHOOD PARKS



Parks provide amenities for a new and existing housing neighborhoods.

OPEN SPACE



- Gathering Space
- Neighborhood Park
- Existing Park/Open space
- A Depot Plaza
- B Mehus Commons
- C Mall
- D Neighborhood Parks

DEPOT PLAZA

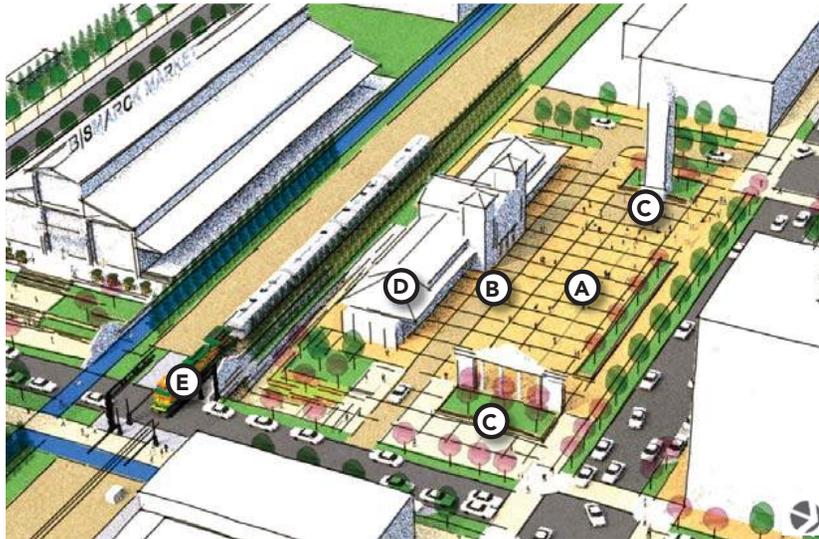
The plaza is envisioned to serve as the primary location for downtown public events.

The existing surface parking is to be relocated to the Bismarck/Burleigh Public Health Building parking lot.

The plaza would include:

- A paved and multi-purpose area
- A curbless plaza access drive for drop-off to the Depot Building, which could be closed to expand the Civic Celebration Zone
- Child-friendly elements such as water features
- A connection to the Public Market and Civic Center by a Fifth Street pedestrian underpass

DEPOT PLAZA ELEMENTS



- A Civic Celebration Zone
- B Access Drive
- C Seating Areas/Water Feature
- D Depot Building Retail
- E Fifth Street Pedestrian Underpass

EXISTING DEPOT BUILDING AND PARKING LOT



DEPOT PLAZA



MEHUS COMMONS

A complement to Depot Plaza, the Commons provides an open space amenity that organizes downtown's civic use. The commons would include the following features:

AUDITORIUM GREEN



A forecourt and greenspace showcasing the Belle Mehus Auditorium and extending along Broadway Avenue from Fifth Street to Sixth Street would include:

- A flexible space on Sixth Street that can be temporarily closed for Belle Mehus Auditorium Events
- More green than paved areas incorporating elements such as a lawn and/or large landscaped beds
- Areas for outdoor dining, small assembly and public art
- Walkways

BURLEIGH MALL



Alleyway enhancements linking the Mehus Commons with the Burleigh County Courthouse and lawn would include:

- A curbless street design with a strong pedestrian bias and limited or restricted auto access
- Special brick or concrete paving
- All utilities relocated underground
- Opportunities for public art display and sculptural elements
- Seasonal elements
- Overhead lighting

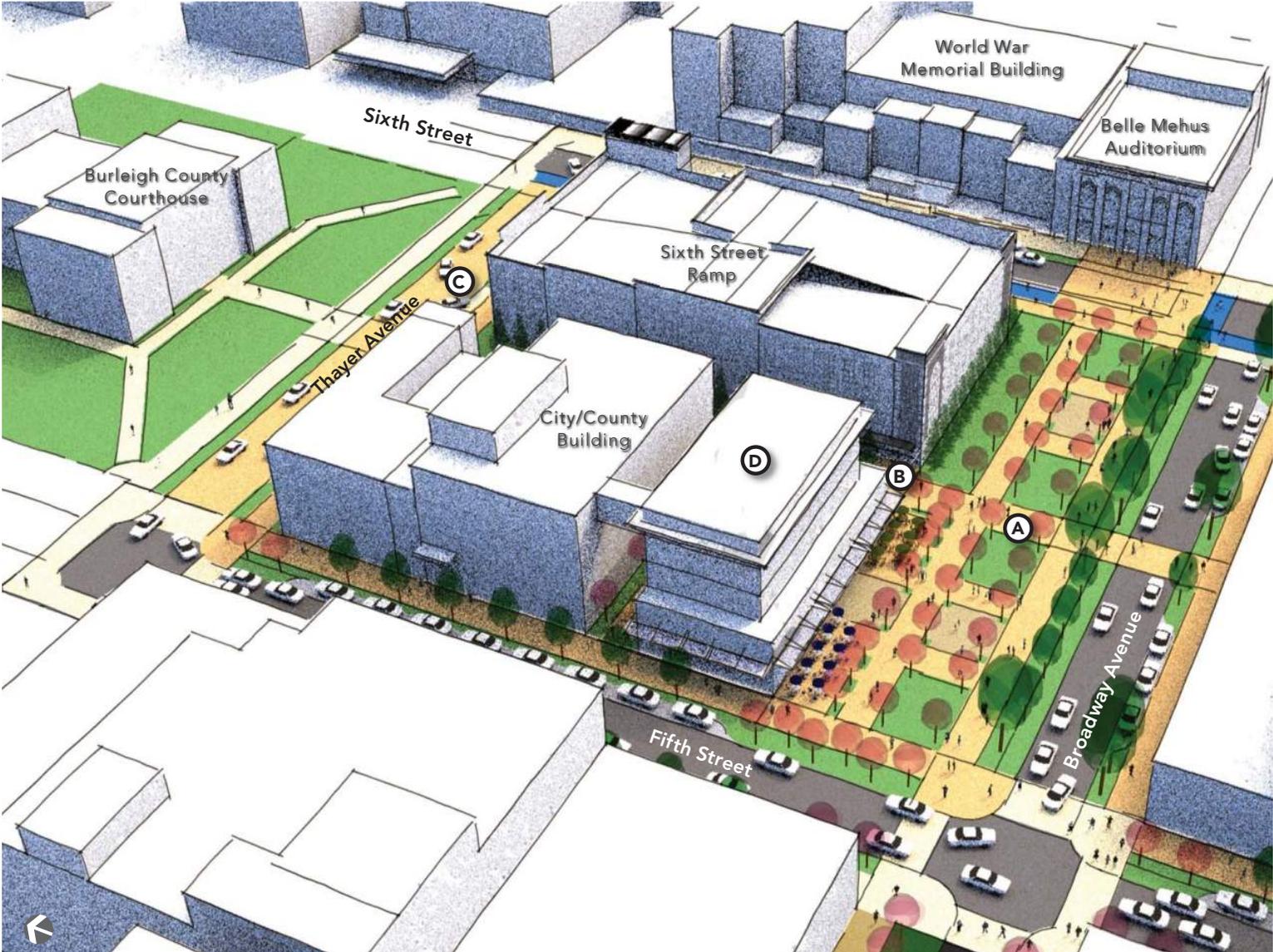
THAYER FESTIVAL STREET



A curbless street design accommodating temporary street closure from Fifth Street to Sixth Street for events would include:

- Widened sidewalks
- A two-lane roadway, one lane in each direction
- Curbside parking
- Landscaping and pedestrian scaled lighting

MEHUS COMMONS



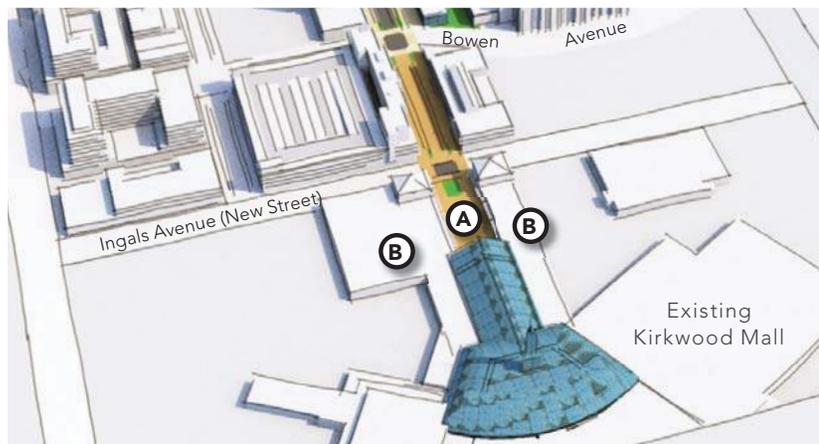
- A Auditorium Green
- B Burleigh Mall
- C Thayer Festival Street
- D New Development

MALL

The Mall open space provides a Fifth Street 'front-door'. The mall would serve as a privately owned and maintained gathering space for daily and special event activities.

Mall elements include:

- A festive environment created by use of ornamental lighting
- Fountains or other child-friendly amenities
- Locations for public art
- Landscaped planting beds



- A Pedestrian-Only Street
- B Storefronts

MALL



- Gathering Space
- Existing Park/Open space

NEIGHBORHOOD PARKS

New parks located in the northwest, southwest, and southeast quadrants, provide an amenity for existing housing and future high-density housing. Parks should be no smaller than one-half acre.

Neighborhood Parks elements would include:

- Lawn, canopy trees and planting beds
- Children’s play areas and structures
- Benches, trash receptacles and drinking fountains
- Restrooms and covered structures (optional)

SOUTHWEST QUADRANT PARK



- A Neighborhood Park
- B New Condos & Apartments

NEIGHBORHOOD PARKS



- Neighborhood Parks
- Existing Park/Open space

CIVIC/CULTURAL

Downtown must be strengthened as the region’s primary visitor destination. The civic/cultural uses will in turn stimulate retail sales and lodging business.

The Civic/Cultural framework:

- Envisions the full expansion of the Civic Center
- Recommends re-use and adaptation of the World War Memorial Building as a hub for community recreational activities, arts education, galleries, and performance spaces

CIVIC CENTER



MEMORIAL BUILDING



BELLE MEHUS



CIVIC/CULTURAL



EMPLOYMENT

Downtown Bismarck would be strengthened as the region's center for financial, medical, and energy related employment.

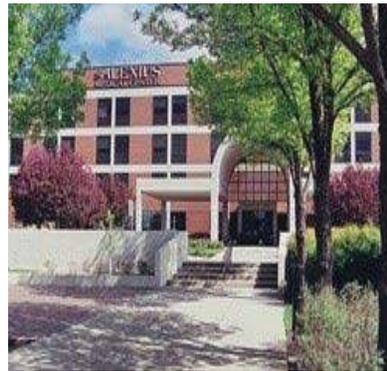
The Employment framework:

- Identifies space for government uses
- Includes parcels that accommodate office buildings that are competitive with suburban office sites
- Would accommodate expansion of medical facilities

GOVERNMENT



MEDICAL



FINANCIAL



ENERGY



NEW EMPLOYMENT



HOUSING

Downtown housing is located in close proximity to jobs and shopping.

The Housing framework:

- Identifies the opportunity for a high-density neighborhood in the southwest quadrant
- Would provide opportunities for a variety of housing types for all incomes and ages
- Envisions housing on upper floors along Main Avenue and Fifth Street

CONDOMINIUMS



APARTMENTS



TOWNHOMES



NEW HOUSING



TRANSIT HUB

The hub is centrally located for easy access to downtown retail, services and the Civic Center.

The Front Avenue location is strategically located for a Multi-Modal Transportation Hub that can incorporate parking and transit, making the Hub an ideal candidate for federal funding through the MAP-21 program. Tied to a substantial federal funding source, the ramp and transit center would be considered a priority project. Further design refinement of a ramp and transit facility should be developed in the near term and a formal grant application prepared.

The Transit Hub could be constructed as a single project on the site northeast of Front Avenue and Sixth Street or in conjunction with the Front Avenue Ramp. A concept for a stand alone transit center hub is illustrated below and would include:

- A center platform island serving twelve full-size (40') buses to be shared by Capital Area Transit (CAT) and other regional providers
- A building that includes a waiting area, information desk/work space, driver restroom, small break room and public restrooms
- Two to three auto parking spaces for CAT/Bis-Man Transit
- Taxi queing spaces

TRANSIT HUB CONCEPT



- A 12 Bus Bays (40')
- B Center Platform & Building
- C CAT/Bis-Man Parking
- D Taxi
- E Rail Trail
- F Rail Trail Ramp
- G Stair to Underpass

TRANSIT HUB



PARKING

Retail expansion will require additional retail parking in close proximity to existing and new shops.

Retail parking framework:

- Envisions two retail ramps that serve the Main Avenue and Fifth Street retail and entertainment corridors including the Civic Center
- Would include ground-floor retail on retail framework streets
- Would be visible and accessible from retail streets
- Would be designed for easy internal circulation, especially for those unfamiliar with maneuvering within parking ramps

Employee parking framework:

- Would envision three employment ramps to serve existing and future employment growth

All ramps would not look like a parking-ramp, rather the ramp design vocabulary would use fenestration similar to nearby office, housing or retail buildings. Furthermore, it is recommended that all future ramps include active uses at the groundfloor.

GROUND-FLOOR RETAIL



COMPATIBLE DESIGN



PARKING



FRONT AVENUE RAMP

The parking ramp replaces an existing parking lot and Bismarck/Burleigh Public Health Building and is located near the Civic Center and close to Main Avenue and Fifth Street retail shops. Parking includes:

- A minimum 240' x 300' dimension with one-way auto circulation
- Glass-enclosed elevators located on the Fifth Street facade that provide a direct view to retail businesses and an all-weather connection to the Civic Center
- Ground-floor retail tenant space along Fifth Street
- Vehicular access from Front Avenue

FRONT RAMP LOOKING SOUTHEAST



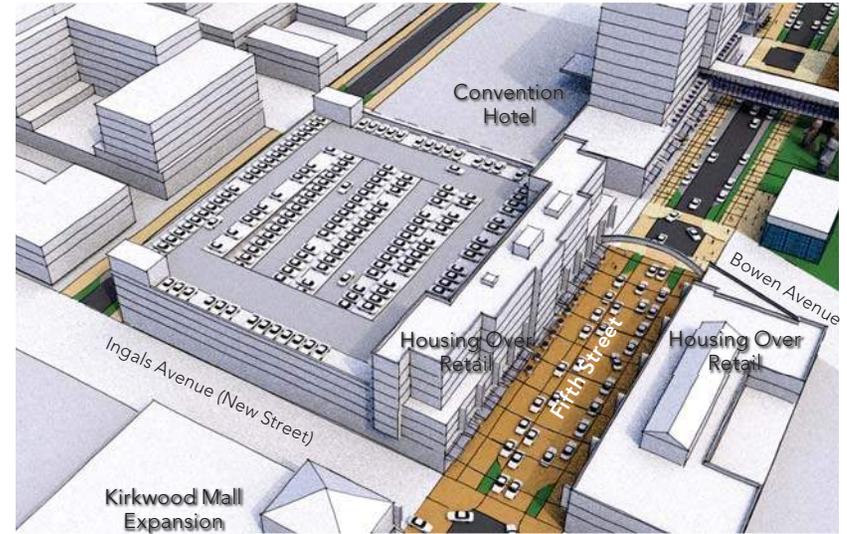
BOWEN AVENUE RAMP

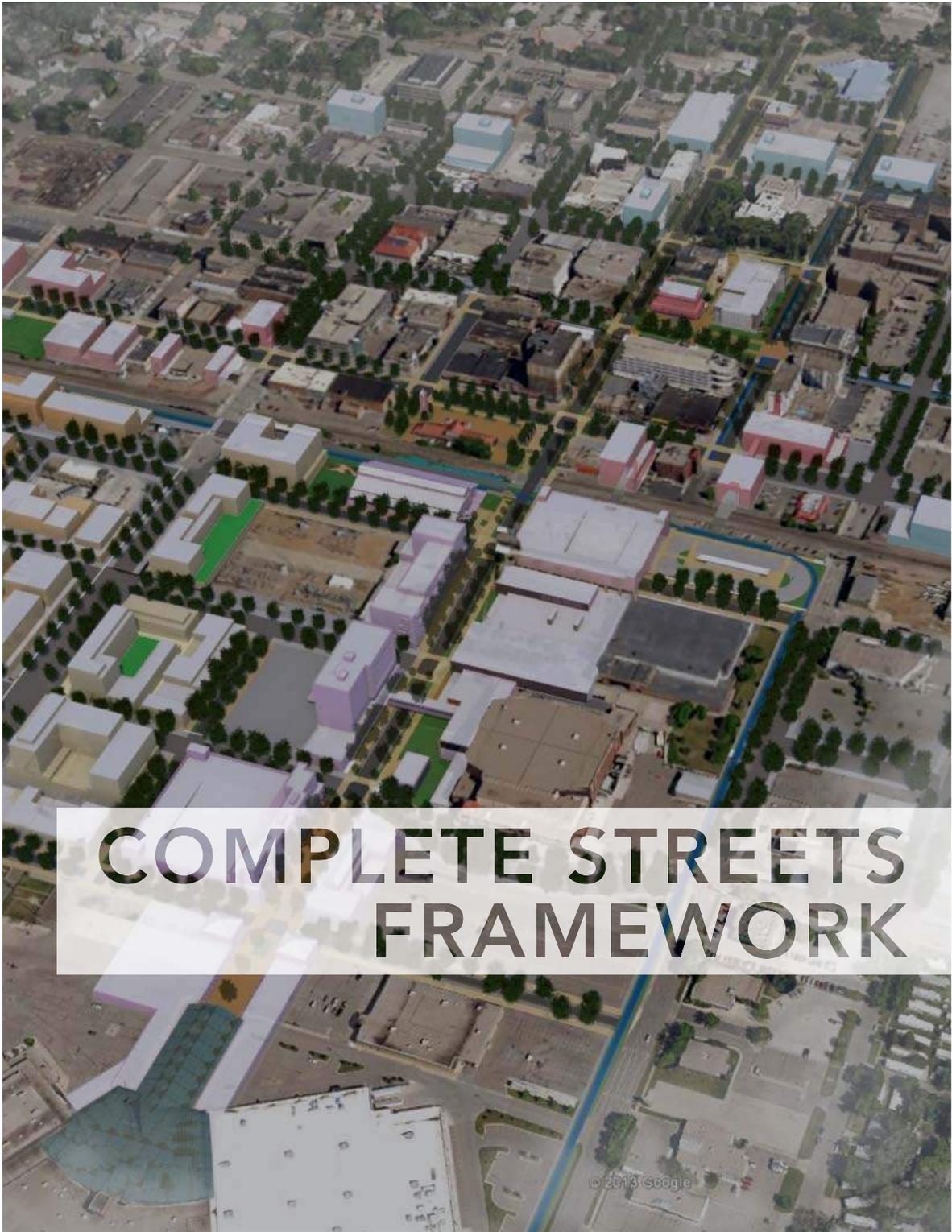
The parking ramp provides parking for the Civic Center, retail shops, entertainment uses, Kirkwood Mall expansion and supplemental parking for the Convention Hotel.

The ramp could incorporate a separate liner building that conceals the parking structure with an active retail ground floor along Fifth Street and upper floor housing above. The design of the ramp would incorporate:

- A minimum 240' x 300' ramp dimension with one-way auto circulation
- Glass-enclosed elevators located on the Bowen Avenue facade that provide a direct view to retail businesses and possible sky-bridge connection to the Convention Hotel

BOWEN RAMP LOOKING NORTHWEST





COMPLETE STREETS FRAMEWORK

OVERVIEW

The Complete Street framework incorporates recommendations for creating a pedestrian-friendly downtown and provides pedestrian, bicycle and transit enhancements coupled with auto traffic-calming measures. Not all downtown streets are required to include all modes.

PEDESTRIAN



BICYCLE



TRANSIT



AUTO



PEDESTRIAN

A network of pedestrian-friendly streets link downtown to the outlying neighborhoods.

The Pedestrian framework includes the following components:

Signature Street

Main Avenue and Fifth Street establish the 'cruciform' structure for retail development and Fifth Street provides a linkage between the Kirkwood Mall to the public library. The envisioned pedestrian improvements incorporate landscaping and widened sidewalks to foster walking, outdoor seating, and public art display.

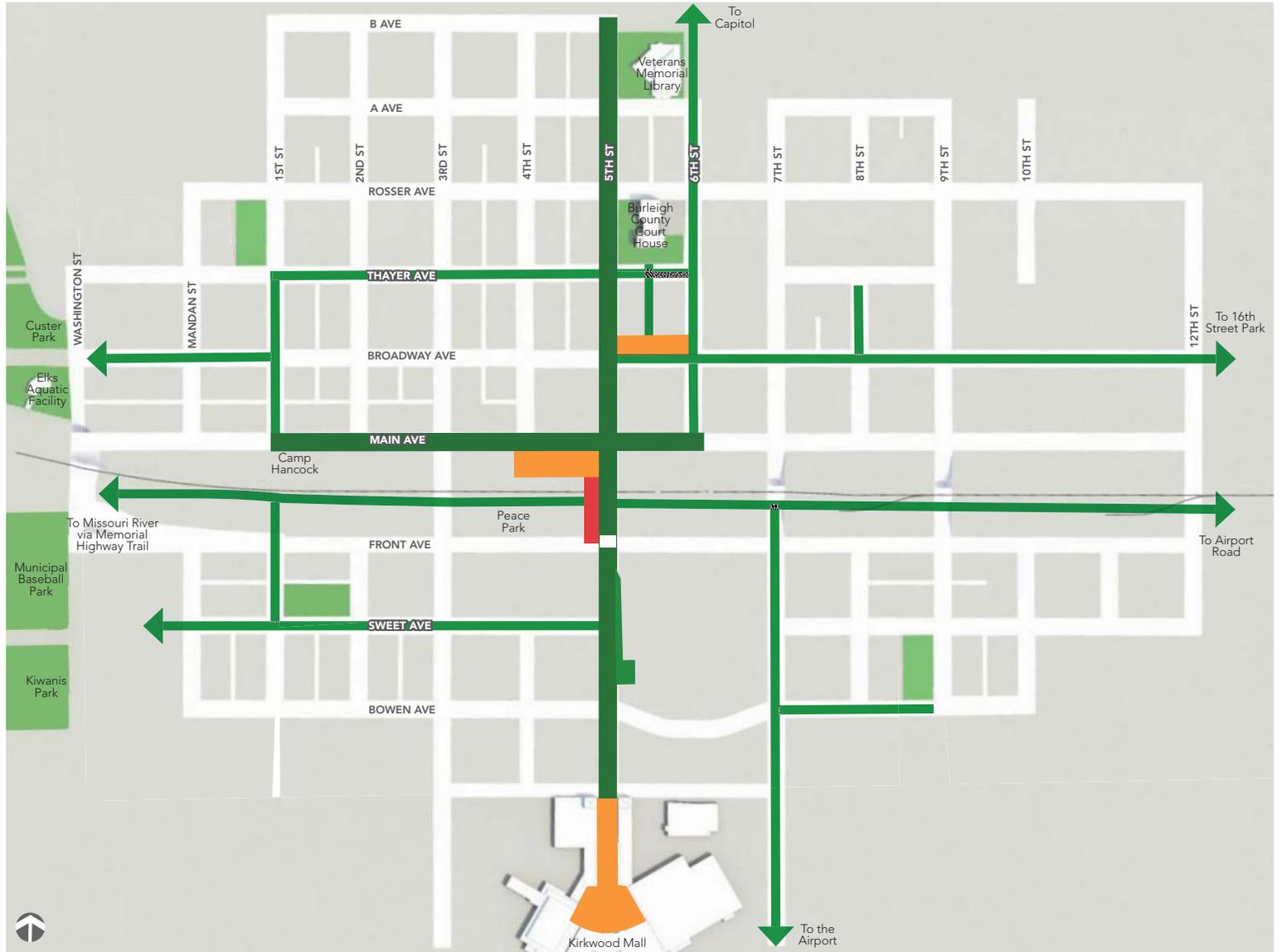
Neighborhood Connector

Streets linking downtown to outlying neighborhoods between destinations such as parks, new housing, employment and shopping areas are identified. These routes may include off-street shared multi-use trails and at elevated crossings, such as at Seventh Street and Ninth Street, the design should incorporate a cantilever to the existing rail crossing or separate pedestrian/bicycle bridge structure.

Pedestrian Underpass

The Fifth Street Pedestrian Underpass is envisioned as a light and airy connection under the BNSF rail line thereby linking Main Avenue to the Civic Center and Kirkwood Mall.

PEDESTRIAN



- Signature Street
- Pedestrian Underpass
- Neighborhood Connector
- Public Gathering Area
- Parks

BICYCLE

A safe off-street bicycle framework increases bicycle ridership.

The off-street system consists of:

- Protected bikeways located between the sidewalk and parking lanes
- Shared pedestrian and bicycle multi-use trails

PROTECTED BIKEWAY



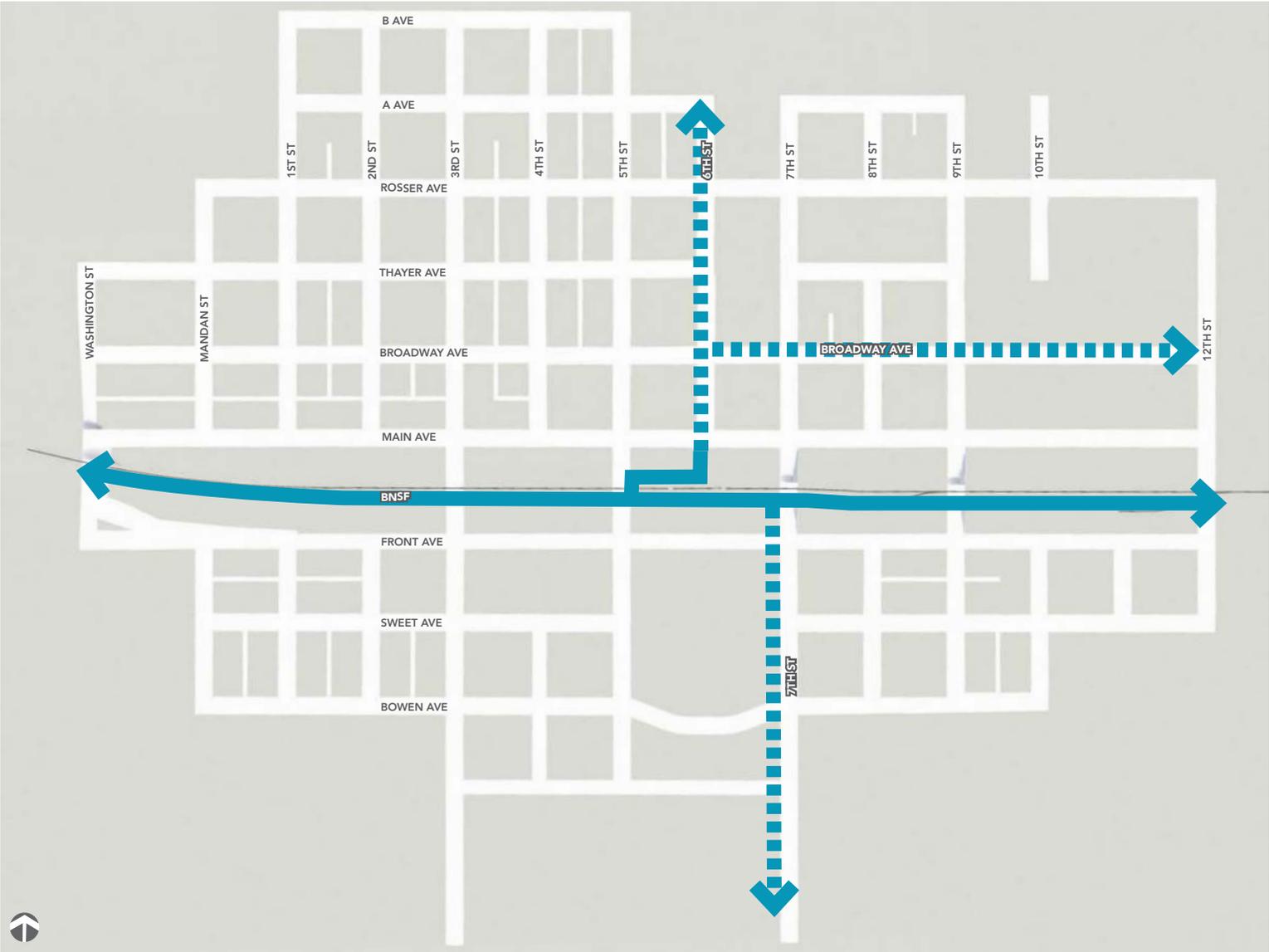
MULTI-USE TRAIL



Common design elements of the protected bikeways and multi-use trails include:

- Bicycle lanes separated from traffic by a curb, landscaped buffer and parked cars
- Special intersection design treatments that minimize auto, bicycle and pedestrian conflicts and provide safe crossings
- Protected bikeway signal phases

OFF-STREET BICYCLE



TRANSIT

Downtown bus transit offers riders the opportunity for improving access to jobs and services.

In 2011, Capital Area Transit adopted the *Mobility 2017 Transit Plan: Transit Roadmap for Bismarck and Mandan*.

The plan recommended the following preferred modifications that expand service with the following objectives:

- Increase funding and improve service headways with a fixed-route service and better frequencies (minimum of 60 minutes midday, 30 minutes peak)
- Match service levels with demand to enhance ridership
- Eliminate one-way loops and provide direct bi-directional service
- Minimize double transfers so routes pulse into and out of a single transfer location
- De-emphasize service to elementary and middle schools

Based on the preferred modifications, possible ways to serve the Front Avenue transit hub are identified on the potential service routes map.

POTENTIAL SERVICE ROUTES



Front Avenue Transit Hub

AUTO

Within the downtown core, auto access will be improved, and 'calmed' to enhance livability.

The auto framework includes:

Traffic Calmed Streets

Main Avenue from First to Sixth and Fifth Street from Bowen to Rosser are envisioned to be converted from a four to a two-travel lane street with a dedicated left turn lane. The converted roadways provide adequate capacity for existing and future traffic and the dedicated turn lanes improve traffic safety and reduce through vehicle conflicts with left turning vehicles.

Two-way Traffic Conversion

Chancellor Square is envisioned to be converted to two-way traffic, thereby providing a Main Avenue parallel alternative route (along Broadway Avenue). A benefit of two-way conversion is improved vehicular access to and increased drive-by exposure for adjacent businesses.

New Streets

New streets provide access to new housing development and the proposed Bowen Avenue parking ramp. New streets are envisioned to include two travel lanes, curbside parking, wide sidewalks, canopy street trees, and landscaped curb-extensions at intersections.

Mobility Streets

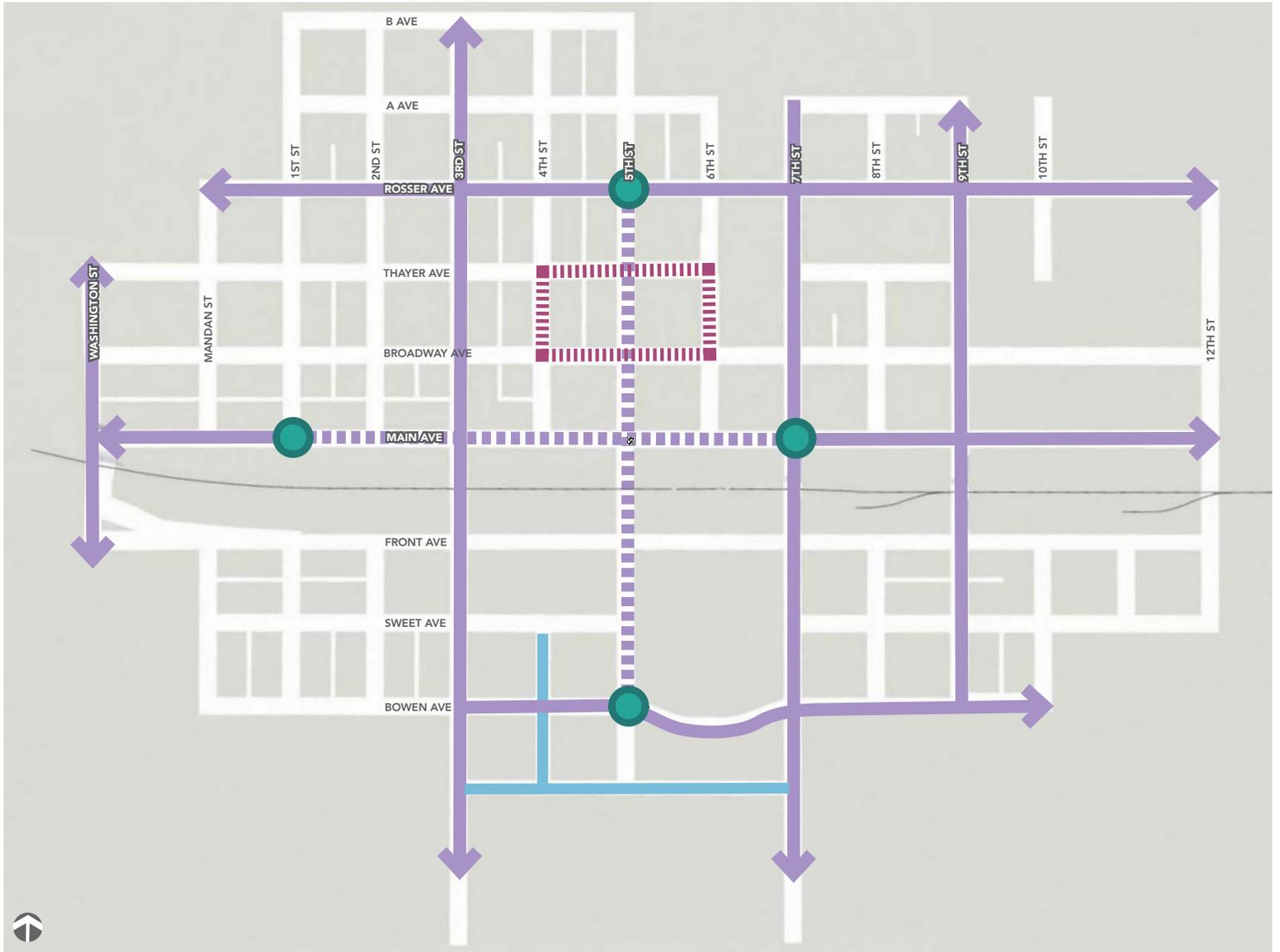
Downtown access is maintained on existing arterials. The envisioned roadway improvements include:

- Two through lanes on Third Street , and an eastbound right-turn lane at Main Avenue and Third Street
- Dual westbound left-turn lanes at Main Avenue & Seventh Street
- Removal of the overhead beacon at Bowen Avenue & Seventh Street
- Adding protected left-turn phasing at Main & Third/Main & Front

Gateways

Gateways are located to define the transition from mobility streets to traffic-calmed streets. The gateways should incorporate directional and informational signage, and streetscape elements such as signature lighting and banners.

AUTO



- Traffic Calmed Streets
- Two-Way Traffic Conversion
- Mobility Street
- New Streets
- Gateway

COMPLETE STREETS IMPROVEMENTS

Elements of the Complete Streets improvements for the downtown core are identified on the following pages and include:

- Main Avenue Improvements (First Street to Sixth Street)
- Fifth Street Improvements (Front Avenue to Kirkwood Mall)
- Fifth Street Improvements- Variations (South of Bowen Avenue and north of Front Avenue)
- Fifth Street Pedestrian Underpass
- Sixth Street Cultural Trail (Main Avenue to Avenue A)
- Rail Trail
- Chancellor Square- Two-Way Conversion

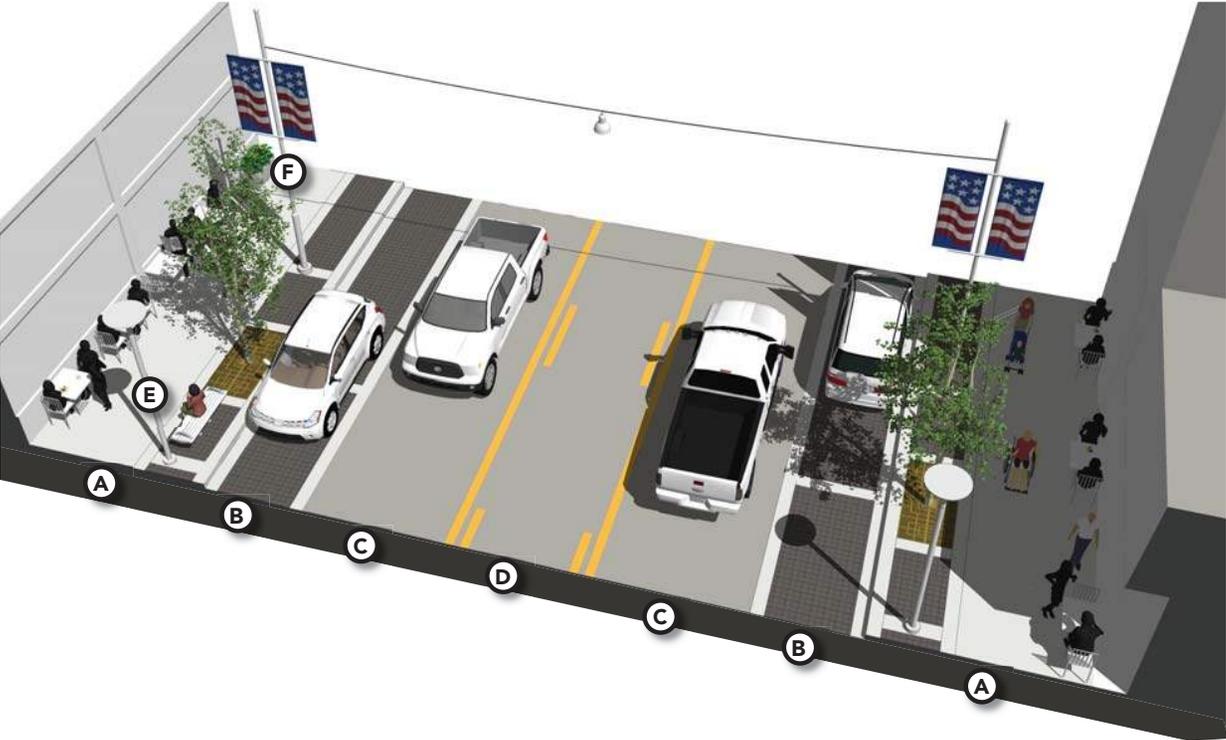
COMPLETE STREETS IMPROVEMENTS



MAIN AVENUE IMPROVEMENTS

Right-of-way improvements to Main Avenue (First Street to Sixth Street) will calm traffic. Widened sidewalks will improve the pedestrian environment that will, in turn, increase retail sales.

TYPICAL MAIN AVENUE SECTION



- A Widened Sidewalk (15')
- B Parking Lane (8')
- C Travel Lane (12')
- D Turn Lane (10')
- E Sidewalk Lighting
- F Overhead Suspended Light Fixtures

Main Avenue elements:

- Reduced Roadway Width—Removal of two travel lanes and the addition of a continuous left turn lane
- Enhanced key intersections—Landscaped curb extensions, special crosswalk treatments, and depressed corner curbs
- Signalization timing adjustments to reduce vehicle speed
- Prohibition of new driveways
- Additional suspended overhead lighting to enhance the shopping experience and provide a signature element to the street

TYPICAL MAIN AVENUE PLAN

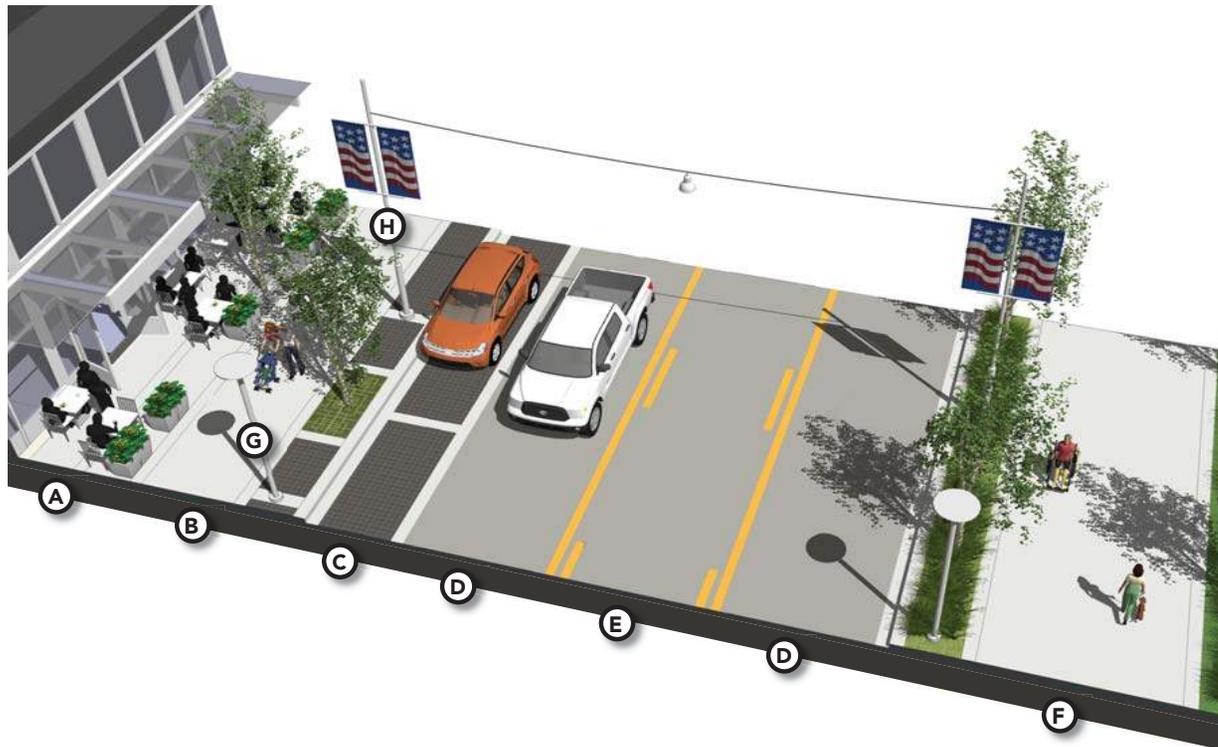


- A Convert 4-Lanes to 2-Lanes;
Add a Continuous Left Turn Lane
- B Curb Extensions
- C Depressed Curbs at Corners with
Tight Corner Radii to Reduce
Crossing Distances
- D Special Crosswalk Treatment

FIFTH STREET IMPROVEMENTS

Right-of-way improvements to Fifth Street are envisioned to foster nightlife activities such as restaurants and provide opportunities for additional shopping. Widened sidewalks will accommodate Civic Center event pedestrian surges. Opportunities for public art along a 'promenade' sidewalk between the Kirkwood Mall and Main Avenue will be provided.

TYPICAL FIFTH STREET SECTION



- A Sidewalk Seating/Dining Building Easement or Setback (10')
- B Wide sidewalk (15')
- C Parking Lane (8')
- D Travel Lane (12')
- E Turn Lane (13')
- F 'Promenade' sidewalk, public art, and curbside landscaping (20')
- G Pedestrian-Scaled Sidewalk Lighting
- H Overhead Suspended Light Fixtures

Fifth Street elements:

- Traffic calming design—Reduced number of travel lanes from four to two with continuous left turn lane
- Enhanced key intersections—Landscaped curb extensions, special crosswalk treatments and tabled intersections or depressed corner curbs
- Adjusted signalization to reduce roadway speed
- Prohibition of new driveway access—Existing driveways will be maintained if desired by current property owners or businesses
- Pedestrian Underpass—Located on the west side of Fifth Street at the BNSF rail crossing; the underpass provides unimpeded pedestrian access to and from the Civic Center, Kirkwood Mall and Main Avenue

FIFTH STREET—VIEW LOOKING NORTH TO FRONT AVENUE



FIFTH STREET IMPROVEMENTS— VARIATIONS

Variations from the typical Fifth Street section include:

- 'Festival Street'- South of Bowen Avenue to Ingals Avenue
- North of Front Avenue to the Library

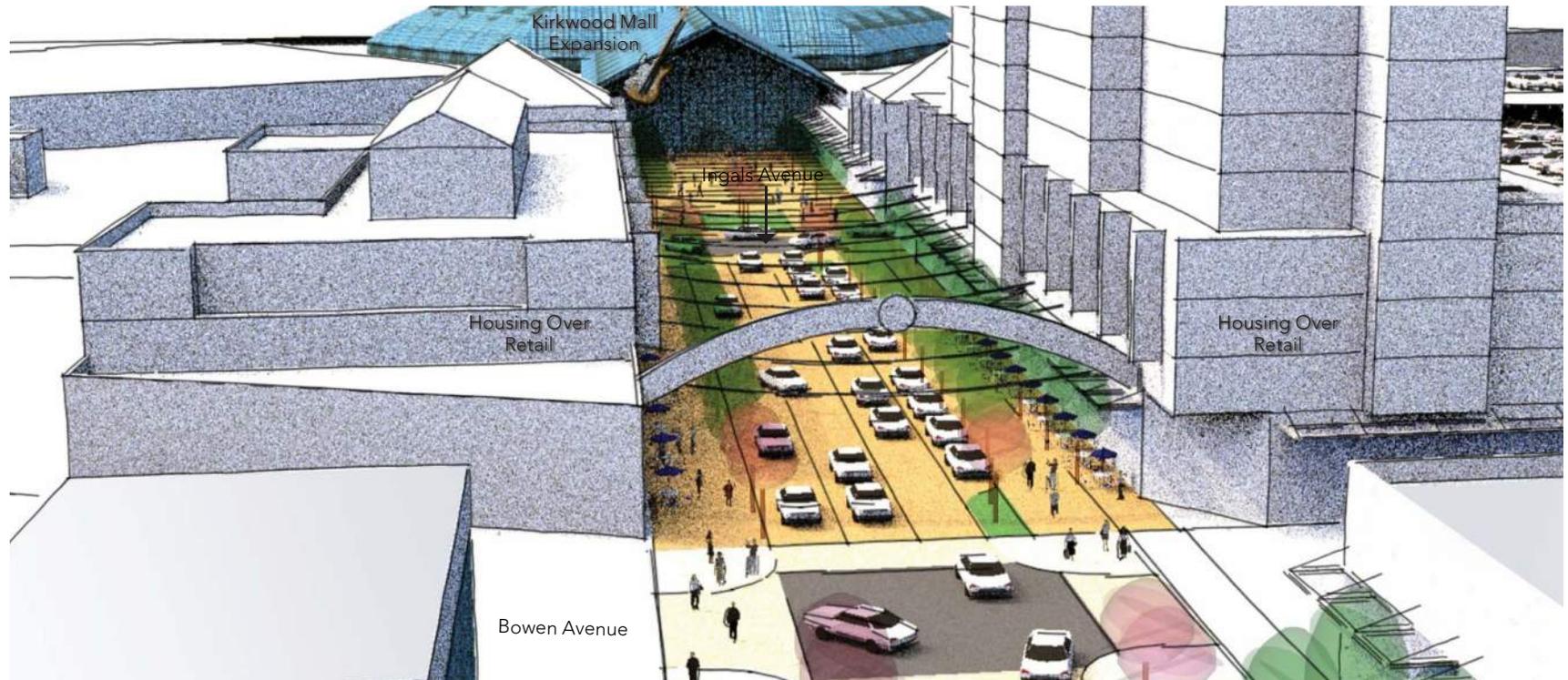
Festival Street—Bowen Avenue to Ingals Avenue

The Festival Street would be temporarily closed for street fair events or activities associated with the Civic Center.

Fifth Street elements:

- A curbless, brick paved surface from building edge to building edge
- Wide sidewalks (Minimum 18' each side)
- A two-lane roadway (12' lanes)
- Curbside parking both sides of street (8' parking lane)
- Curbside landscaping and pedestrian scaled lighting
- Curb extensions and enhanced crossings at intersections
- Overhead suspended street light fixtures

FESTIVAL STREET—VIEW LOOKING SOUTH OF BOWEN AVENUE TO KIRKWOOD MALL



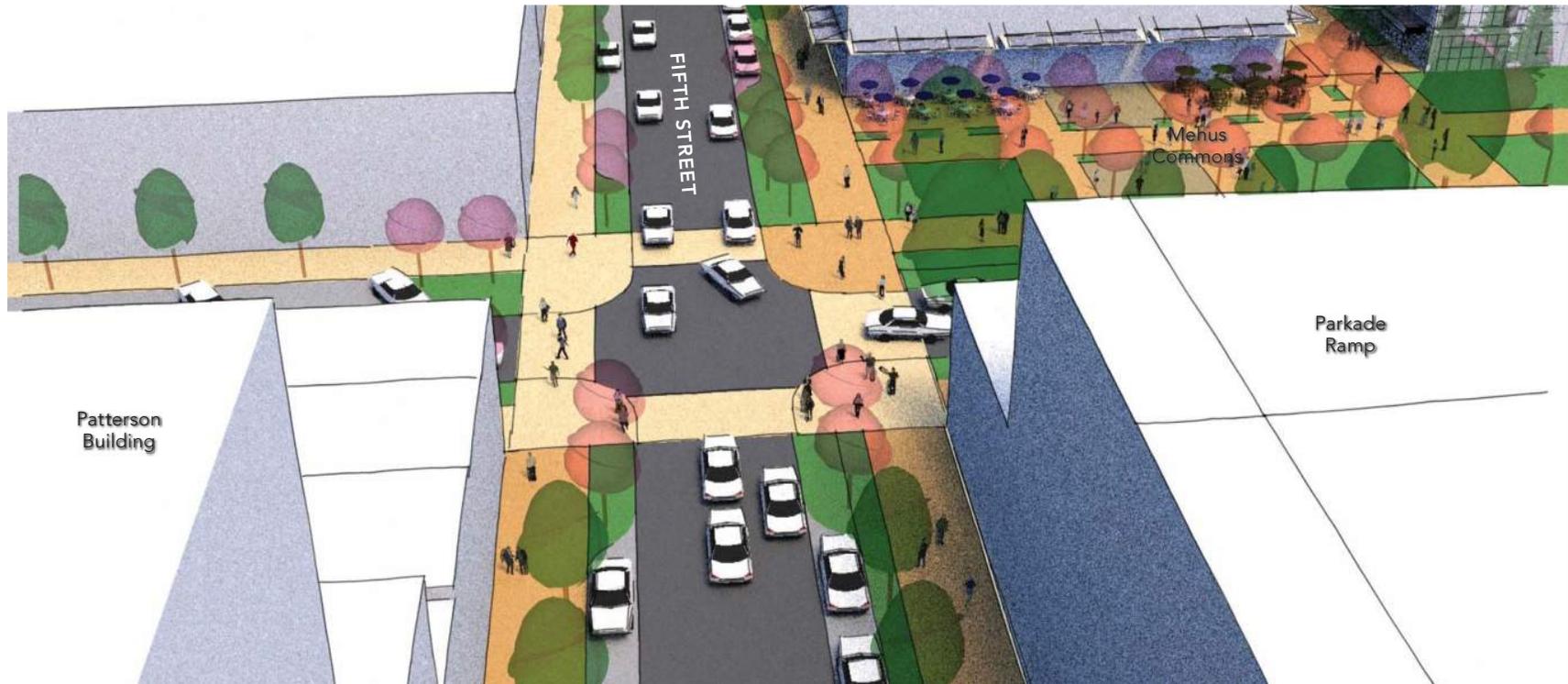
Fifth Street—Front Avenue to Library

Fifth Street north of Front Avenue is envisioned to incorporate a wide sidewalk with additional landscaping along the east side of the roadway. Curbside parking on the west side of the roadway should be maintained. In areas where curbside parking serves retail uses on the east side of the roadway, curbside parking would be provided and the landscaping and/or sidewalk width would be reduced.

Fifth Street elements:

- Wide sidewalks
- Curbside landscaping
- A two-lane roadway, one lane in each direction
- A left turn lane where necessary
- Curb extensions and enhanced crosswalks at all intersections

FIFTH STREET—VIEW LOOKING NORTH AT BROADWAY AVENUE



FIFTH STREET PEDESTRIAN UNDERPASS

The Fifth Street pedestrian underpass is located adjacent to and outside of the existing street right-of-way. A 10' easement will be required which will necessitate modification of the Bismarck Art & Galleries Association and Bismarck Parks and Recreation Building.

The twenty foot wide underpass would be designed to be graffiti resistant and well lit even during daylight hours.

LOOKING NORTH



UNDERPASS VIEW LOOKING NORTH



FIFTH STREET UNDERPASS



- A Stairs
- B 20' Wide Underpass
- C ADA Compliant Ramp
- D Sloping Sidewalk
- E 8' Wide Landscaped Buffer
- F Landscaped Terracing and Stairs

SIXTH STREET CULTURAL TRAIL

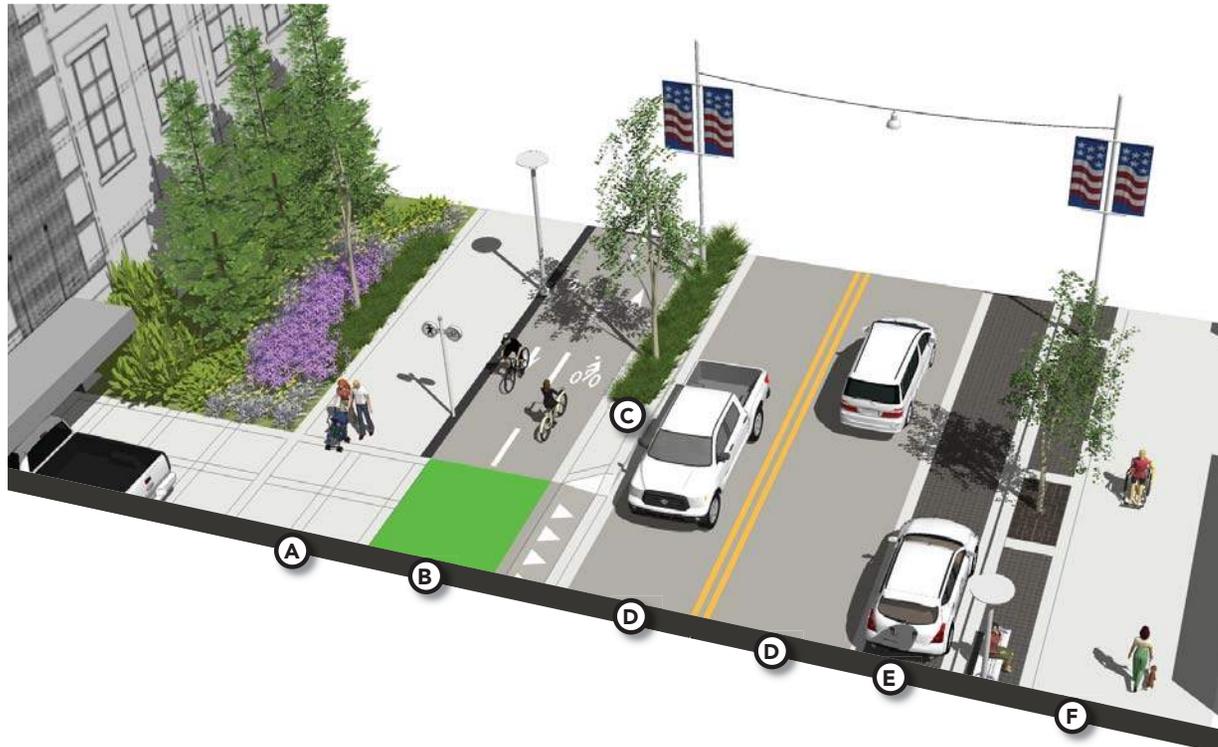
Right-of-way improvements on Sixth Street would include a protected bikeway along the west side of the street, coupled with enhanced sidewalks and crosswalks, linking Downtown to the Capitol campus.

Cultural trail improvements outside of the Subarea boundary will require additional study, and design refinement and is beyond the scope of this plan.

Sixth Street elements:

- Two-Way traffic
- Enhanced key intersections—landscaped curb extensions on the east side of the street, special crosswalk treatments and tabled intersections or depressed corner curbs
- Special intersection design treatments that minimize auto, bicycle and pedestrian conflicts and provide safe pedestrian and bicycle crossings and left-turning auto movements
- Adjusted signalization to reduce vehicle speed and new bicycle signals
- Modified Driveway Treatments—special bike pavement markings and driveways level with the sidewalk

TYPICAL SIXTH STREET SECTION



- A Wide Sidewalk (14' east side)
- B Bi-Directional Protected Bikeway (12')
- C Raised Curb Planting Bed (6')
- D Two Travel Lanes (12' Each)
- E Parking Lane (8')
- F Wide sidewalk (16' west side)

SIXTH STREET CULTURAL TRAIL

Sixth Street Ramp

In an effort to not preclude the future protected bikeway, the following recommendations for the construction of Sixth Street in conjunction with the new ramp are as follows:

- Construct only sidewalk and curb improvements along Sixth Street and Thayer Avenue
- Omit construction of planned planters or curb extensions
- Do not depress sidewalks at driveway locations to the ramp
- Lay down curb at the Thayer Avenue intersection with a maximum 15' radius



RAIL TRAIL

The rail trail provides a pedestrian and bicycle link from the downtown to the Missouri River.

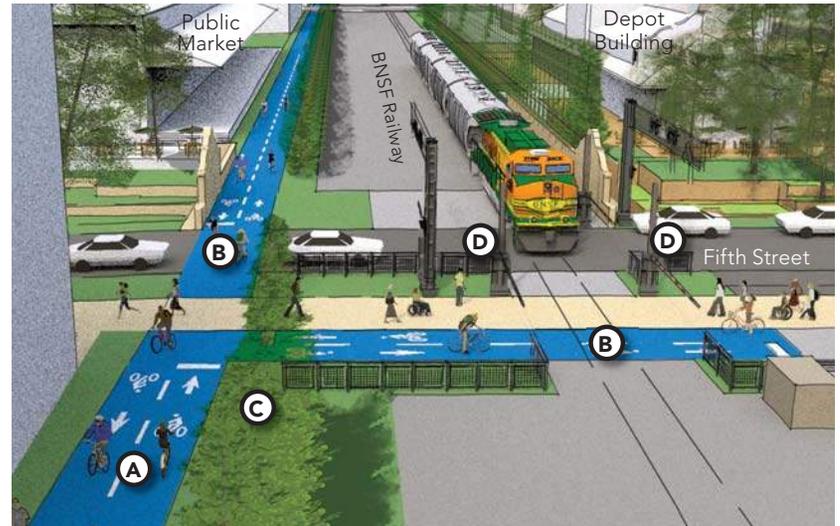
The rail trail is located adjacent to the southerly edge of the BNSF right-of-way. The trail is envisioned to extend from Seventh Street to the existing paved trail west of Washington Street. Trail elements include special crosswalk treatments at cross streets and rail crossings to minimize conflicts with autos and trains.

RAIL TRAIL EXAMPLE



RAIL TRAIL- VIEW LOOKING WEST

- A Shared pedestrian and bicycle multi-use trail (12')
- B Special pavement markings at street and rail crossings
- C Landscaping/Fencing Barriers
- D Pedestrian and bicycle signal arms at rail crossing



TWO-WAY TRAFFIC CONVERSION (CHANCELLOR SQUARE)

Two-way conversion of Chancellor Square requires improvements that can be limited to restriping and signal adjustments.

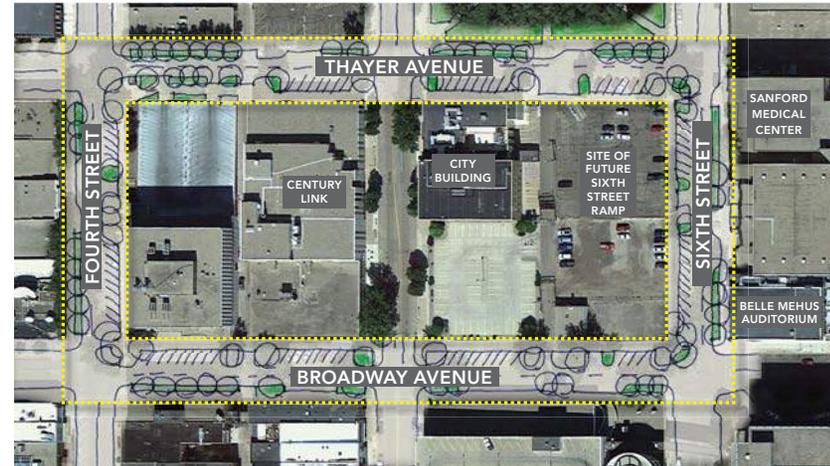
Two-Way elements:

- Restriping of the travel lanes from one-way to two-way direction
- Reverse restriping of angled parking along Thayer, Fourth, and Broadway to accommodate change in vehicle travel direction. As an alternative to restriping, current striping could remain for back-in angled parking.
- Relocating traffic control signs, signals and other devices (as necessary)

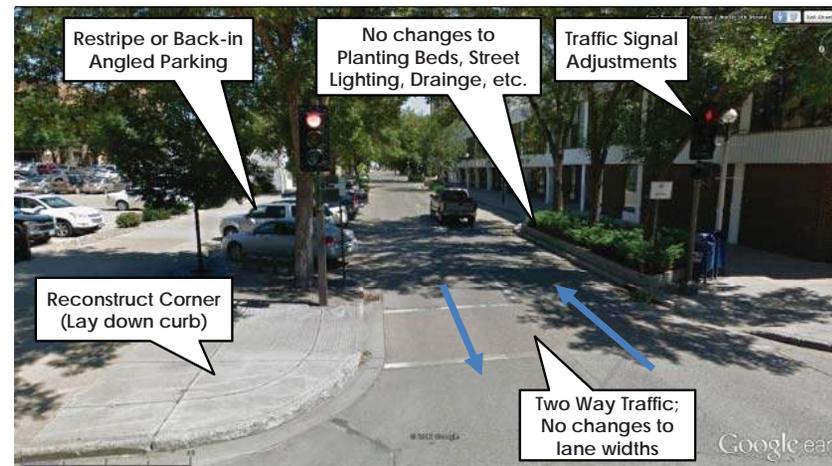
Additional long term changes should include:

- Reconstructing curbs, gutters, and storm drains (as needed)
- Adjusting, adding or removing street lights
- Reconstructing and replanting landscape beds

EXISTING CHANCELLOR SQUARE



TWO-WAY ELEMENTS





**IMPLEMENTATION
STRATEGY**

OVERVIEW

Subarea Plan implementation requires the identification of public actions that will produce a sustained and widespread private market reaction. Revitalization of Downtown Bismarck will require the expenditure of City financial and staff resources to ‘prime the pump’ for private development.

A ‘menu’ of numerous projects is identified in the Subarea Plan. Not all projects are equal. Some are time sensitive and need to commence immediately while others will require additional study and coordination. The Implementation Strategy identifies a concise list of game-changing and essential projects that best address the project objectives.

The implementation measures, primarily investments in physical infrastructure improvements (including associated planning, programming, design, engineering and construction) should be initiated primarily by the City of Bismarck and in some instances through coordination with the private development sector as public-private partnerships. Additionally, to ensure consistency with the Downtown Bismarck Subarea Plan proposals, regulating policies, plans, ordinances, guidelines and financing strategies will need to be either updated or created by the City and consultants and adopted by the Bismarck City Commission, MPO or other regulating bodies.

Where redevelopment is identified for existing, privately-held commercial or residential properties, it will primarily occur through private transactions between willing buyers and sellers. The adoption of the Subarea Plan does not force existing uses to change. Existing uses not consistent with the implementation strategy should be ‘grandfathered’- permitted to remain and operate their businesses ‘as-is’ in perpetuity until owners deem it is their best interests to re-purpose, redevelop or sell their buildings or sites.

It is recommended that the Bismarck City Commission not use eminent domain powers for the purpose of redevelopment of identified implementation projects.

GAME-CHANGERS

Establishing plan implementation momentum early is essential for the long term success of the Subarea Plan. Symbolically, initiation of these projects provides evidence to the general public, downtown stakeholders, and investors that the City is financially committed to the revitalization of downtown land use and transportation concepts of the Subarea Plan. Some of the projects are 'game-changing' in that they fundamentally change the investment environment of the downtown. Strategically, all four projects identified are essential to 'set the table' for later projects identified in the five year timeframe to proceed. Without the implementation of these projects, other essential projects will either fail to be implemented or fall short of their revitalization potential.

The game-changing projects are identified and brief descriptions of key actions are described on the following pages:

MAIN AVENUE



EXISTING CONDITIONS

DEPOT PLAZA



FIFTH STREET



CONVENTION HOTEL



GAME-CHANGERS



- 1 Main Avenue
- 2 Depot Plaza
- 3 Fifth Street
- 4 Convention Hotel Site

MAIN AVENUE TRAFFIC CALMING

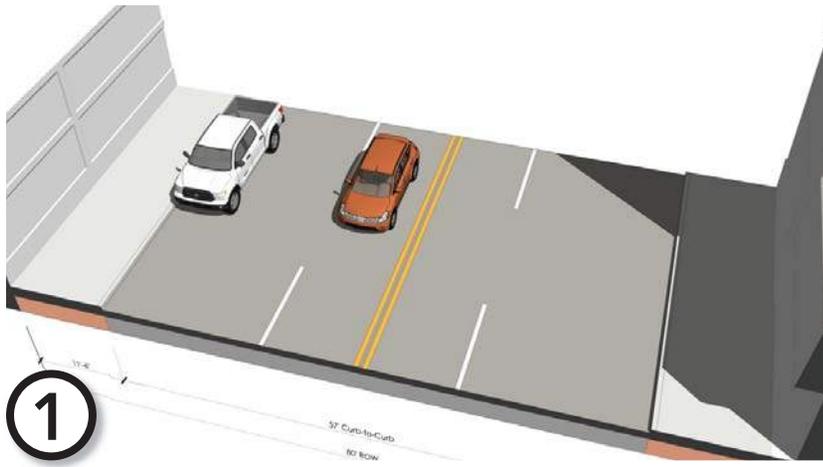
Main Avenue traffic calming from First (west) to Sixth (east) Streets is a 'game-changing' project that will re-establish a pedestrian-friendly environment and stimulate considerable in-fill development and redevelopment of existing ground floor storefronts.

For project implementation to occur, additional traffic analysis and design refinement will be needed immediately following plan adoption and completed within three years.

Traffic analysis should address and clarify potential improvements including:

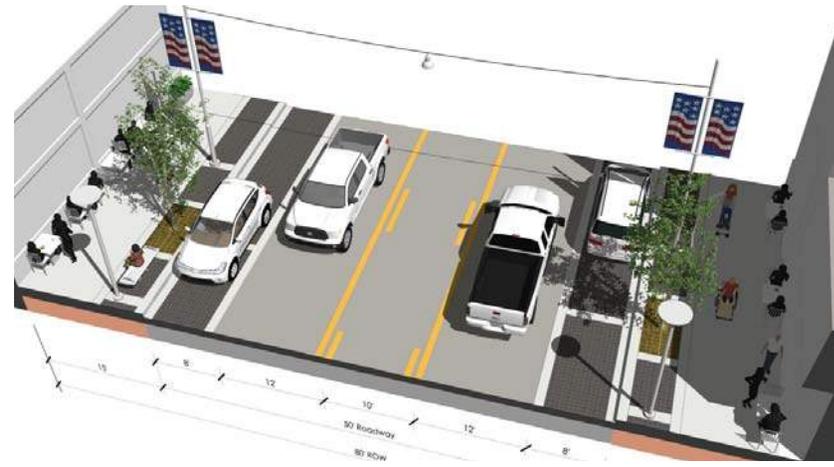
- Requirements for two-way traffic on Broadway from Fourth to Sixth
- Access modifications at key intersections, in particular at 4th and 6th Streets (left turn prohibition)
- Right turn lane requirements on Main Avenue (Third and Seventh)
- Modifications to or additional travel lanes on Third to improve northbound and southbound traffic

A detailed design concept and preliminary cost estimates for the street need to be developed soon after traffic analysis completion. Following these tasks, preparation of construction documents, refinement of cost estimating, bidding, selection of contractor and completion of construction should be finished within three years of plan adoption.



EXISTING CONDITIONS

- Four Lanes
- Parking Lanes Each Side
- 11' Sidewalks



PROPOSED PROJECT

- 'Road Diet' Three Lanes
- Widen Sidewalks
- Maintain Parking Lanes Each Side
- Enhanced Landscaping
- Corner 'Bulb-Out' Curb Extensions and Landscaping
- Improved Crosswalks

DEPOT PLAZA

Re-establishing a public assembly space at the historic depot is the most significant ‘game-changing’ action to revitalize the Downtown. The most notable benefits include:

- Creating a multi-purpose, dedicated space for civic activities
- Providing a focus for retail, housing, and employment development along Main Avenue

Establishing a new Depot Plaza, however, offers many challenges. Implementation will need to commence with both short-term and longer-term actions.

The proposed site currently serves as a Depot building parking lot (100 spaces) for tenants, customers and surrounding businesses. As part of the Subarea Plan’s parking strategy, a retail-serving parking ramp is proposed at Front Avenue and Fifth Street. It will provide adequate nearby replacement parking for the Depot Building. In the short-term, a two-phased strategy for replacement parking would include:

- Increasing the utilization of the Third Street Ramp for public parking use
- Restriping the current Bismarck-Burleigh Public Health Building parking lot to gain 26 additional spaces and a second phase that removes the building and brings in an additional 100 spaces

The proposed Depot Plaza implementation strategy suggests that the City acquire both the Depot parking lot and building and provide a reasonable timeframe for current tenants to have the opportunity to lease new tenant space. Once the plaza is constructed, the current tenants should be offered first-right-of-refusal to occupy their current tenant space.

Key immediate first-year actions for the Depot Plaza development by the City of Bismarck include:

- Securing an option for the future purchase of the Depot Plaza site
- Assisting, if necessary, with the relocation of current business to alternative tenant spaces
- Preparing concept design and preliminary cost estimates for the Depot Plaza and Depot Building

Within two to three years:

- Purchasing the Depot site
- Preparing plaza/building construction documents and finalize cost estimates
- Bidding, selecting a contractor, and constructing the plaza
- Offering existing tenants the opportunity to relocate to Depot



EXISTING CONDITIONS

Lot Size: 51,621 SF (1.19 AC)

Use: Commercial-Restaurant (Historic Depot)

On-site parking lot

Access driveway

Outdoor dining area



PROPOSED PROJECT

- Hardscape and landscape civic gathering space
- Historic Depot renovation
- Maintain access drives (close during events)
- Pedestrian Underpass stairways, ramps (Fifth Street Underpass essential project)

FIFTH STREET IMPROVEMENTS

Combined with improvements to Main Avenue, Fifth Street improvements are the ‘game-changing’, public right-of-way improvements within the study area. An enhanced streetscape armature will provide a safe and beautiful linkage between downtown destinations: the Kirkwood Mall, the Civic Center, and the Main Avenue retail establishments. It will also spur significant development not only along the street corridor but also nearby parcels within the new Entertainment District. In particular, Kirkwood Mall owners have identified Fifth Street improvements as a necessary project for future Mall expansion.

Key immediate first-year actions for the Fifth Street by the City of Bismarck include:

- Establishing a 10’ sidewalk easement and building setback along the westside of Fifth from the BNSF railway to the Kirkwood Mall
- Completing any necessary traffic analysis and planning required for reduction of roadway capacity from four to three lanes
- Preparing concept design and preliminary cost estimates for the entire street from the Public Library to Kirkwood Mall

By the second year:

- Preparing construction documents and finalizing cost estimates
- Bidding, selecting a contractor, and constructing the initial phase between Main Avenue and the Kirkwood Mall (minimum)

Within three years:

- Bidding, selecting a contractor, and constructing the remaining streetscape improvements north of Front Avenue



EXISTING CONDITIONS—TYPICAL BLOCK



PROPOSED PROJECT—TYPICAL BLOCK



EXISTING CONDITIONS—LOOKING SOUTH



PROPOSED PROJECT—LOOKING SOUTH

- 'Road Diet': Rosser to Ingals Avenue
- Streetscape improvements within current right-of-way plus 10' dedicated setback (west side of street) from BNSF railway to Kirkwood Mall
- Underground the overhead utility wires

CIVIC CENTER CONVENTION HOTEL

The site for a future Civic Center Convention Hotel is strategically located. Construction of a Civic Center Hotel is not only essential for maximizing the return on investment of the future expansion of the Civic Center facility, but also identified by Kirkwood Mall owners as a necessary trigger for their commitment to a Fifth Street-oriented Mall expansion identified in the Subarea Plan.

The Hotel has been sited to link the Civic Center to the Kirkwood Mall while not impacting the current parking supply of either of these uses. **To meet necessary urban design siting and building programmatic requirements for the hotel, a privately-owned parcel located north of the City's parcel will need to be acquired.**

Currently, hotel development interest exists for this site if it can be assembled. To expedite the development of this site, the City should enter into an exclusive public-private joint development agreement with a single developer rather than an open request-for-proposals (RFP) process for this 'game-changing' project.

Key immediate actions for the implementation of the Civic Center Hotel by the City of Bismarck include:

- Securing an option for the future purchase of the identified commercial parcel
- Assisting, if necessary, with the relocation of the current businesses to alternative downtown tenant spaces
- Purchasing of the commercial parcel
- Entering into an exclusive joint venture public-private development agreement with a hotel developer
- Establishing development requirements and schedule for development

Within two to three years:

- Designing, bidding, selecting a contractor, and initiating construction of the hotel



EXISTING CONDITIONS

- Privately-Owned Parcel:** 36,375 SF (0.83 AC)
- Use:** Commercial (Sports Bar)
On-site parking
- City-Owned Parcel:** 51,081 SF (1.17 AC)
- Use:** Commercial



PROPOSED PROJECT

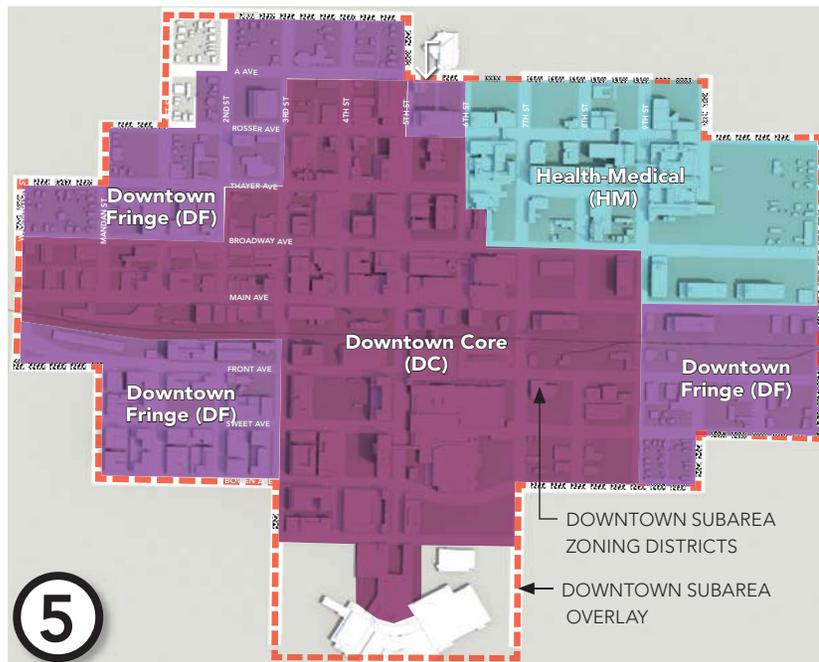
- Total Lot Size:** 87,456 SF (2 AC)
- Proposed Use:** Convention Hotel with parking lot (est. 100 spaces)
Ground-floor retail fronting Fifth Street

ESSENTIAL PROJECTS

The remaining 'menu' of implementation projects should be initiated and substantially completed within five years of plan adoption. The following list of projects does not imply priority or importance.

At the passage of the five-year timeframe, the implementation strategy should be assessed and updated. This will allow for all proposals or emerging projects since the completion of the Downtown Bismarck Subarea Plan to be accounted for. Additionally, consideration should be given to updating the Plan.

DOWNTOWN SUBAREA OVERLAY

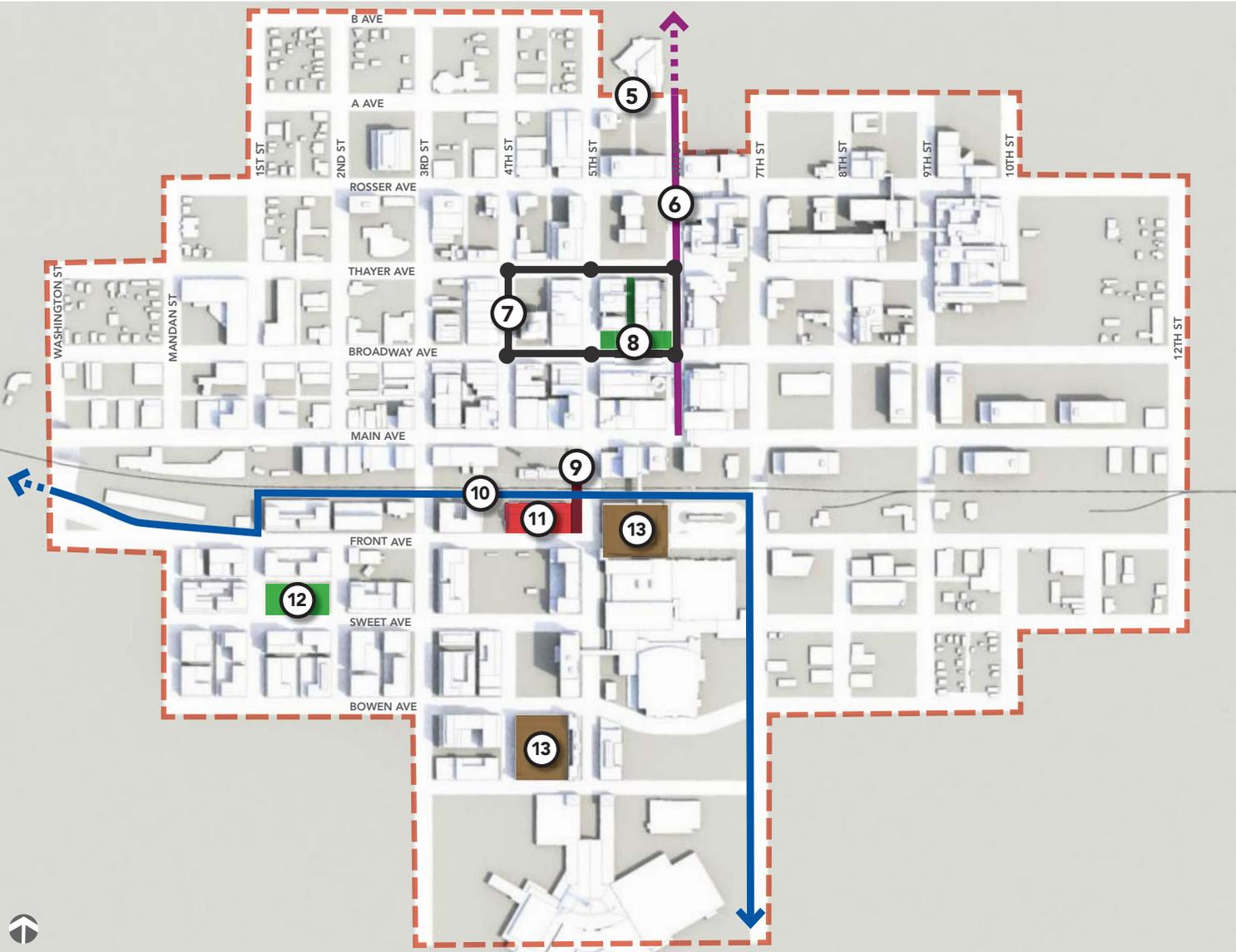


Establishing the regulatory framework in the form of codes, standards and discretionary guidelines for design review are time sensitive measures to be instituted to ensure future development meets the intent of the Subarea Plan, encourages quality development and provides certainty to private developers and the community. The necessary regulatory and design review process updates include:

- Creating a Downtown Subarea Overlay District and mapping of overlay standards to include retail opportunity areas, build-to-lines, actives edges, building heights and areas of no minimum residential lot size requirement
- Consolidating the Subarea into three zoning districts by expanding the Downtown Core, Downtown Fringe and Health Medical Zone Districts
- Amending the permitted uses for each zoning district to allow for the predominate mix of uses identified in the Urban Design Framework of the Subarea Plan with an emphasis on accommodating and encouraging mixed-use and residential development
- Creating Design Guidelines, a Design Review Committee, and a Design Review Process as an essential tool for review of Subarea buildings and public areas.
- Codifying Street Standards and details for the Subarea to ensure unified and consistent development of streets, sidewalks, intersections and public spaces.

ESSENTIAL PROJECTS

- 5 Downtown Subarea Overlay
- 6 Sixth Street Cultural Trail
- 7 Mehus Commons
- 8 Chancellor Square
- 9 Fifth Street Underpass
- 10 Rail Trail
- 11 Public Market
- 12 Neighborhood Park
- 13 Bowen Avenue Ramp



SIXTH STREET CULTURAL TRAIL



The enhanced bikeway and pedestrian trail project implementation will require additional study and design between the northern study area boundary (A Ave) and the North Dakota State Capitol grounds.

- The design concept will require further traffic engineering.
- Integration of the trail into the Chancellor Square two-way project is required. As part of the two-way conversion, any temporary angled parking should be striped for eventual conversion into a protected bikeway, sidewalks and landscaped areas. Permanent structures such as planting beds, street lighting, traffic signalization and storm drain catchment basins should be designed to not preclude future Sixth Street Cultural Trail improvements.
- Implementation steps include preparation of construction documents, refinements of cost estimates, bid, selection of contractor and construction.

MEHUS COMMONS



The public open space and redevelopment site project implementation will require additional design refinement and site acquisition.

- Negotiations for an option to purchase the Kokkeler Jewelry site or to acquire first right-of-refusal to purchase the Kokkeler Jewelry site should commence no later than three years after plan adoption with the property owners or their representatives. Consideration of relocation of the Kokkeler Jewelry into new ground floor storefront of a new mixed use building along the Commons edge should be offered along with other relocation options.
- Negotiations for an option to purchase the southerly portion or the entirety of the Sanford parking lot site should commence no later than three years after plan adoption with the property owners or their representatives.
- Construction documents and cost estimates will need to be created, bid and a contractor will need to be selected within five years.

CHANCELLOR SQUARE



Re-establishing the historic two-way traffic pattern on Broadway, Sixth, Thayer and Fourth will have multiple benefits, including:

- Improved emergency vehicle access to downtown hospitals
- Improved access to existing parking lots, the future Sixth Street Ramp and other existing parking ramps
- Increased drive-by traffic and improved storefront visibility of retail establishments, especially those on Broadway Avenue

However, the greatest mobility benefit will be that it will provide a parallel alternative Main Avenue route for through traffic along Broadway Avenue. In turn, a two-way Broadway Avenue will benefit businesses with increased auto drive-by visibility, and improved access. Providing a Broadway Avenue alternative route will enable Main Avenue to operate acceptably over the next twenty year horizon.

The implementation of Chancellor Square can be phased: Broadway Avenue can be converted to two-way traffic while Thayer, Fourth and Sixth remain as one-way streets.

FIFTH STREET PEDESTRIAN UNDERPASS



Pedestrian access improvements between Depot Plaza and destinations along Fifth Street are necessary to ‘knit the rail seam’ and link Main Avenue to Fifth Street. The improvements will need to be approved and coordinated with the BNSF railway. The railway through downtown is of national importance and BNSF is likely to be deliberate in their consideration of an underpass that may impact trackways or rail rights-of-ways. As a result, the implementation timeline of an underpass may, as a parallel effort with the Depot Plaza design, be protracted. To implement the underpass, the following should occur:

- Development of a refined design concept, construction strategy and conceptual cost estimates
- Preparation of any necessary traffic analysis
- Coordination and discussion of concepts and construction with BNSF representatives
- Assembling of key parcels adjacent to the railway for plan elements such as stairs or ramps along with any parcels that might be required for a temporary trackway shoefly, staging area, etc. to construct the project within three years
- Preparation of construction documents, refinement of cost estimates, bid, selection of contractor and construction within five years

RAIL TRAIL



Bikeway and pedestrian access improvements along City-owned and privately-owned parcels will need further design refinement and coordination with other plan projects. Within three years of plan adoption the following should occur:

- Preparation of a refined design concept that is coordinated with/phased so as not to impact parallel design of the Fifth Street Underpass, the Front and Fifth parking ramp, Civic Center, Kirkwood Mall or other improvements
- Identification of easements for trail rights-of-way need to be acquired
- Preparation of construction documents, refinement of cost estimates, bid, selection of contractor and initiation of construction within five years of plan adoption

PUBLIC MARKET



The site is strategically located at the hub of the Main-Fifth retail framework. The subarea land use framework suggests demolition or adaptive re-use of existing structures for a year-round indoor public market retail use at this site. However, if a public market is not feasible, the site should be reserved for a single use retail 'anchor' tenant, a mixed-use residential, or commercial multi-story building with ground floor retail uses. The City should prepare requests for proposals (RFP) for the development of the site within five years. Implementation of the Fifth Street pedestrian underpass will require right-of-way within an area that is currently occupied by the existing commercial structure. Redevelopment of the site should reserve a portion of the site for this use. Currently the parcel includes a parking lot and commercial building occupied by the Parks and Recreation Department and Bismarck Art & Galleries Association. Within three years of plan adoption, the City should:

- Prepare a public market feasibility study to determine the requirements, actions and financing required for its development
- Prepare a strategy for relocating existing tenants to other commercial office space within the downtown.
- Prepare public market construction documents, refine cost estimates, bid, select a contractor and initiate construction within five years

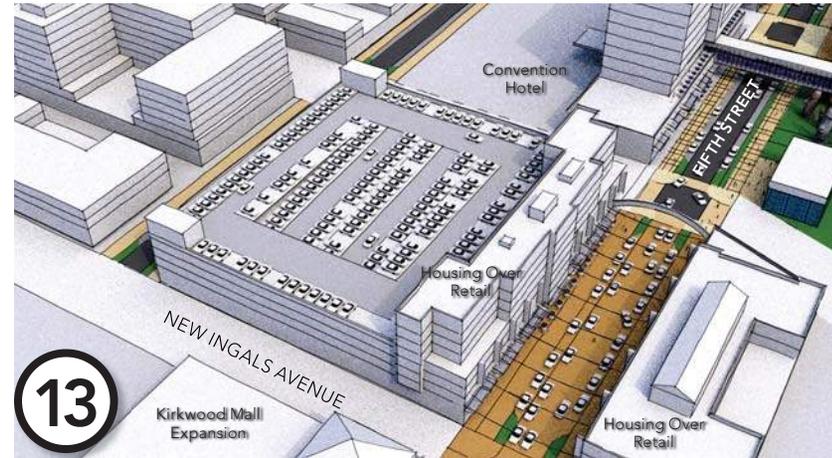
NEIGHBORHOOD PARK



Providing a public park as a focus for high-density residential development is an essential amenity to spur private sector redevelopment of current low-density, multi-family parcels. Within four years of plan adoption the City should:

- Enter into negotiations to assemble and acquire the half block bounded by First Street , Sweet Avenue, and Second Street
- Develop a relocation strategy for the current tenants of the site
- Develop a conceptual park concept and cost estimates
- Finalize park design and prepare construction documents, refine cost estimates, bid, select contractor and initiate construction within five years of plan adoption

BOWEN AVENUE RAMP



The ramp is strategically located to serve the Convention Hotel, Civic Center and is essential to meet Kirkwood Mall expansion parking needs. The ramp site will occupy the current City parking lot at the southwest corner of Fifth and Bowen. Consideration should be given for inclusion of a portion of the site for residential apartments over ground-floor retail constructed either adjacent to the ramp or integrated as a 'liner' building to screen the parking ramp along Fifth Street. Following plan adoption, the city should:

- Prepare a conceptual ramp design and development study that establishes potential parking requirements, impacts (utilities, etc.), costs, and potential financing strategies within two years
- Explore joint development of the ramp and residential mixed-use retail buildings fronting Fifth Street with private interests, including the Kirkwood Mall owners
- Finalize ramp design and prepare construction documents, refine cost estimates, bid, select a contractor and construct within five years to accommodate Mall expansion

RESPONSIBILITIES

All of the projects identified will require coordination and leadership by the City of Bismarck. In some instances public-private partnerships will need to be established. It is recommended that a single point-of-contact staff position be created to coordinate and manage these projects through implementation. As an oversight body, it is recommended that willing members of the Downtown Subarea Steering Committee, Technical Advisory Committee and representative downtown interests be transformed into an 'Implementation Oversight Committee'. This committee would review proposals and provide recommendations to the Bismarck City Commission for any expenditure of public resources throughout the life of the implementation plan.

The Implementation Oversight Committee should:

- Be formally appointed by the City Board of Commissioners
- Receive support from City staff regarding necessary data and information needs, scheduling of regular meetings and preparation of an annual report and briefing materials
- Meet at regular intervals throughout the life of the Subarea Plan implementation to review and provide recommendations to the City Commission on implementation progress
- Prepare an annual report and briefing to City Commissioners

The following is an outline of the three fundamental elements of an Implementation Oversight Committee.

Committee Members

The Implementation Oversight Committee should include a diverse and representative group of downtown members including property and business owners, representatives of downtown business associations and advocacy groups, and City departments, agencies, and commissions. Oversight Committee members might be representatives of:

- Downtown Business Improvement District
- Renaissance Zone Authority
- Downtown Business Association
- City Transportation, Planning and Public Works

- Housing Authority and Parking Authority
- Transit Agency
- City Commission and Planning and Zoning Commission
- Downtown Owner
- Property Owner

Committee Purpose

The Implementation Oversight Committee should meet regularly to review implementation progress, and identify opportunities for advancing implementation efforts. Tasks of the Oversight Committee should include:

- Meeting Monthly
- Keeping the Master Plan 'Alive and Breathing'
- Tackling topics and components of the Master Plan, setting goals, finding community members to engage in goals, advocating for goals, and creating committees to achieve goals
- Seeing this plan through
- Recognizing that City support is essential for plan success
- Recognizing that accomplishments of the Master Plan are the result of a community-wide effort

Committee Annual Report

The Implementation Oversight Committee should prepare an annual report. An annual report and briefing to City Commission identifies implementation progress and provides a basis for establishing partnerships and prioritization of City funds for implementation projects. The annual report should include:

- An update on accomplishments for the last year
- An updated list of Master Plan accomplishments since the start of the plan
- The committee's immediate goals for the near future

SCHEDULE

The schedule calls for action on all projects within the first five years to ensure that Plan momentum is established.

PROJECTS	TIMELINE				
	YEAR 1	YEAR 2	YEAR 3	YEAR 4	YEAR 5
① Main Avenue Traffic Calming	Start	Construction Documents	Construct		
② Depot Plaza	Start	Construction Documents	Construct		
③ Fifth Street Improvements	Start	Construction Documents	Construct		
④ Civic Center Convention Hotel	Assemble Site	Construction Documents	Construct		
⑤ Codes and Standards Design Guidelines Street Standards	Amend Code				
	Prepare Guidelines				
	Prepare Street Standards				
⑥ Sixth Street Cultural Trail			Start	Construction Documents	Construct
⑦ Mehus Commons			Start	Construction Documents	Construct
⑧ Two-Way Traffic Conversion			Start	Construction Documents	Construct
⑨ Fifth Street Pedestrian Underpass	Start			Construction Documents	Construct
⑩ Rail Trail				Start	Construction Documents
⑪ Public Market				Start	Construction Documents
⑫ Neighborhood Park				Start	Construction Documents
⑬ Bowen Avenue Ramp		Start	Mall Expansion		

INVESTMENT — GAME-CHANGERS

The implementation strategy identifies only four game-changing projects. These are the essential public actions required to stimulate significant private investment within the Downtown.

Cumulatively, the return on investment ratio directly related to these projects is likely to be significant. The Investments–Benefits tables and diagram below illustrates the amount of public investment (\$22,500,000) that if spent, is anticipated to stimulate substantial private investment (\$317,900,000).



INVESTMENTS–BENEFITS DIAGRAM

PUBLIC INVESTMENT SUMMARY	
Project Name	Projected Cost
Main Avenue Traffic Calming	\$3,500,000
Depot Plaza	\$8,500,000
Fifth Street Improvements	\$10,500,000
Civic Center Convention Hotel	\$0

PUBLIC INVESTMENT TOTAL: \$22,500,000

PRIVATE INVESTMENT SUMMARY		
Project Name		Projected Cost
Housing (Unit)*	430	\$108,450,000
Hotel (Rooms)*	250	\$42,500,000
Office (SF)*	363,000	\$94,650,000
Retail (SF)*	289,000	\$72,300,000

PRIVATE INVESTMENT TOTAL: \$317,900,000

* Includes on-site parking

GAME-CHANGER
INVESTMENT-
BENEFITS



■ New Development

- 1 Main Avenue
- 2 Depot Plaza
- 3 Fifth Street
- 4 Convention Hotel Site

INVESTMENT — BUILD-OUT

At build-out, total private investment indicated is estimated to be over \$964 million dollars.

PRIVATE INVESTMENT SUMMARY		
Project Name		Projected Cost
Housing (Unit) *	1,830	\$445,950,000
Hotel (Rooms)	250	\$42,500,000
Office (SF)	1,350,000	\$392,050,000
Retail/Commercial (SF)*	364,000	\$84,300,000
PRIVATE INVESTMENT TOTAL:		\$964,800,000

* Includes on-site parking

POTENTIAL BUILD-OUT



■ New Development

REGULATORY UPDATES

The Downtown Subarea Plan’s urban design and complete streets frameworks should be utilized as a basis for updating policies, regulations and standards. The City of Bismarck and Bismarck-Mandan MPO will be responsible for carrying out relevant changes to policy plans, zoning regulations/standards and street standards for the downtown subarea.

POLICY PLAN UPDATES

This Downtown Bismarck Subarea Plan adopted by the City Commission and Metropolitan Planning Organization (MPO) is a supporting element to the Long Range Transportation Plan (LRTP), and the Growth Policy Plan. The adopted Plan replaces the Central Business District Plan (1993) as the guiding policy framework and implementation plan for Downtown Bismarck. The Downtown Subarea Plan’s urban design and complete streets frameworks will be utilized to inform the current in-process update to the Long Range Transportation and Growth Management Plans to ensure policies support downtown revitalization as envisioned in this document.

ZONING ORDINANCE UPDATES

The intent of the zoning ordinance updates is to identify essential permitted land use types, and site design/improvement standards within a Downtown Subarea Overlay boundary. Unnecessary permitted uses and inappropriate site design and development standards should be modified or eliminated. Throughout the Downtown Subarea Overlay, existing permitted conforming uses should be ‘grandfathered’ to remain, be improved upon, or sold and operated ‘as-is’ until renovated as property owners deem necessary.

To align current code requirements with the Subarea Plan, two approaches to address inconsistencies might be to:

- Maintain current District zoning and provide additional or revised permitted use tables. Additionally, provide new or revised site design and development standards to be applied within the boundary of the Downtown Subarea Overlay
- Consolidate the Downtown Subarea Overlay into three Zoning Districts by expanding the Downtown Core, Downtown Fringe and Health-Medical Zoning Districts. For each zone, rewrite the permitted use tables, and create new or revised dimensional and design standards consistent with the urban design framework of the Downtown Bismarck Subarea Plan.

Additionally, all development projects within the Downtown Subarea Overlay should be subject to the design review and approval set forth in a new Downtown Design Guidelines document.

Specific Permitted Use Updates

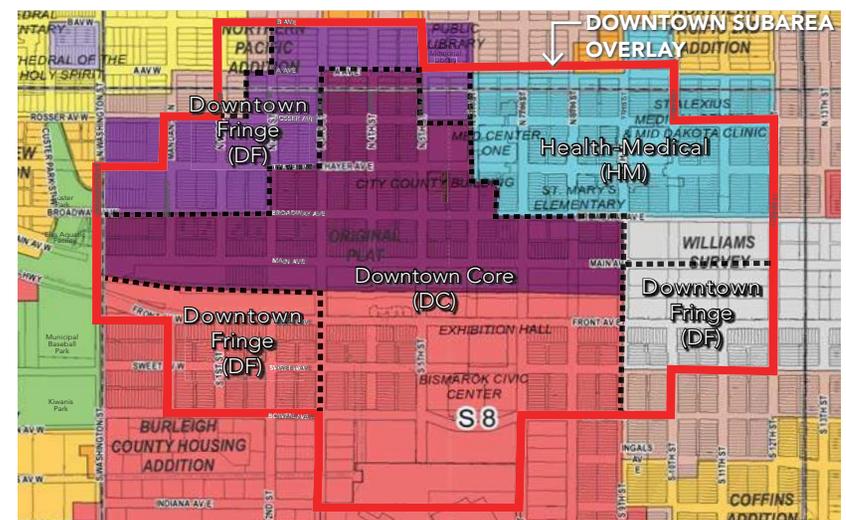
Permitted uses within the Downtown Subarea Overlay should be modified as needed to support the uses identified in the urban design framework of this Plan.

The following are essential zoning ordinance permitted-use updates that must be addressed to effectively support the fundamental concept of the Subarea Plan which is to make:

- Main Avenue and Broadway Avenue pedestrian and retail friendly
- Fifth Street a shopping, entertainment, and dining destination

Modify existing permitted-use tables— prohibit, provide restrictions, or set forth conditions-of-use upon auto-oriented uses (drive-through windows of banks, restaurants, and other similar businesses) and allow for and encourage retail development along Main Avenue, Broadway Avenue and Fifth Street and higher density residential development (no maximum dwelling unit density requirements) within existing and new development.

RECOMMENDED SUBAREA OVERLAY AND ZONING DISTRICTS



Retail Opportunity Areas—defined exclusively as the sale of goods, restaurants, and entertainment uses, encourage the street frontages along Main Avenue, Broadway Avenue and Fifth Street to include ground-floor retail and limit or exclude non-retail uses that are not complimentary to shopping, dining and entertainment. Exceptions for residential or office lobbies should be allowed/ reviewed as a conditional use. Prohibition of service bays, entrances to parking lots or service and loading bays should also be enacted.

Dimensional and Design Standards Updates

To ensure that the Main Avenue, Broadway Avenue and Fifth Street storefronts are animated, additional ‘form-based’ building site design and façade design standards should be enacted.

Require ‘active edges’—block frontages along Main Avenue and Fifth Street should also include a provision for active edges on the ground floor of buildings. Active edges are building frontages with direct sidewalk entries and a high degree of transparency. Active edges must provide a minimum of 70 percent transparent glass along ground-floor retail facades; frosted, tinted, reflective or other types of glass that diminish transparency should be prohibited and primary ground-floor uses must be oriented to the public right-of-way.

The following active-edge criteria should be met for all other ground-floor uses throughout the Downtown Subarea:

- Primary entrances must be oriented toward the street
- Windows should be provided along facades with a minimum percentage of 50 percent transparency required

Requirement of ‘Build-to-Lines’— block frontages within the retail opportunity area should also include a provision for Build-to-Lines. This requirement establishes continuous ‘street wall’ that frame retail streets and the plaza. Exceptions to these ‘zero-setback’ requirements might include— windows and walls recessed up to 18” from the property line to accommodate columns or other architectural elements. Conditional approval of deeper setbacks should be reviewed for:

- Dining and entertainment uses that intend to use the setback area for outdoor seating
- Interruptions for accommodation of passageways, building lobbies, or private courtyard entrances

DOWNTOWN SUBAREA DESIGN GUIDELINES

The role of Downtown Bismarck Design Guidelines is to aid designers and developers in understanding the City’s urban design expectations by providing a framework for an orderly discretionary review process that would supplement and strengthen downtown regulatory codes and design standards. The guidelines would ensure a degree of order, harmony and quality within the built environment; they would foster the development of buildings and projects that are attractive individually yet contribute to a downtown that is unified and distinctive as a whole.

The City should fund the creation of Downtown Design Guidelines, that apply to the Downtown Subarea Overlay, to ensure high quality projects and development certainty for private investors and developers. A Design Review Commission should be created to review projects and a design review process should be based on the extent of the work proposed. Typically, more extensive (major) projects should be reviewed by a design review committee, while less extensive (minor) projects might be reviewed by planning staff. The Design Review Commission should be appointed by the City Commission and comprised of architects, landscape architects, engineers, real estate developers and citizens at large. The Design Review Commission would provide recommendations to the Planning and Zoning Commission for projects, using the Design Guidelines as an exclusive evaluation tool.

DOWNTOWN SUBAREA STREET DESIGN STANDARDS

Updating the Downtown Street Design Standards will ensure that a complete-streets approach to circulation design is built as envisioned in the Downtown Bismarck Subarea Plan. Additionally, the Standards would ensure that a unified, consistently applied design of streetscape improvements are constructed throughout the Downtown Subarea. Downtown Street Standards will need to be developed by the City for streets within the Downtown Subarea. These would replace the Streetscape Design Guidelines for Downtown Bismarck (1995). Recommended street types and an outline of necessary standards are included in a separate Downtown Bismarck Subarea Plan Appendix.

FUNDING

Financing of all projects will likely come from a variety of sources. Once the Bismarck City Commission arrives at an agreement on the implementation projects, the responsibilities, schedules, and specific financing strategies should be finalized.

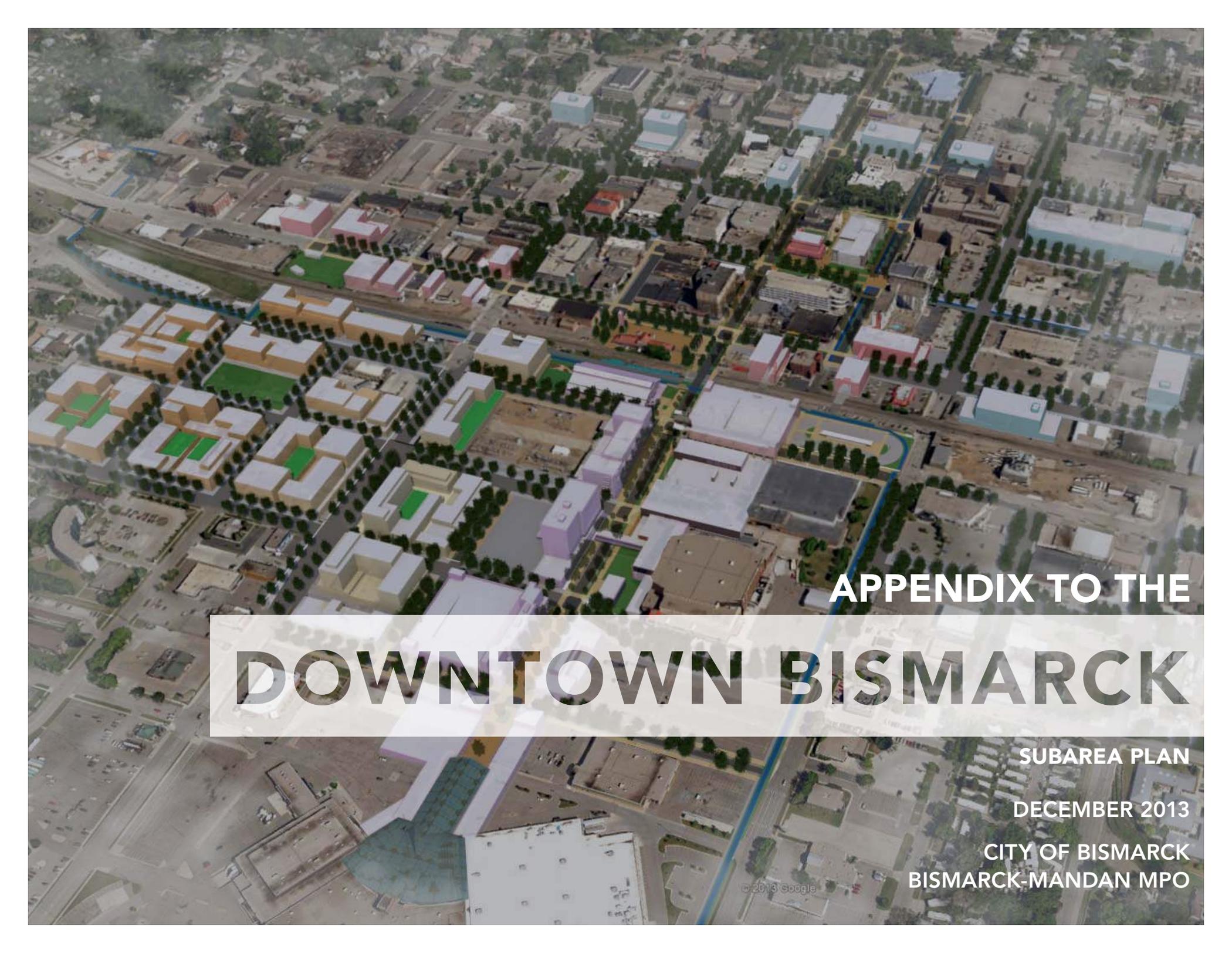
Current City sources, such as General Fund, Capital Improvement Plan, Tax-Incremental Financing (TIF), and Renaissance Zone funding should be explored. Downtown district boundaries for these sources of funding may need to be adjusted and specific TIF projects (if appropriate) identified.

The City might consider submitting applications for TIGER (MAP 21) grants or other federal funding sources for public infrastructure projects such as Fifth Street Improvements, Main Avenue traffic calming, and the Fifth Street pedestrian underpass.

CURRENT FUNDING BOUNDARIES



- Renaissance Zone
- Urban Renewal Plan Area
- New Development



APPENDIX TO THE
DOWNTOWN BISMARCK

SUBAREA PLAN

DECEMBER 2013

**CITY OF BISMARCK
BISMARCK-MANDAN MPO**

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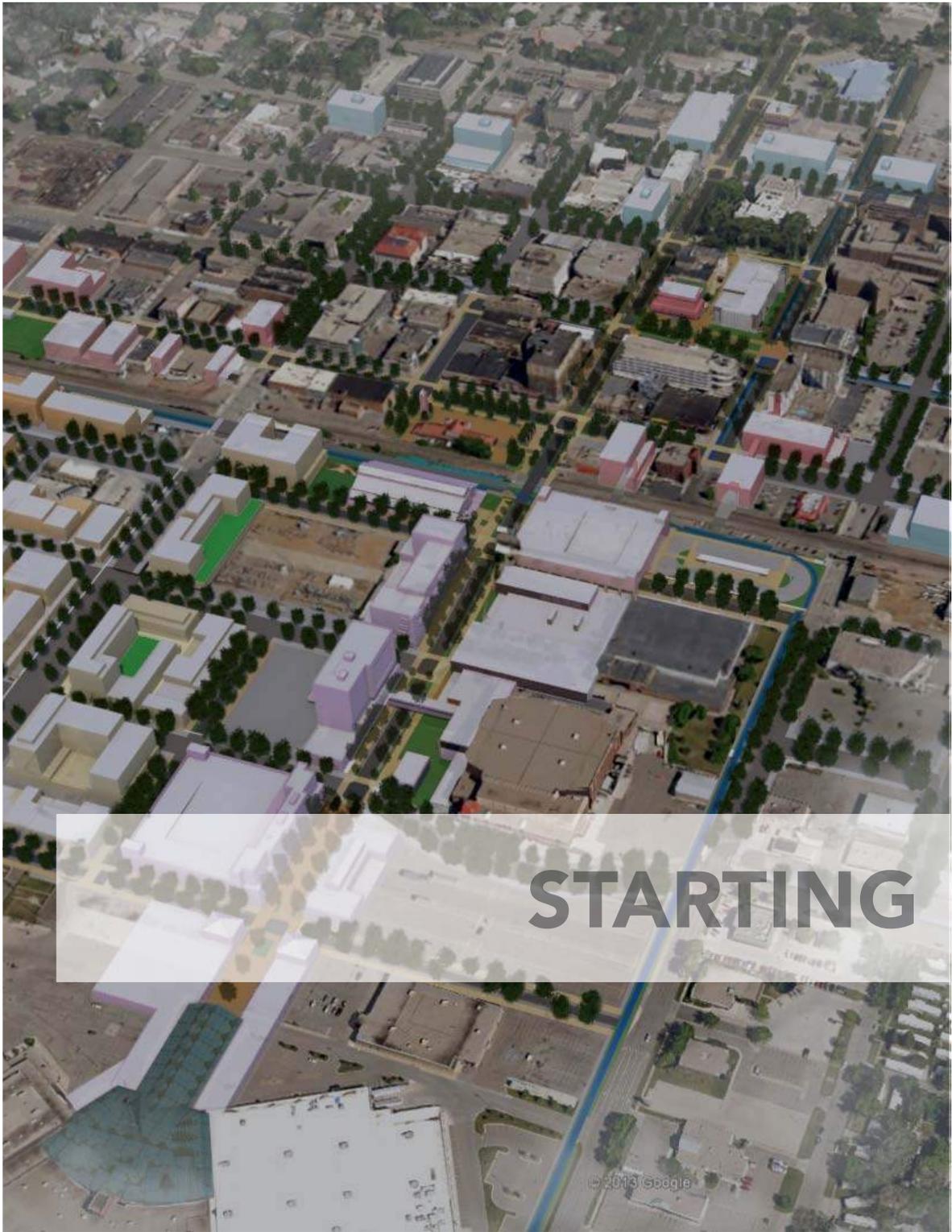
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EXISTING CONDITIONS SUMMARY MEMO

Date: March 05, 2012

To: Steve Saunders, Transportation Planner
Bismarck-Mandan MPO

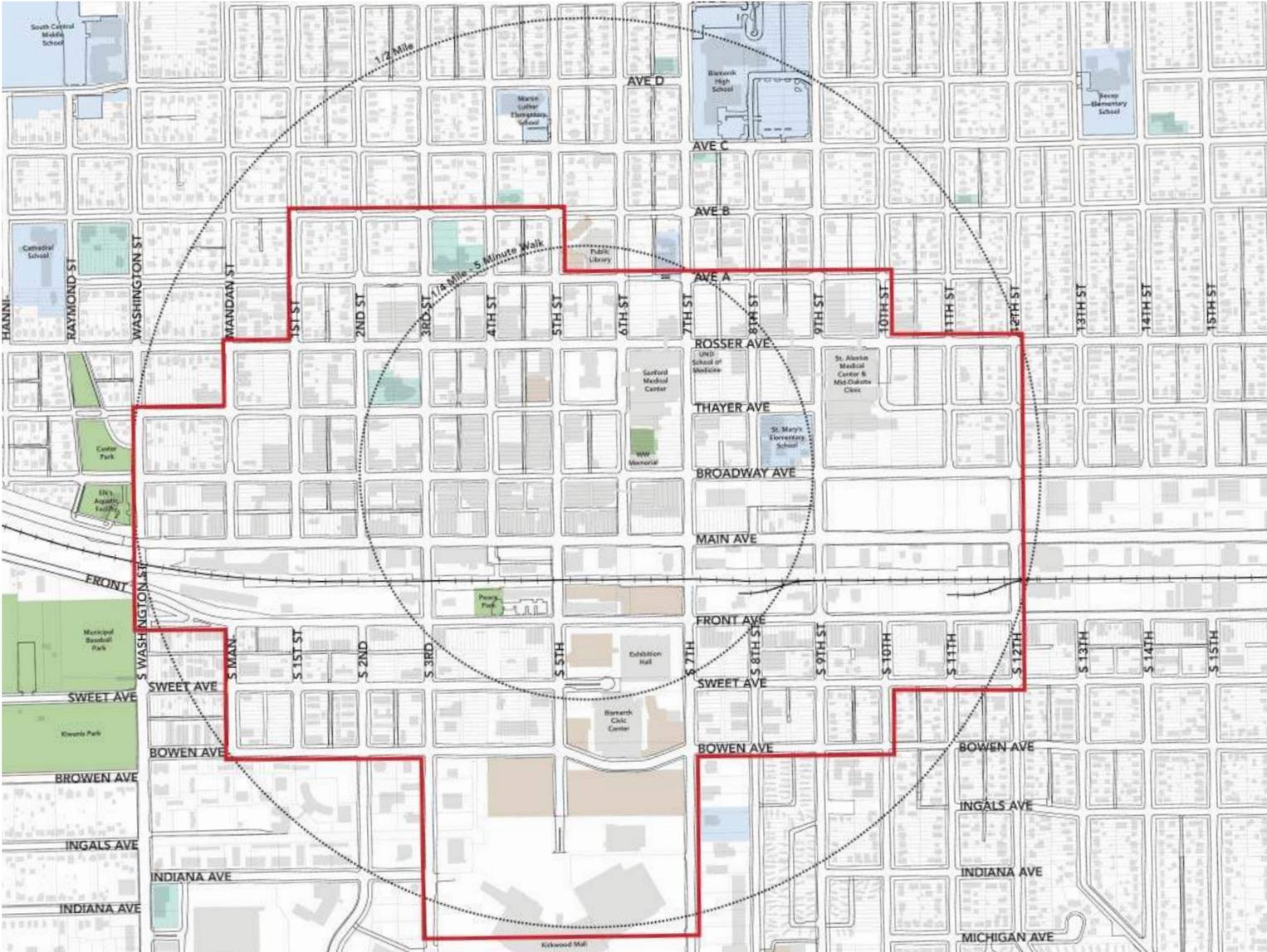
From: Jason Graf, Associate Principal
Crandall Arambula

Project: Downtown Bismarck Subarea Study—Task 1.4, Existing Conditions Summary Memorandum

This memo documents existing physical conditions, plans, policies and regulatory documents relevant to the project study area. The memorandum addresses the following:

- **Complete Streets-** evaluate existing conditions of area streets, and identify planned and proposed motor vehicle/active transportation projects
- **Traffic-** based on existing and historical traffic data within the study area establish the base line level of operation from a planning-level roadway capacity perspective
- **Parking-** determine existing parking demand with an emphasis on the needs of retail uses; identify surface parking lots where conversion to structured parking is feasible
- **Transit Center-** review transit center recommendations of the Mobility 2017 Plan
- **Urban Design-** review and analyze existing study area land uses and identify ‘soft parcels’ where redevelopment, intensification or improvements may be viable
- **Plans, Policies and Regulatory Documents-** review and analyze existing regulatory codes, relevant plans, standards, guidelines and development approval processes
- **Financing-** review current infrastructure and development financing tools
- **Demographics-** review existing area demographics and future growth trends

Study Area



COMPLETE STREETS

Documents Reviewed include:

- Downtown Bismarck Parking Study (2008)
- Long Range Transportation Plan-LRTP 2010-2035 (2010)
- Mobility 2017 Plan (2011)
- NDDOT 2012 Average Daily Traffic Counts

Relevance to the Subarea Study:

The following background data is being reviewed within the lens of a complete street. A complete street approach to circulation design is in contrast with a typical engineered approach that maximizes roadway capacity and speed for auto and trucks while often providing inadequate pedestrian and bicycle facilities. At a minimum, a complete street provides:

- Sidewalks for pedestrians
- Bike lanes for cyclists
- Travel lanes for drivers
- Travel lanes and stops for bus transit

Auto

Relevance to the Subarea Study:

Downtown's role as a destination for the community necessitates that the pedestrian remain a high priority in street design. Three elements of downtown streets design impact the ability to comfortably walk or bike and include:

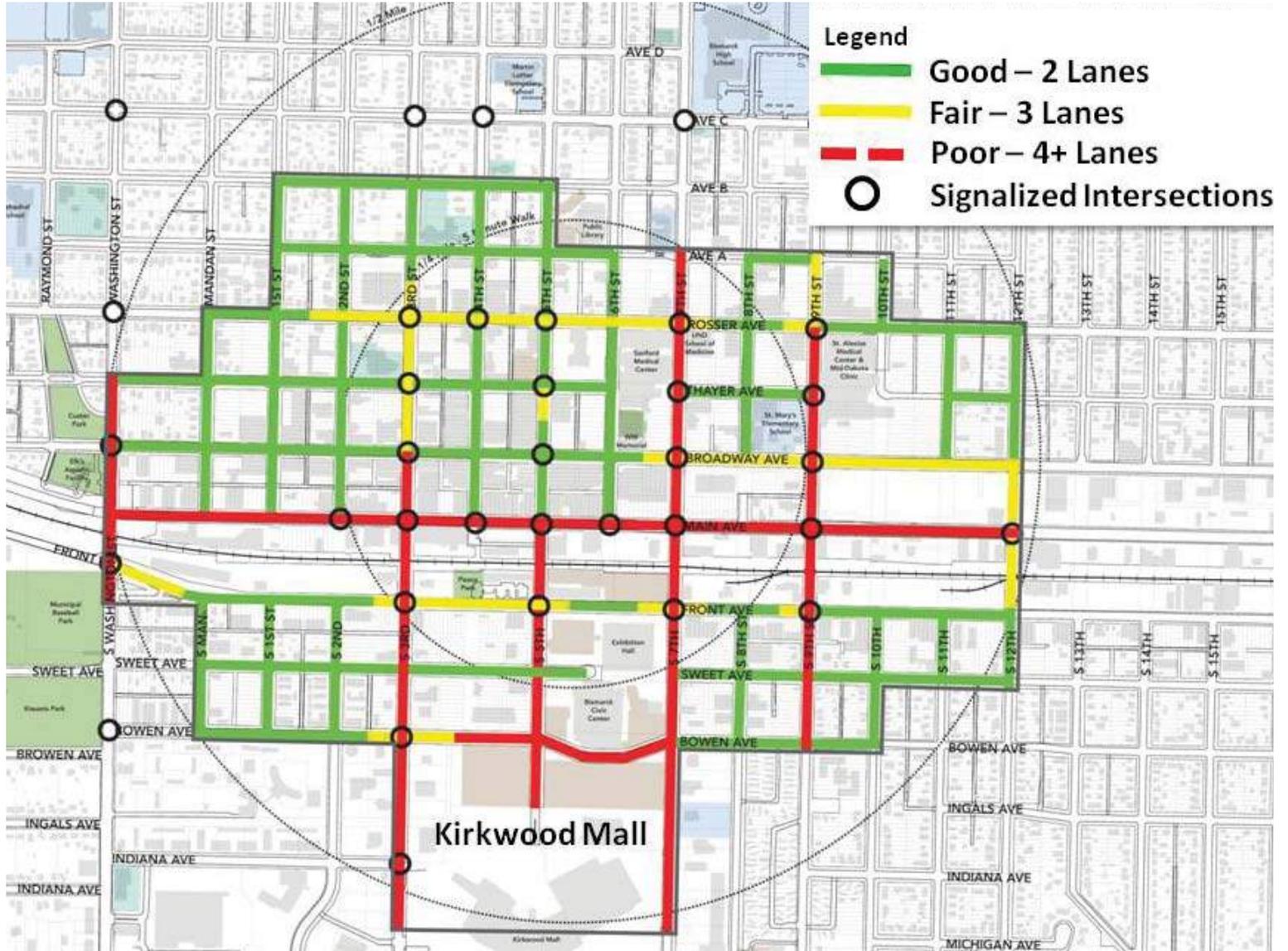
- *Number of travel lanes and roadway width*- the number of travel lanes and their width impacts pedestrian crossings, traffic speeds (tend to be higher) and the intimacy of the street.
- *Location of traffic signals*- signals regulate traffic flow and allow for safe crossings for pedestrians.
- *Traffic speed*- speeds above 25 mph are more dangerous for pedestrians and cyclists.

Analysis

An analysis of the number of lanes on downtown streets and location of signals were identified and rated based on their impact on the pedestrian environment. In general those streets with fewer lanes provide a shorter crossing distance for pedestrians and signalized intersections ensure safe crossing.

- No downtown planned road widening is identified in the Transportation Improvement Plan for 2013-2016
- Adequate signals are located to ensure safe pedestrian crossings
- Several roadways are 4 lanes wide and increase pedestrian crossing distances beyond a comfortable distance, and detract from the intimacy of the street to enhance and support the adjacent built environment
- For all streets there is very little use of curb extensions which is necessary to reduce pedestrian crossing distance (especially on wider streets with on-street parking), provide greater pedestrian visibility to oncoming traffic and improve pedestrian safety at intersections

Auto- Travel Lanes



Pedestrian

Relevance to the Subarea Study:

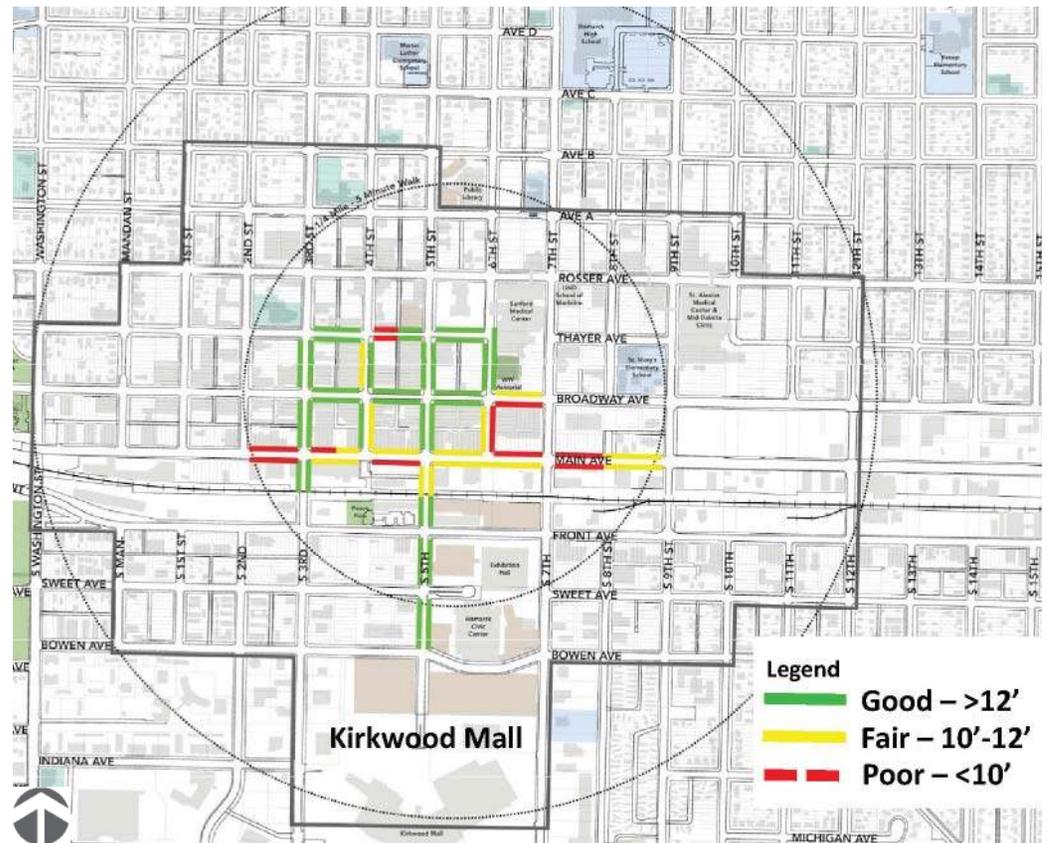
A walkable pedestrian friendly environment is what distinguishes downtown from other areas in the community. The widths of sidewalks play a critical role in the comfort level for pedestrians.

Streets with the highest number of people walking, typically those in relation to downtown shopping and entertainment areas, require adequate sidewalk width. A healthy retail environment requires a minimum of 12' sidewalks with 15' preferred.

Analysis

Sidewalks around existing concentrations of employment, retail and entertainment uses were evaluated.

- The area of Chancellor Square (4th to 6th—between Broadway and Thayer) provides a good example of adequate sidewalks along with 3rd Street, 5th Street, Thayer Avenue and Broadway Avenue
- Main Avenue which serves as the major entry into downtown is in need of sidewalk improvements and has substandard widths less than 12'



Sidewalks

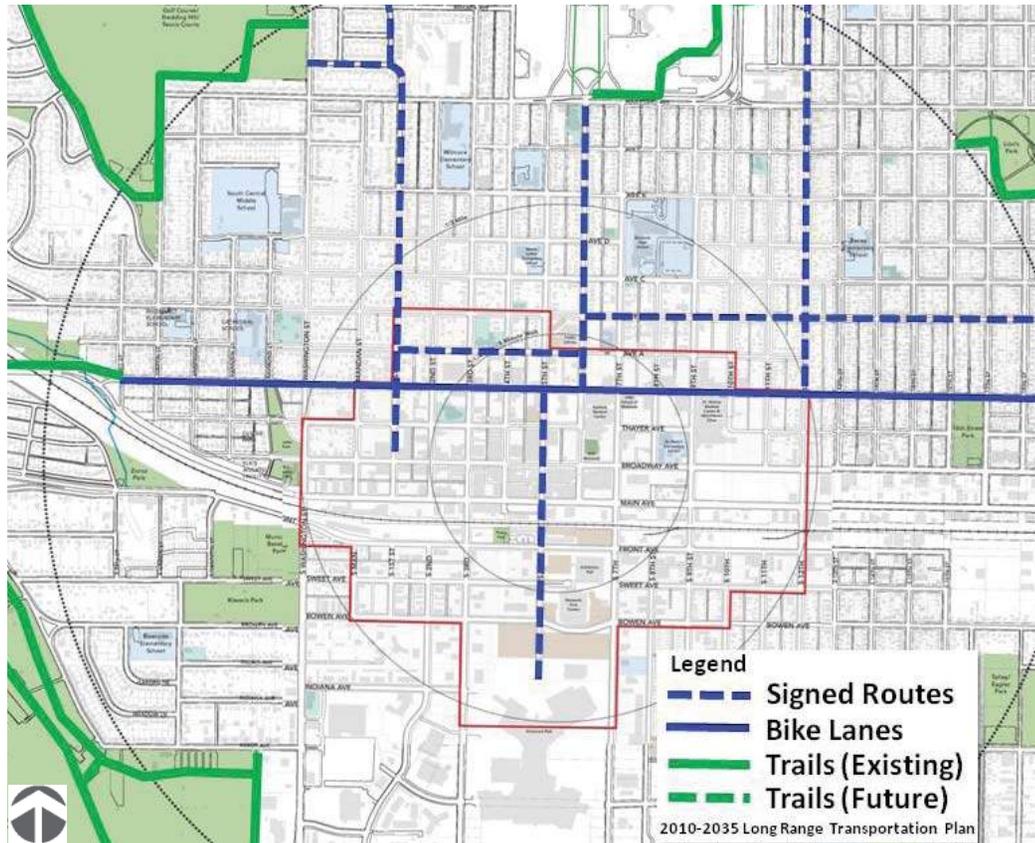
Bicycle

Relevance to the Subarea Study:

The Bismarck bicycle network consists of an extensive network of off-street trail loops associated with parks/open spaces and the Missouri Riverfront and limited on-street bicycle facilities that include bike lanes and signed routes as indicated.

Analysis

- No facilities exist to support bicycling for those other than experienced cyclists who choose to ride with traffic
- Planned facilities are inadequate to promote any significant shift in mode split to cycling
- A continuous east/west and north/south trail or protected bikeway through the study area should be a high priority to encourage ridership and link downtown with outlying destinations and existing trail networks



Bicycle Facilities

Transit

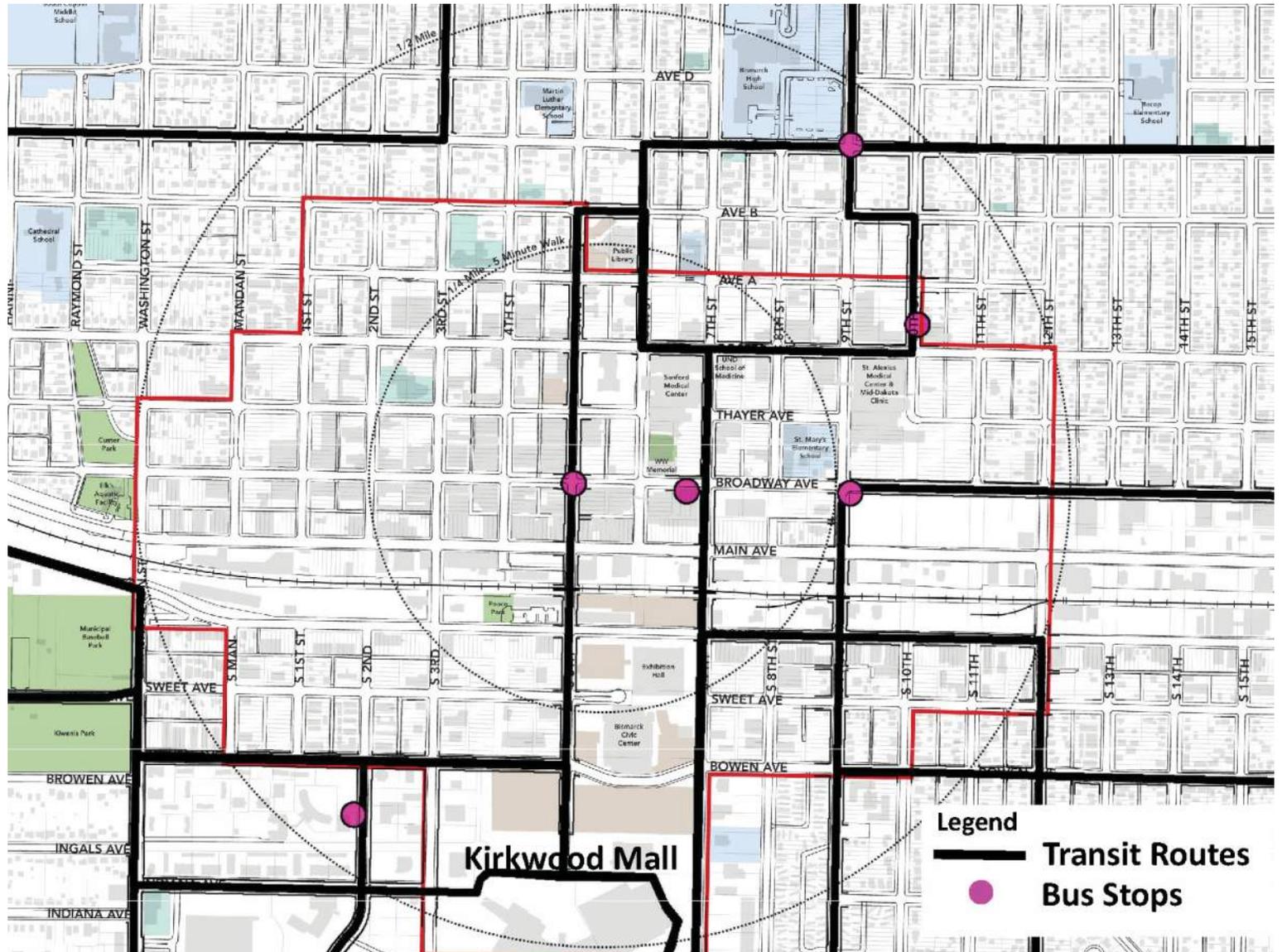
Relevance to the Subarea Study:

Capital Area Transit (CAT) is a fixed route bus service that operates on a total of 12 routes across Bismarck and Mandan. Depending on the route, CAT operates from approximately 6:00 AM to 7:00 PM, Monday through Friday, with limited Saturday service. Most service headways are either 30 minutes or one hour frequencies, with Bismarck to Mandan/Mandan to Bismarck service running every two hours.

Analysis

- Current routes have limited headways and as such do not encourage high transit use as an alternative to driving
- Current routes run in a loop system maximizing geographic area but providing longer travel times and indirect routes to traffic generating uses
- Downtown bus transfers occur at the Kirkwood Mall away from major employment and government uses located north of the rail line
- Mobility 2017 has determined the need for a downtown transit center

Transit Routes



TRAFFIC

Documents Reviewed include:

- Long Range Transportation Plan-LRTP 2010-2035 (2010)
- NDDOT 2012 Average Daily Traffic
- City of Bismarck Updated Truck Routes (2012)

Relevance to the Subarea Study:

Based on existing and historical traffic data within the study area the following analysis provides a base line level of operation from a planning-level roadway capacity perspective.

Functional Classifications

- Traffic is distributed throughout downtown along:
- East/west principal arterials at Main Avenue (4 lanes) and Bismarck Expressway (5 lanes)
- North/south principal arterials at 7th Street (4 lanes/one-way southbound) and 9th Street (4 lanes/one-way northbound)
- Minor arterials along Washington Street (5 lane-south of Rosser Avenue), Rosser Avenue (3 lane), 3rd Street (4 lane south of Main Avenue, (3 lane north of Main Avenue), and 5th Street (4 lane south of Main Avenue, 2 lane north of Main Avenue), Front Avenue (2 lane), and Bowen Avenue (2 lane)
- Designated truck routes exist along Bismarck Expressway, Washington Street south of Main Avenue, Boulevard Avenue, Main Avenue, 7th and 9th Streets, and 12th Street south of Main Avenue

Freight Rail and Crossings

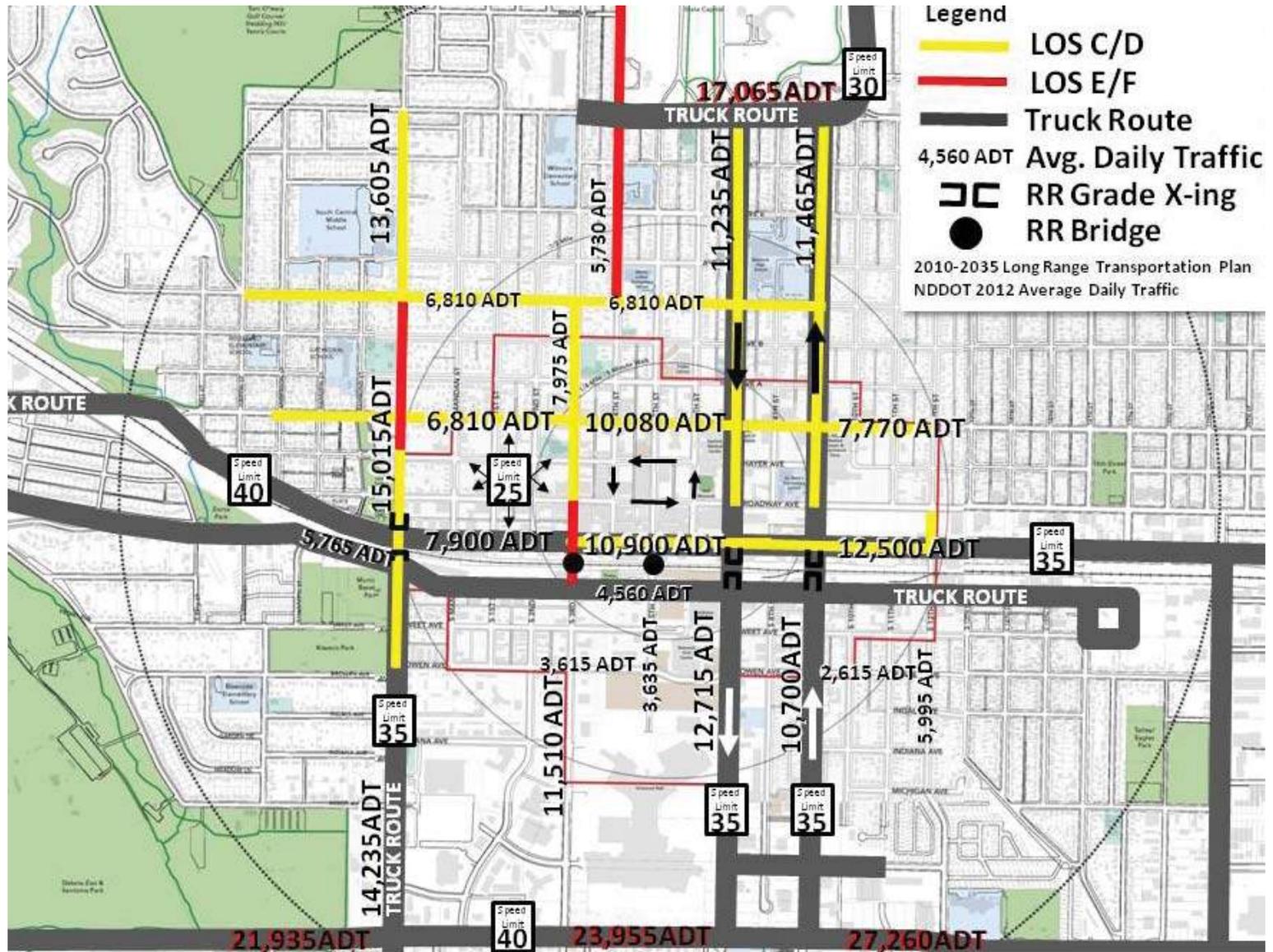
Bismarck is served by the Burlington Northern Santa Fe Railroad and the Dakota, Missouri Valley & Western Railroad with a single track mainline running through downtown between Main Avenue and Front Avenue. The rail line consists of:

- **22 trains per day**
- **At grade crossings at 3rd Street, 5th Street and 12th Street**
- **Grade separated bridge/underpass at Washington Street, 7th Street and 9th Street**
- **The City is currently implementing a Quiet Rail Zone through downtown with improvements to the at-grade crossings**

Traffic Operations and Recommended Transportation Plan

Generally, downtown streets provide more than adequate capacity for automobile travel. The quality of traffic flow is determined by the concept level of service. The level of service for downtown streets that are nearly meeting or exceeding deficiencies include portions of Washington Street, Main Avenue, 3rd Street, Rosser Avenue, Avenue B, 7th Street and 9th Street. No planned traffic improvement projects have been identified within the study area.

Traffic



PARKING

Documents Reviewed include:

- Downtown Bismarck Parking Study (2008)

Relevance to the Subarea Study:

Downtown parking consists of on-street spaces and off-street facilities within private surface lots, and three City-owned ramps managed by the Parking Authority. Downtown parking includes:

- A total of 11,453 parking spaces excluding the Civic Center and Kirkwood Mall
- An additional 1,200 Civic Center Spaces, and 4,000 Kirkwood Mall spaces
- 9,665 off-street parking spaces (1,230 general public spaces and 8,435 private spaces)
- 1,788 spaces on street public parking (16 % of total spaces)
- Construction of a parking ramp for Private monthly users at the southwest corner of 6th Street & Thayer Avenue (500 spaces)

Downtown Parking Supply and Utilization

The Bismarck Downtown Parking Study determined that there is:

- A parking surplus of 3,543 spaces or approximately 34.4% of the effective supply
- Overall, during peak periods parking occupancy utilization was 49.6% of on-street parking and 57.5 % of off-street parking
- During peak periods parking levels reached 80% to 90% capacity for specific blocks (See image above- in yellow).

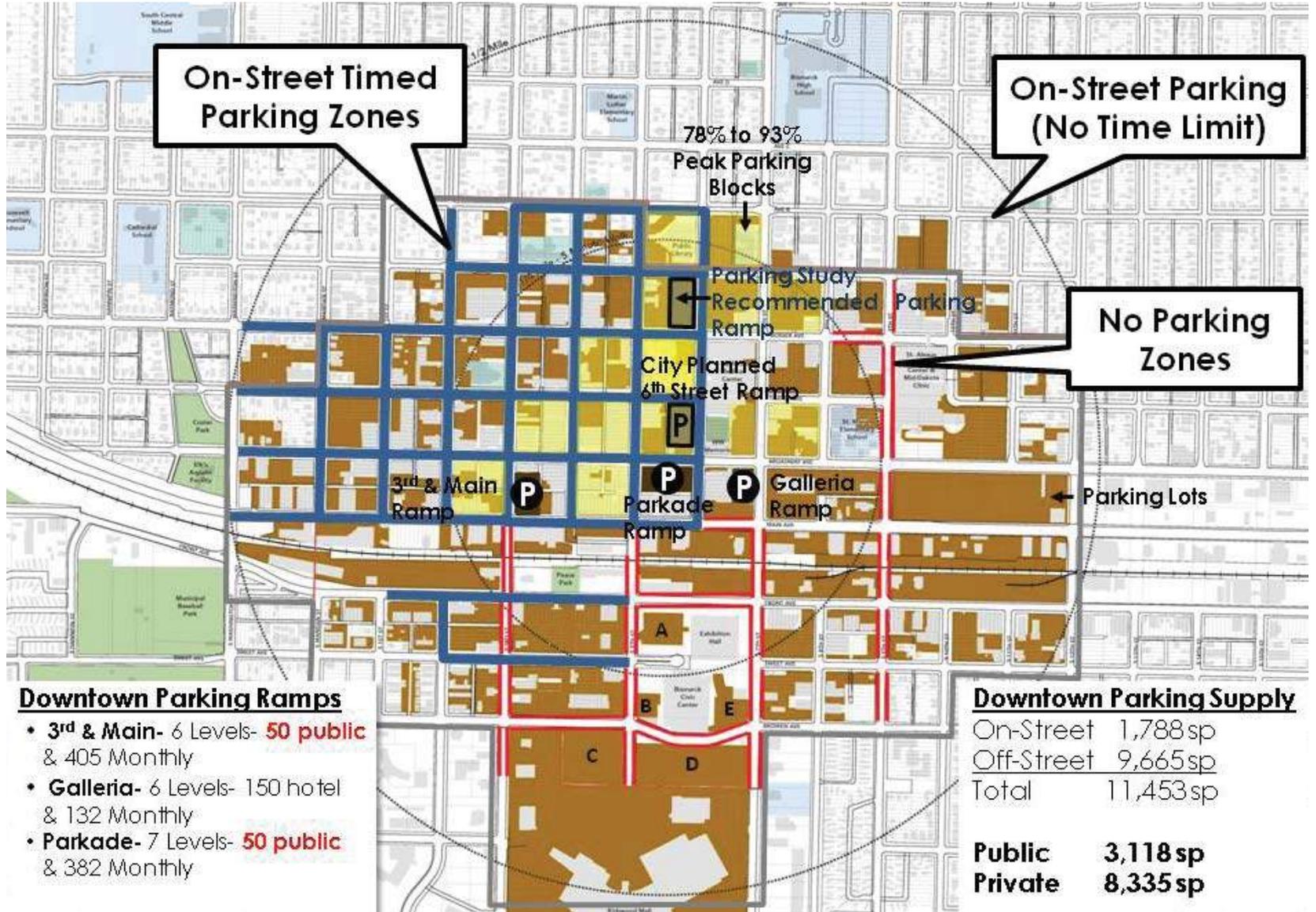
Potential Parking Structure Sites

The Downtown Bismarck Parking Study identified two sites for the location of downtown parking structures (see image above). These structures are intended for use primarily as monthly spaces to support downtown offices and medical uses. The location of these structures is not ideal for the downtown retail customer due to their lack of visibility and proximity to retail.

Downtown Parking Issues

- Downtown retail customers and visitors are underserved by existing parking supply and compete primarily for on-street parking
- There is a need for additional public parking to support downtown businesses and preserve land for development
- The Sanford Medical Center parking is near capacity at peak periods
- There is currently no leased parking available through the Parking Authority
- Only 8% (100 sp) of the 1,230 Parking Authority spaces are for public parking
- The Downtown Parking Study determined that employee and downtown worker parking is negatively impacting the available visitor and customer parking
- Downtown redevelopment is limited by valuable downtown property used for surface parking

Parking



TRANSIT CENTER

Documents Reviewed include:

- Mobility 2017 Plan (2011)

Relevance to the Subarea Study:

The Mobility 2017 Plan provides recommendations for a downtown transit center and upgrade of system routes that support a minimum 30 minute headway

Potential Transit Center Locations

Several potential locations for a transit center in Bismarck have been identified and include the following:

On-street options:

6th Street between Main and Broadway. This location has the advantage of good proximity to many downtown destinations and relatively low traffic, but is somewhat constrained in that there are multiple curb cuts on both sides of the block. It is estimated that 6-8 buses could layover here in an on-line configuration but buses would not be able to arrive and depart independently (the bus in front would have to leave first). About eight to 10 on-street parking spaces would need to be removed to accommodate a transit facility here.

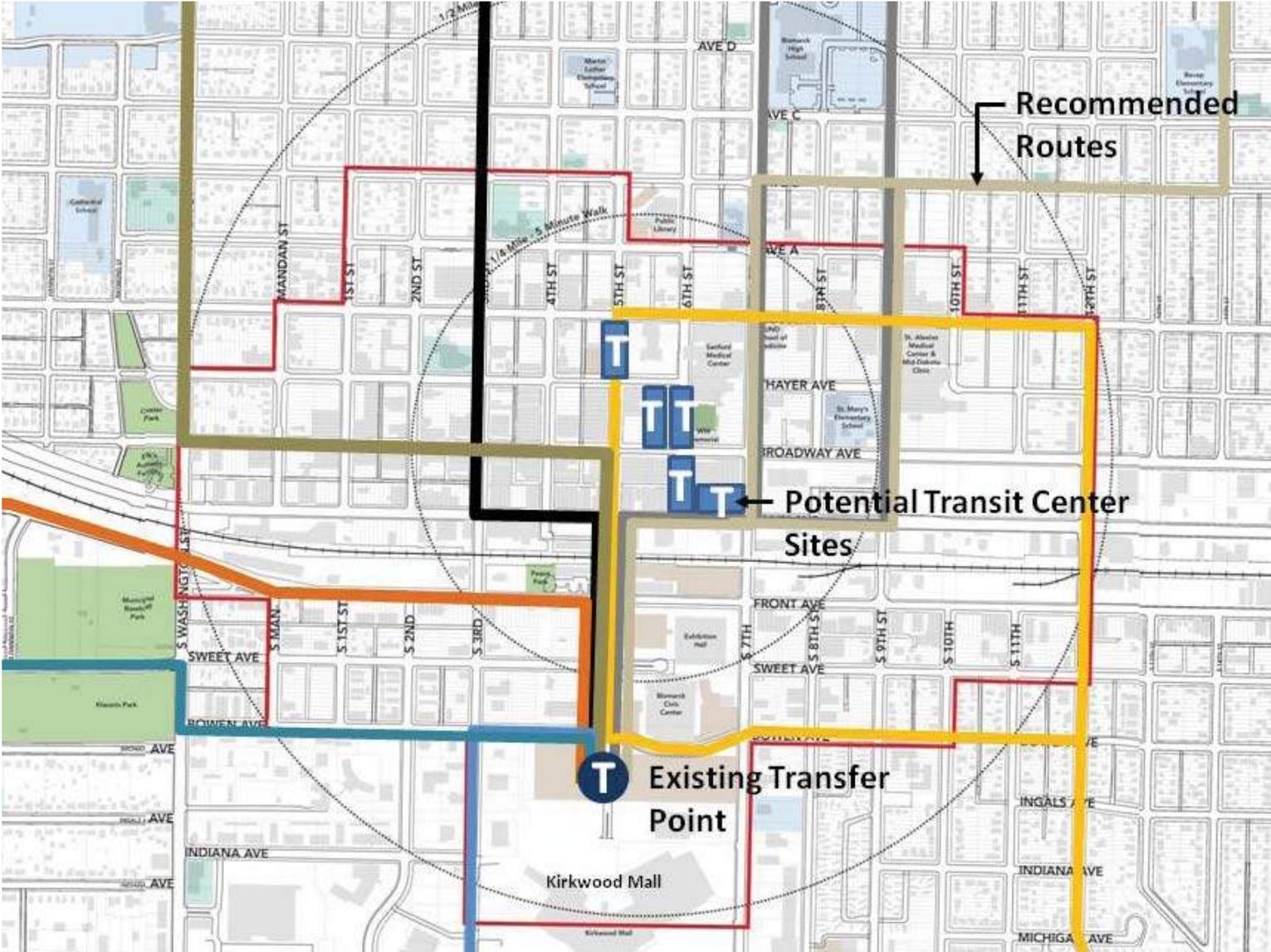
6th Street between Broadway and Thayer. This location is on a one-way street in the northbound direction, which limits the flow of transit to only one direction and one side of the street (likely the west side). Disadvantages of this location are the ability to accommodate the estimated 9 buses and the potential removal of about 20 on-street parking spaces.

5th Street between Thayer and Rosser. This location has the advantage of a continuous curb on both sides of the street, which can accommodate an estimated 10 transit vehicles (or more). The major disadvantage with this site is that it is somewhat removed from the center of downtown. It is estimated that 24-26 on-street parking spaces would need to be removed, assuming transit utilizes the entire block on both sides (although it is anticipated that transit would not need the entire length of the block, and perhaps only one side).

Off-street options:

Main, Broadway, 6th and 7th. This location includes the parking lot on the southern half of the block. While the site is also constrained in terms of space, it could be designed to accommodate 6-8 transit vehicles, which would include several on-street bays on Main Street. Another potential disadvantage with the location is access to the site. It is also estimated that an estimated 50 parking spaces would be lost.

Transit Center Sites



Benefits of a Downtown Location

There are a number of important benefits to locating the transit center in downtown.

- Allows for timed connections. Because Bismarck is largely developed in a radial pattern around downtown, a new transit center in this area would allow most routes to be designed so that the round trip travel times are about the same. This ensures that all (or most) routes in Bismarck can be timed from a central point, facilitating connections between routes and minimizing wait times.
- Provides a comfortable passenger facility. In addition to the main transfer location between routes, the downtown transit center as envisioned would serve as the most prominent transit facility in the region. As such, it should also include a comfortable, enclosed area for passengers to wait. Typical elements of a downtown transit center are discussed in more detail below.
- Promotes development and activity in downtown Bismarck. Transit can and should play a central role in revitalizing downtown Bismarck – both by bringing people downtown and by focusing new development in this area. Transit facilities can also be developed in conjunction with other uses, such as retail, office or housing.
- Allows the fixed route system to grow. As envisioned in the recommended service alternative (Option B), a secure and dedicated passenger transfer facility will also ensure that the CAT system is able to grow. The current transfer centers at Kirkwood and Gateway Malls are not permanent facilities and are limited in terms of available space.

Essential Elements of a Downtown Transit Center

Transit centers can vary widely depending on the size of the transit system and intended function of the facility. The following is a list of elements typically found at medium-sized transit centers and some general guidelines for how those elements could be designed:

- **On-or off-street.** Transit centers can be designed as either on- or off-street, depending on availability of land and right-of-way. While no preference is given to an on-street versus off-street transit facility, any on-street configuration should ensure that passengers have good access to crosswalks. In some cases, physical barriers in the middle of the street can discourage passengers from crossing mid-block at an on-street transit facility. Off-street transit centers are often designed to allow transfers between routes without having to cross the street, but also require the acquisition of property, which is often more valuable in downtown areas.
- **Bus bays.** To ensure adequate space for all vehicles to “pulse” at the transit center, which allows timed connections between all routes, the Bismarck transit center should be designed to accommodate up to nine CAT transit vehicles at one time, in addition to other vehicles (e.g., West River Transit buses, other rural or intercity providers). This assumes all routes operate every 30 minutes (either during peak periods or all day). Bus bays can be designed in a number of different configurations but are typically designed as in-line, sawtooth or diagonal bays.
- **Transit vehicle driveway.** Off-street transit facilities must also include enough right-of-way to allow transit vehicles to circulate within the site, which increases the footprint of the facility.

- **Passenger waiting area.** The transit center should have adequate space for passengers to wait for their bus or get information about CAT. In Bismarck, it is estimated that an enclosed area that can accommodate as many as 50 people (seated and standing) would be appropriate. The windows and doors to the boarding/alighting area should be designed to allow passengers to easily see approaching vehicles, and to ensure “eyes on the street.” This could also be accomplished through real-time passenger information displays.
- **Staffed fare vending and information booth.** As the main transit facility for the CAT system, the transit center would ideally include a staffed ticket and information booth. On-site staff would be able to sell all fare media, provide general information about CAT, and/or help people navigate the system. This could be a relatively small office with a window.
- **Signage and passenger information.** This includes maps, schedules and any other information that helps passengers better navigate and understand the system. Take-away items such as maps and brochures would ideally be located in an enclosed area, but maps and schedules should also be posted outside in the passenger boarding areas for easy reference.
- **Optional elements.** There are a number of other elements that could be incorporated into the design of the transit center. None of the elements listed below are critical to the success of the facility, but do help improve the experience of using transit for all passengers.

— Bathrooms

— Security cameras

— Retail and/or joint development (such as office or residential)

— Bis-Man Transit operations offices

— Bis-Man Transit administrative offices

URBAN DESIGN

Documents Reviewed include:

- Existing Land Use- City GIS
- Renaissance Zone Development Plan Update 2012- (Appendix B, RZ-Block Spreadsheet)
- Central District Plan (1993)

A review and analysis of existing downtown uses (with an emphasis on retail), a survey of ground floor buildings, and identification of soft parcels that may provide opportunities for redevelopment was conducted.

Existing Land Use

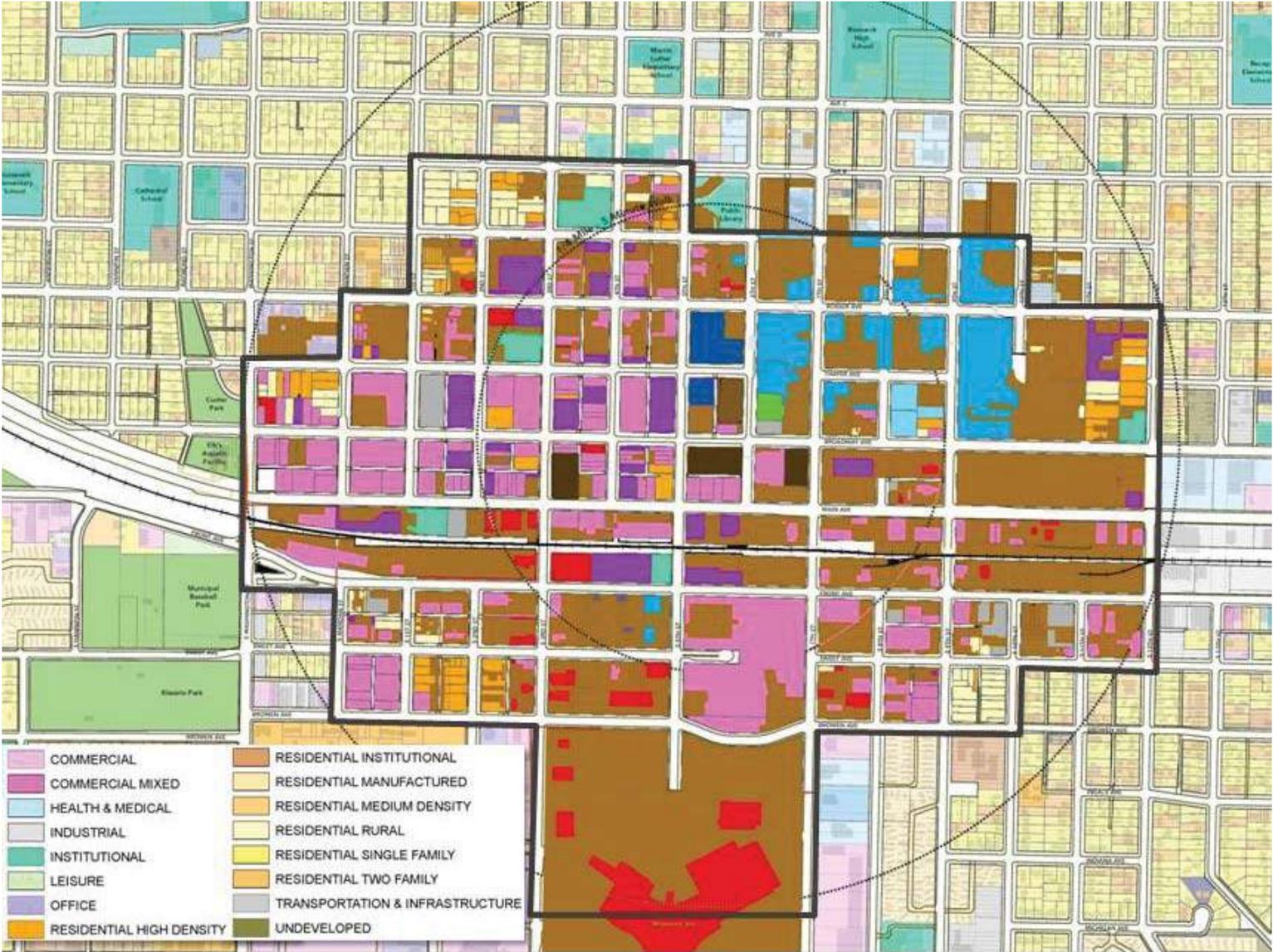
Relevance to the Subarea Study:

Existing land uses provide a snapshot of the type and location of development and investment in downtown.

Analysis

- There is a high concentration of employment in the downtown associated with government, banking, healthcare and education
- There is a severe lack of public space in downtown
- There is a concentration of auto oriented and low intensity development along Main Avenue, Washington Street, 7th Street, 9th Street and south of the railway
- The downtown retail offering is limited and dispersed and competes with the largest concentration of retail located ½ mile south at Kirkwood Mall
- The outer edges of the study area are well defined by mixed-density neighborhoods
- A limited amount of housing exists with an estimated 500 households within the study area
- Significant land area is surface parking and its continued expansion will further deteriorate the downtown investment environment
- Redevelopment opportunities should be directed to low intensity use areas and surface parking lots

Existing Land Uses



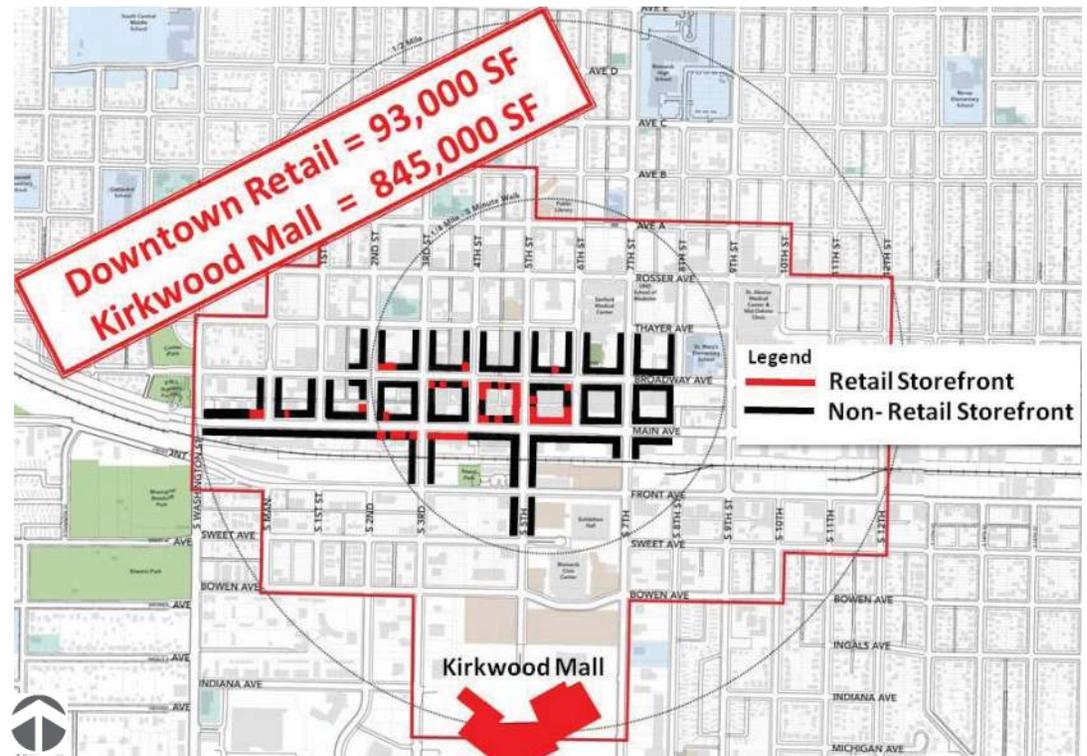
Retail

Relevance to the Subarea Study:

The success of downtown revitalization hinges on the ability to create a retail destination- which typically includes, at a minimum, 150,000 square feet of street oriented retail concentrated over a four block length (5 minute walking distance). The location of ground floor retail was identified along Downtown's historic retail streets- Broadway Avenue, and Main Avenue and cross streets.

Analysis

- There is limited street oriented retail (estimated at 93,000 sf)
- Retail is dispersed and not concentrated
- Competing retail destination at Kirkwood Mall (845,000 SF)



Existing Retail Storefronts

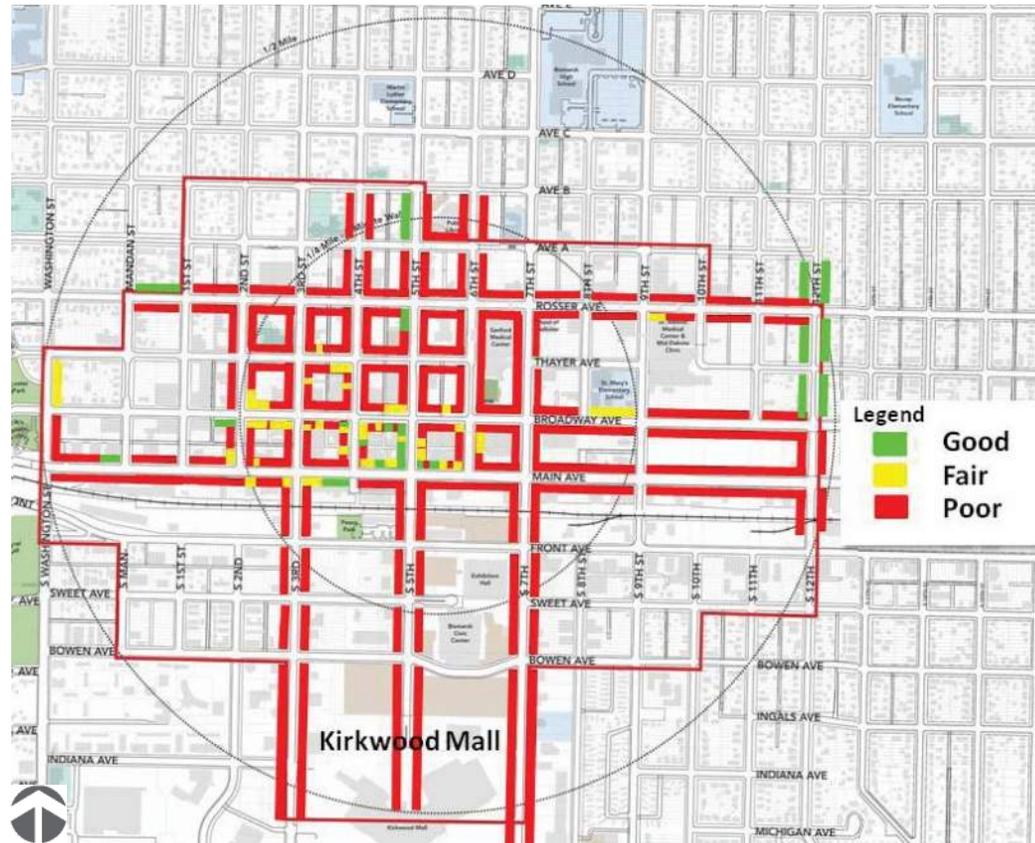
Ground Floor Transparency

Relevance to the Subarea Study:

Ground floors of buildings that incorporate a high level of transparency (50% to 70% of building wall) with windows and doors facing the street create an active edge, animate the public realm and offer a high level of comfort and safety.

Analysis

- The majority of downtown offices, banks, hospitals, the library and government buildings include tinted or reflective glass, blank walls, and limited windows and doors facing the streets
- Residential areas to the north and east of downtown offer a comfortable and safe environment with landscaping and house entries facing the street
- Several blocks along Main Avenue as well as 4th, and 5th Streets provide examples of good active edges and typically include retail, and restaurant establishments with high quality historic building facades



Sidewalks

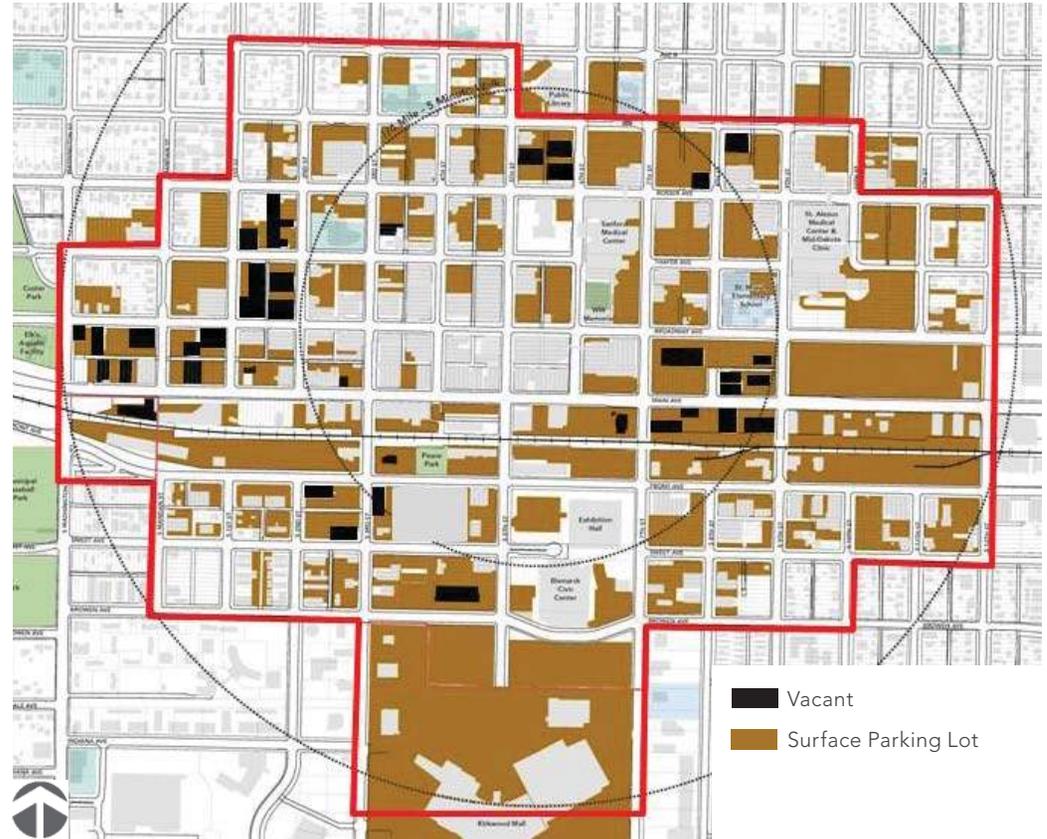
Soft Parcels

Relevance to the Subarea Study:

Potential soft sites include vacant lots, underutilized buildings and parking lots. Soft parcels represent potential opportunities for downtown redevelopment.

Analysis

- Significant land within the study area is parking lots
- Some underutilized buildings exist at the edges of the study area and along Main Avenue, Rosser Avenue, and Broadway Avenue



Soft Parcels

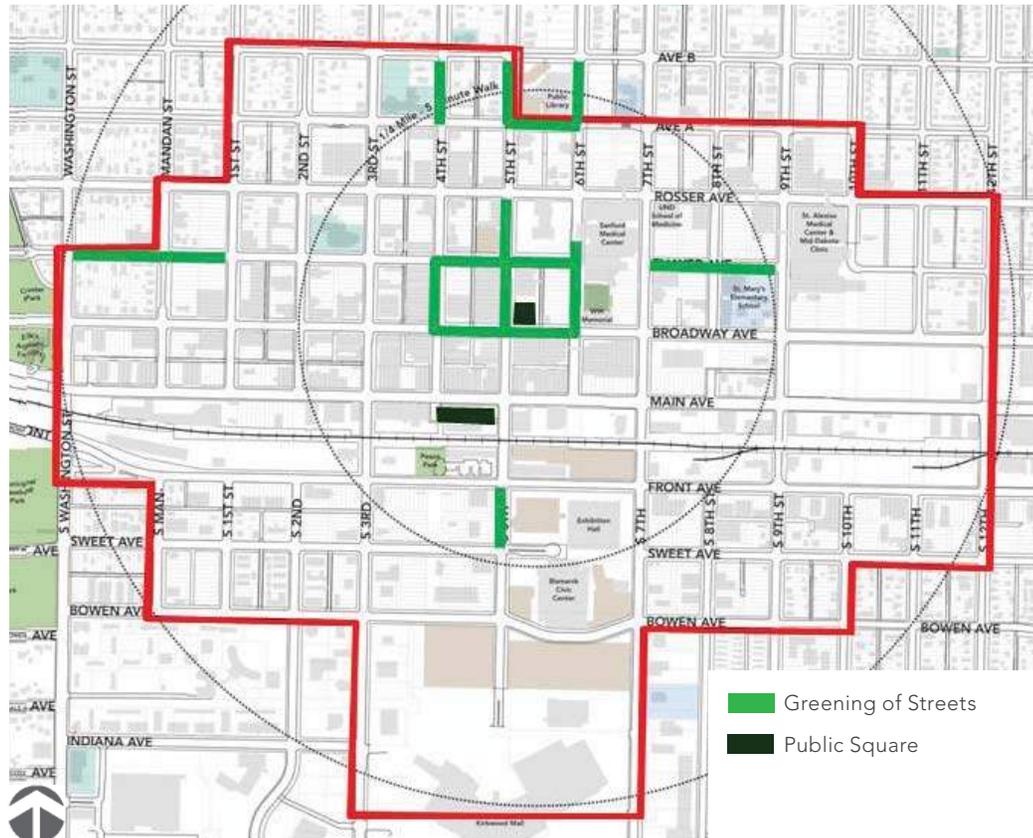
Public Spaces

Relevance to the Subarea Study:

Public spaces such as a public square or park, and additional greening of downtown streets have been identified in the Central Business District Plan as well as other plans and documents as identified on the right.

Analysis

- Chancellor Square has matured with a sizable amount of tree cover
- Existing raised planters in Chancellor Square provide some green relief but also limit the use of downtown sidewalks
- The majority of downtown streets lack street trees
- The Peace Park suffers from an isolated location, limited visibility and is not easily accessible
- The depot public square site identified in the Central Business District Plan provides a limited area not suitable for downtown events
- The Broadway public square site identified in the Central Business District Plan is surrounded by inactive ground floor uses, is less visible, and is not sized well to accommodate larger downtown events



Public Spaces

PLANS, POLICIES, AND REGULATORY DOCUMENTS

Documents Reviewed include:

- Title 14 Zoning
- Land Use/Development Process
- Growth Management Plan (2003)
- Long Range Transportation Plan (LRTP) (2010-2035)
- Transportation Improvement Plan (TIP) (2012-2015)
- Bismarck Strategic Plan (2012)
- MEDAG (2009)
- Bismarck Renaissance Zone Development Plan (2012)
- Central Business District Plan (1993)
- Streetscape Guidelines for Downtown Bismarck (1995)
- Quiet Rail Zone (2008)
- Mobility 2017 (2012)

The following is a review and analysis of existing regulatory codes, relevant plans, standards, guidelines and the development approval process:

Zoning- Title 14

Relevance to the Subarea Study:

Title 14 is the regulating document for zoning uses and establishing development standards in the downtown. Zones include:

Commercial General (CG)- Established as a heavy commercial area located outside the central business district (Downtown Core) to provide commerce and service to the City of Bismarck and surrounding regional market of minimum 7,000 square feet lots, 130' permitted height, and 42 dwellings per acre.

Commercial Regional (CR)- Established as a district in which the principal use of the land is for regional shopping centers and related services of minimum 40 acres- 300,000 square feet gross floor area, 50' permitted height, and minimum parking at 5 spaces/1000 square feet

Downtown Core (DC)- Preserves and enhances the mixed-use, pedestrian-oriented nature of the City's downtown area. The district allows a wide range of uses to enhance downtown Bismarck's role as a commercial, cultural, governmental, health/ medical, entertainment and residential center. Design standards maintain the historical integrity, enhance the quality of design, and preserve human-scale downtown development. Minimum 5,000 to 7,000 square feet lots, and 130' permitted height. Design standards apply within this zone.

Downtown Fringe (DF)- The Downtown Fringe serves to provide a transitional area between the Downtown Core District and adjacent commercial and residential zoning districts. The uses allowed in this district usually require larger parcels and a greater emphasis on automobile access and parking. Minimum 5,000 to 7,000 square feet lots, and 130' permitted height. Design standards apply within this zone.

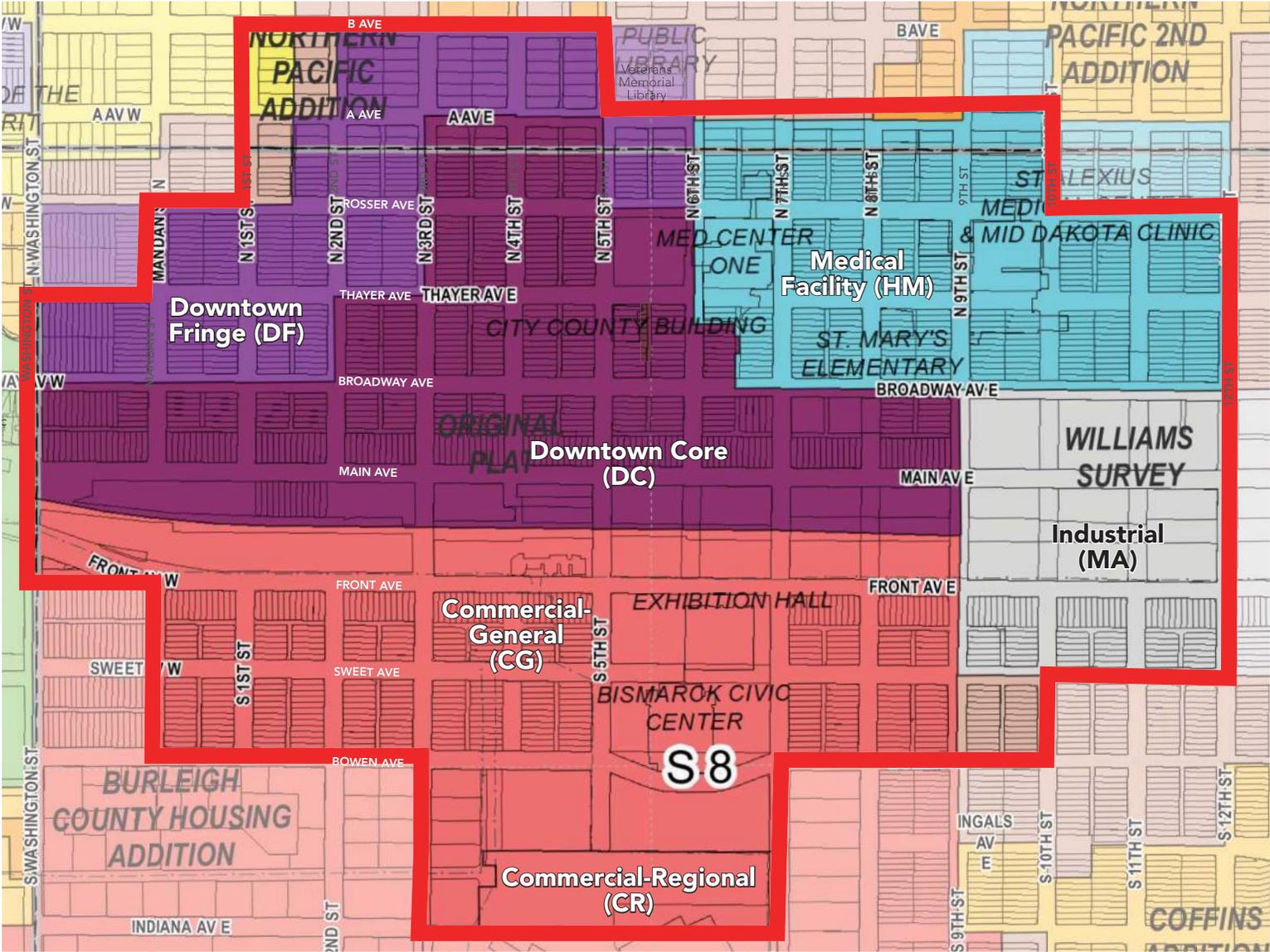
Medical Facility (HM)- Established as a district in which the principal use of the land is for health and medical uses with maximum lot and dwellings the same as the RM residential zone and 50' permitted height with exceptions.

Industrial (MA)- Established as a district in which the principal use of the land is for heavy commercial establishments and non-nuisance industries of minimum 10,000 square feet and 75' permitted height.

Residential-Multi Family (RM)- Established as a district in which the principal use of land is multifamily dwellings and similar high density residential development of 2,300 square feet to 3,795 square feet lots at 30 units per acre and 60' permitted height.

Residential-Single Family (R5)- Established as a district in which the principal use of the land is for single-family dwellings of 7,000 square feet lots at 5 dwellings per acre and 40' permitted height.

Title 14- Zoning Districts



Land Use/Development Process

Relevance to the Subarea Study:

The Planning Division administers land use regulations and coordinates the development application process and includes a City Planning and Zoning Commission that hears requests for major and minor subdivision plats, zoning changes, zoning ordinance text amendments, annexations, special use permits and rural lot splits. The Land Use Development Procedures are indicated below.

	Zoning Change	PUD Amendment	Major Plat/ Vacation	Minor Plat	Annexation/ Detachment	Special Use Permit	Zon Ord Text Amendment	Street Name Change
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APPLICATION:

Appl. Deadline	33 days prior	33 days prior	33 days prior	33 days prior	33 days prior	33 days prior		
Fee	\$500 \$650 - PUD	\$450	\$850-plat (6) \$475-vac	\$600 (6)	\$325	\$275		
Est. Approval Time	120 days	45 days	120 days	45 days	45 days	33 days	120 days	30 days

PLANNING COMM:

Consideration	Yes	Yes	Yes		Yes		Yes	
Public Notice	once/wk 2 wks prior		once/wk 2 wks prior	once/wk 2 wks prior				
APO Notification	10 days prior	10 days prior	10 days prior	10 days prior		10 days prior		
Public Hearing	Yes	Yes	Yes	Yes		Yes	Yes	

CITY COMM:

Consideration	Yes		Yes	Yes	Yes		Yes	Yes
Petition								
Public Notice	once/wk 2 wks prior				once/wk 2 wks prior (1)		once/wk 2 wks prior	
APO Notification								10 days prior to cons.
Public Hearing	Yes				Yes		Yes	

MISC:

Legal Notice	once after approval	once after approval					once after approval	
Appr. Instrument	Ordinance	Ordinance	Motion	Motion	Ordinance or Res.	Motion	Ordinance	Resolution
Record			Yes	Yes	Yes			Yes

LEGAL:

References	ZO Sec. 14-07 NDCC 40-47	ZO Sec. 14-04-18	ZO Sec. 14-09 NDCC 40-48/40-50	ZO Sec 14-09 NDCC 40-48/40-50	NDCC 40-51.2	ZO Sec. 14-03-08	ZO Sec. 14-07 NDCC 40-47	ZO Sec. 14-09-12
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Land Use/Development Process

	Street/Alley Vacation (3)	Setback Line Vacation	Non-Access Line Vacation	Release of Easement	Lot Modification (4)	Rural Lot Splits (5)	Fringe Area Road Master Plan Amendment	Land Use Plan Amendment
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APPLICATION:

Appl. Deadline						33 days prior	33 days prior	33 days prior
Fee	\$250		\$200	\$150	\$25	\$275	\$250	\$250
Est. Approval Time	45 days	30 days	21 days	14 days	7 days	33 days	120 days	120 days

PLANNING COMM:

Consideration							Yes	Yes
Public Notice						once/wk 2 wks prior	once/wk 2 wks prior	once/wk 2 wks prior
APO Notification						10 days prior	10 days prior	10 days prior
Public Hearing						Yes	Yes	Yes

CITY COMM:

Consideration	Yes	Yes	Yes	Yes			Yes	Yes
Petition	Yes		Yes	Yes				
Public Notice	once/wk 4 wks prior (7)	once/wk 2 wks prior	once/wk 2 wks prior				once/wk 2 wks prior	once/wk 2 wks prior
APO Notification		10 days prior (2)						
Public Hearing	Yes	Yes	Yes				Yes	Yes

MISC:

Legal Notice	once after approval							
Appr. Instrument	Resolution	Resolution	Resolution	Resolution	Letter	Motion	Resolution	Resolution
Record	Yes	Yes	Yes	Yes		Yes		

LEGAL:

References	NDCC 40-39				ZO Sec 14-09 NDCC 40-48/40-50	ZO Sec 14-09-04	Department Policy	Department Policy
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Land Use/Development Process

Renaissance Zone Development Plan (Revised 2012)

Relevance to the Subarea Study:

The Renaissance Zone was established to encourage reinvestment in downtown and encompasses a 39-block area in downtown. The Renaissance Zone provides:

- Tax exemptions and credits to both residents and businesses for revitalization and redevelopment activities within the Zone.
- Property tax and income tax incentives to property and business owners who invest in qualified projects.

The Renaissance Zone and development plan is administered by the Community Development Department.

Growth Management Plan

Relevance to the Subarea Study:

The Growth Management Plan consists of a policy plan for development and services, a generalized future land use plan and a parks, open space and public land plan within a set boundary that approximates the four-mile extraterritorial jurisdiction. The boundary includes the City of Bismarck, and numerous townships within Burleigh County.

Long Range Transportation Plan (LRTP) (2010-2035)

Relevance to the Subarea Study:

The LRTP recommends the following multimodal improvement program elements to address future impacts on the region’s mobility.



Renaissance Zone Boundary Map

- Roadway system improvements that address safety, congestion/serviceability, and connectivity/accessibility goals.
- Non-motorized system investments that plan for an expanded trail network and establish an on-street bicycle route system.
- Transit system expansions that provide opportunities for the fixed route bus system to provide enhanced service through 2035.
- Travel demand management programs that will help address the longterm travel needs of the metropolitan area, leveraging the project/program investments recommended by managing the growth in travel demand on the multimodal system.
- Detailed corridor/subarea studies of areas and issues that require more work than is associated with the LRTP

Transportation Improvement Program (TIP) (2013-2016)

Relevance to the Subarea Study:

The 2010-2035 Long Range Transportation Plan details the “long range” and “short range” transportation improvement projects for the next 25 years. The TIP is part of the planning process that encompasses projects that are consistent with the “short range” project portion of the 2010-2035 Long Range Transportation Plan.

- There are no TIP projects planned within the study area for the 2013-2016 time period

Bismarck Strategic Plan (2012)

Relevance to the Subarea Study:

The Strategic Plan serves as a collectively supported, integrated road map of how Bismarck moves toward its desired future. Keith Hunke, Assistant City Administrator, in consultation with Bill Wocken, City Administrator, Mayor Warford and City Commissioners, and members of the Project Team, has assumed the responsibility for ensuring the Strategic Plan is implemented. The following are strategic plan goals and objectives directly relevant to downtown.

▪ **Enhance revitalization efforts for the downtown area.**

Objectives:

1. Update downtown master plan.
2. Explore funding opportunities and work with developers and partners to explore open space.
3. Lead and explore marketing of the Renaissance Zone and CORE program.

▪ **Become a destination place.**

Objectives:

1. Establish a public civic square in the downtown area.
2. Create a first class full service convention center.
3. Work with community partners to develop and implement a plan for jointly promoting special events and attractions regionally and nationally.

▪ **Downtown: To have a vibrant, lively and attractive destination as the heart of the community.**

Objectives:

1. Increase market-rate housing quantities and availability
2. Implement a Quiet Rail Zone
3. Relieve parking problems along the Rosser Avenue corridor between 3rd Street and 7th Street with the development of a ramp
4. Implement a public plaza or public open space along the north side of Broadway Avenue between 5th Street and 6th Street.
5. Update the Downtown Master Plan.
6. Help facilitate and support collaborative efforts to develop an arts and cultural center
7. Multimodal transportation opportunities and facilities

Mayor’s Economic Development Advisory Group (MEDAG) 2009

Relevance to the Subarea Study:

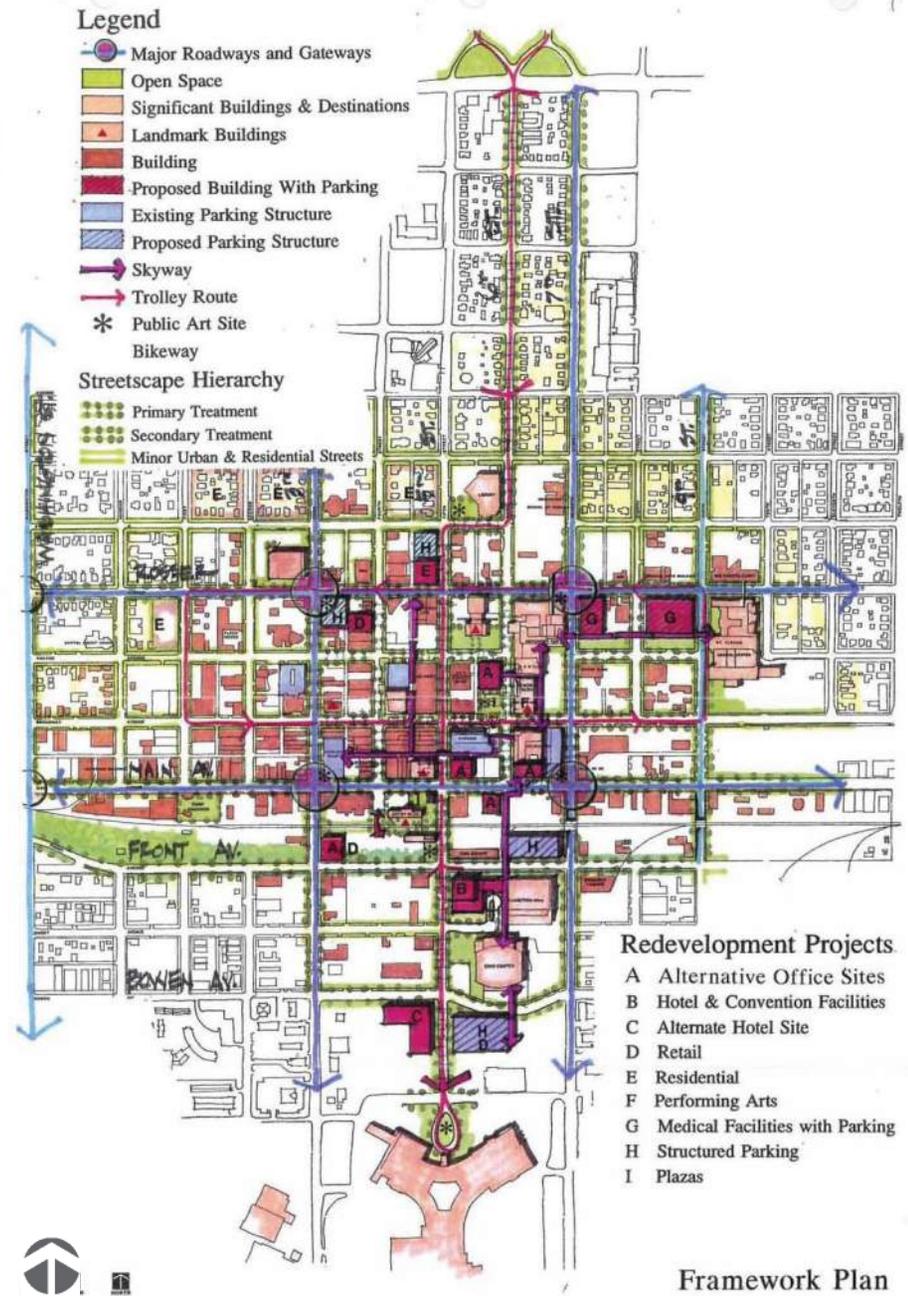
The Mayor’s Economic Development Advisory Group was established by Bismarck Mayor John Warford and Mandan Mayor Tim Helbling and supported by the City Commissions of Bismarck and Mandan. The group met bi-weekly over a four-month period between October 2008 and January 2009. The group’s purpose was to produce a set of initiatives/recommendations that could enable more efficient and effective local economic development efforts. Key elements include Civic Center improvements, the creation of an Applied Energy Technology Center and Great Plains Energy Corridor headquartered in Bismarck.

Central Business District Plan (1993)

Relevance to the Subarea Study:

The Central Business District Plan represents a guide for urban design efforts that support downtown revitalization within the study area. Elements of the plan include:

- The Vision for Downtown and Guiding Design Principles
- A Framework Plan for Redevelopment
- Transportation and Parking Elements that support mobility and parking
- Implementing actions for Downtown Redevelopment, the Design and Appearance of Downtown Buildings and Streets and Transportation and Parking



Central Business District Plan

Streetscape Guidelines for Downtown Bismarck (1995)

Relevance to the Subarea Study:

The design guidelines apply to redevelopment of Bismarck's downtown streets. The guidelines include street hierarchies and guidelines for site planning, building massing, and landscaping. The street hierarchies include:

- Primary Downtown Street-within the core retail, business, and cultural areas to receive the most intense streetscape treatment
- Secondary Downtown Street- outside the downtown core and provide linkages between residents and downtown to receive a moderate use of streetscape elements
- Special Streets- Located along, Main Avenue, 5th Street, and Front Avenue that serve special functions as the major approaches into downtown and will require special design treatments

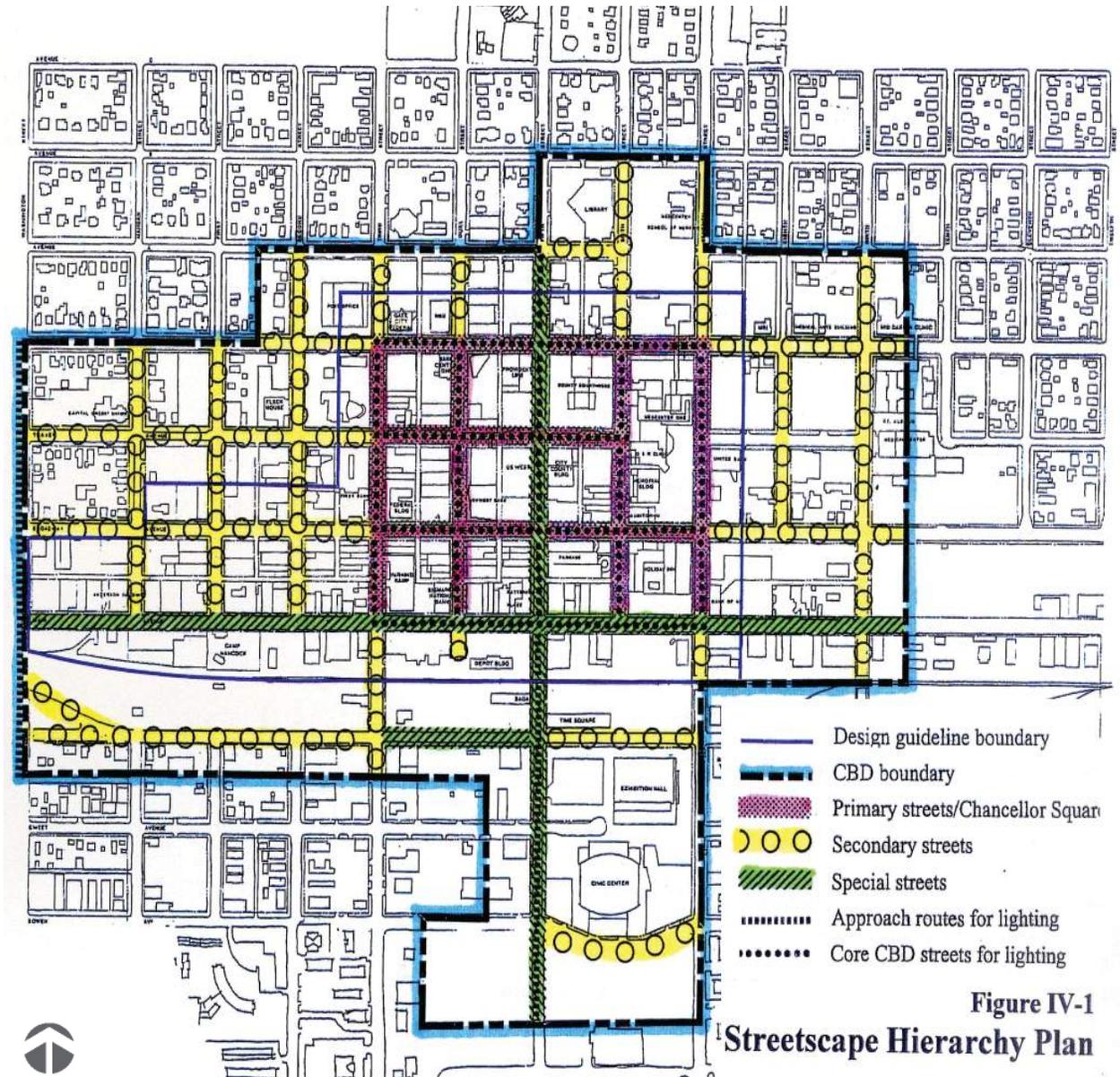


Figure IV-1
Streetscape Hierarchy Plan

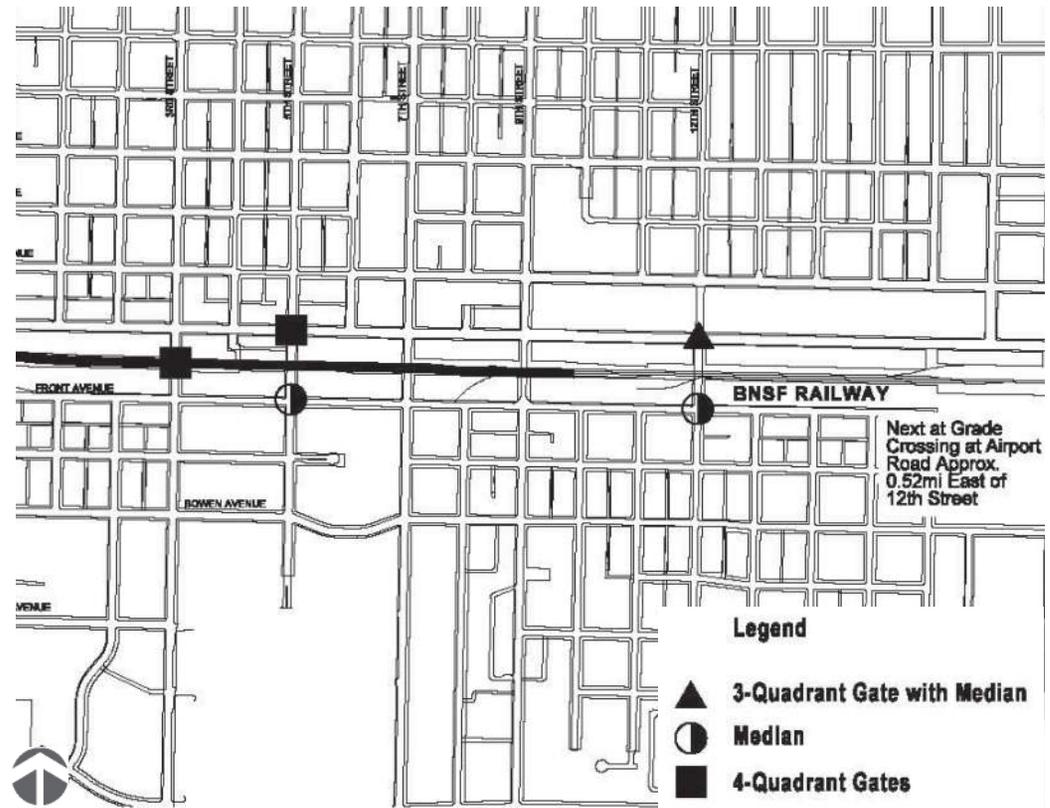


Central Business District Streetscape Design Plan

Quiet Rail Zone Assessment (2008)

Relevance to the Subarea Study:

Quiet Rail is anticipated to reduce the negative impact of noise associated with rail freight service and is a key action determined by the City to improve conditions for adding housing in the downtown. The City of Bismarck completed an assessment of a Quiet Rail Zone between 3rd and 12th Streets that included a range of alternatives. The City is currently funding pre-engineering and design services to implement the quiet rail zone. The cost of implementing at-grade crossings at 3rd, 5th and 12th Streets is estimated at \$3,700,727. Funding will likely come from TIF funds of the downtown Urban Renewal District.



Quiet Rail Zone Assessment

Mobility 2017 Transit Plan (2011)

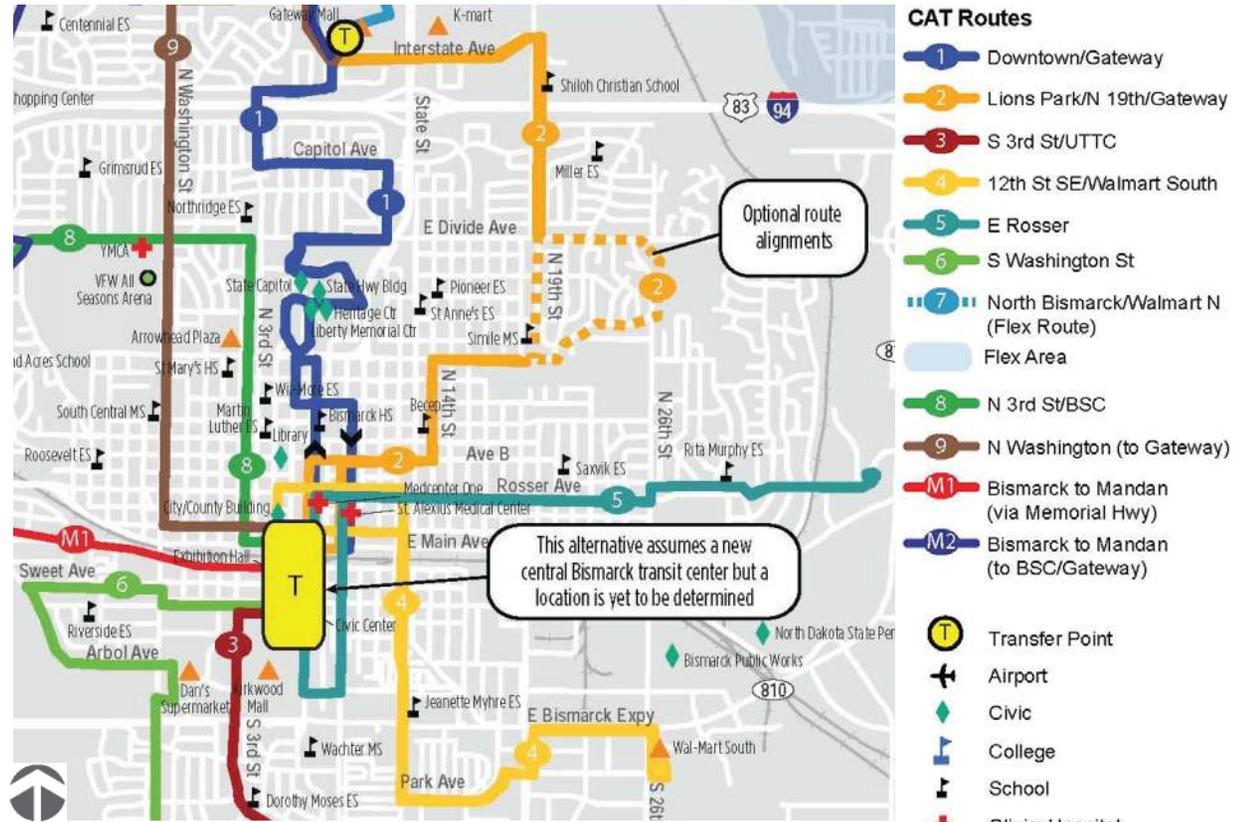
Relevance to the Subarea Study:

The purpose of Mobility 2017: Transit Roadmap for Bismarck and Mandan, is to undertake a comprehensive review of transit services to ensure the bus network keeps up with demand, addresses the diversity of needs, and is sustainable.

Fixed-Route Service Recommendations

Alternatives were developed (1) that assume no significant change in funding (status quo) but assume different levels of service restructuring and (2) an expansion in funding for fixed-route service in Bismarck and Mandan in line with a significant service restructure. The recommended alternative calls for a service expansion with the following objectives:

- Increase funding; improve service headways. Although the proposed route structure could be implemented with no change in funding, the recommendation is to shift a greater level of resources into the operation of fixed-route service to allow for better frequencies (minimum of 60 minutes midday, 30 minutes peak)
- Match service levels with demand. The focus is on enhancing ridership rather than providing coverage throughout the region
- Eliminate one-way loops. Bi-directional service provides the most direct routing and eliminates confusion associated with service that is provided in only one direction
- Minimize or eliminate double transfers. With the exception of one route in north Bismarck, all routes would pulse into and out of a single transfer location



Recommended Transit Routes

- De-emphasize service to elementary and middle schools, allowing for school tripper service if necessary
- Locate a transit center downtown

Future service expansions could include later service hours, Sunday service, or new routes to the University of Mary and Lincoln, with dedicated funding for those services.

FINANCING

Documents Reviewed include:

- Renaissance Development Zone Plan- 2012
- Core Phase II- Implementation Plan, 2006
- Growth Management Plan

Relevance to the Subarea Study:

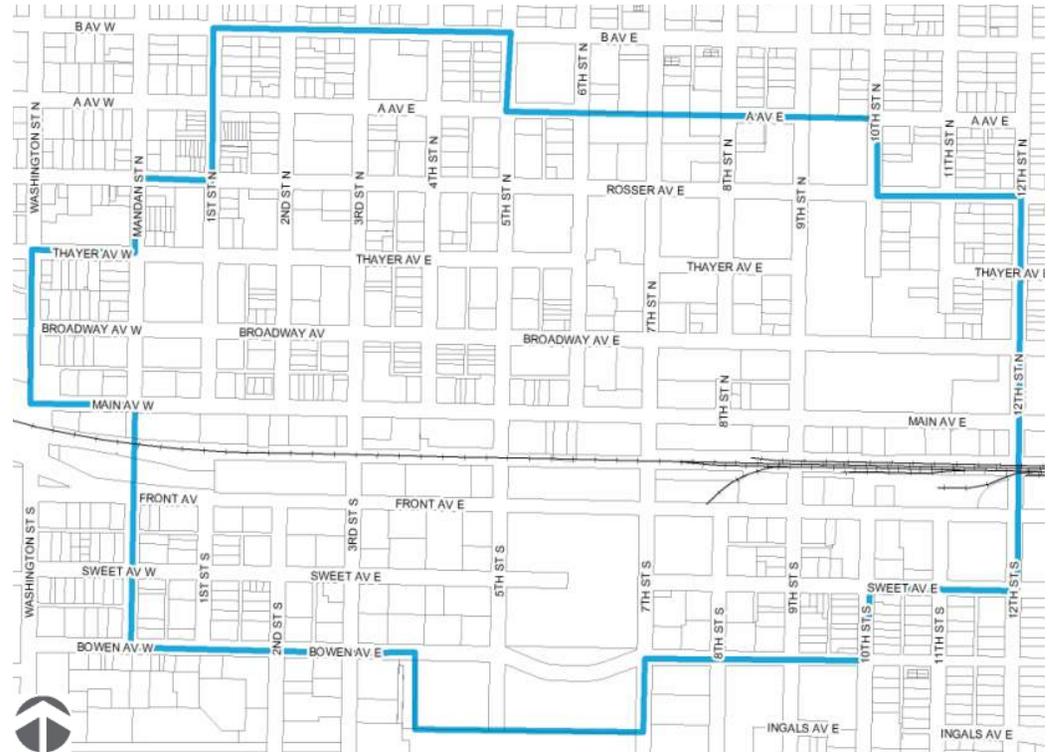
A primary source of funding for downtown projects is tax increment financing available throughout the study area. The current reserve of TIF funds are estimated at approximately \$9 million. Two committed downtown projects- the planned 6th Street Ramp and Quiet Rail improvements have the potential to consume all or a portion of the available TIF funds.

Analysis:

The City of Bismarck has a variety of fiscal resources to assist in the implementation of downtown projects. The methods used to finance infrastructure and other public improvements can influence how development and growth occur within the City. Property taxes, special assessments, sales tax, enterprise funds, community development block grants, and state and federal aid programs are some revenue resources available. These resources can be applied on a case-by-case basis to help achieve the goals and objectives of the Subarea Study.

CORE Incentive Program

The CORE Program was created to stimulate new development in the downtown area and fund projects that will serve as the foundation for future downtown development. These incentive programs are funded by the Downtown Tax Increment District and are available to property owners, developers and tenants for projects within that District. All CORE Incentive Program grants and awards are discretionary and will be considered on a case-by-case basis.



Tax Increment Finance District and CORE Boundary

The funds include:

1. Façade Grant program provides 50% matching funds made to building facades
2. Housing Incentive Grant program provides 20% matching grants for the creation or rehabilitation of housing units within the downtown area
3. Technical Assistance Bank offers financial assistance for professional design services from licensed architects and engineers
4. Sidewalk Subsurface Infill program offers grants for work within the public right-of-way needed to fill subsurface vaults under sidewalks

Property Taxes

Property taxes are a primary revenue source for local government. Tax rates apply uniformly to all property within the City regardless of what the property creates in terms of costs or benefits within the community. For this reason, property taxes are usually used to fund City operations.

Special Assessments

Special assessments are used to levy the costs of specific improvements against the properties that directly benefit from the improvements. Special assessments are traditionally used for infrastructure improvements, including roadways, sanitary sewer, storm sewer, and water mains.

In new subdivisions, the developer is responsible for paying trunk line charges, right-of-way grading, basic water and sanitary sewer main costs up front, with the remainder of the improvements generally being assessed to the individual lots within the subdivision. The North Dakota Century Code also allows the use of special assessments to develop public parks.

Sales Tax

As a home rule municipality, Bismarck has the authority to utilize sales tax to fund specific projects and reduce property taxes. This 1% sales tax is currently used to fund roadway construction and maintenance of City facilities, economic development activities, and to offset property taxes by an equivalent of 25 mills. Sales tax is also used to fund community betterment projects, which are voted on by the residents of Bismarck. The most recent vote in 2002 authorized funding for several project that would not otherwise have funding, such as the construction of a new fire station, construction of the new 911 emergency center, development of new recreational trails, and improvements to the Community Bowl.

Enterprise Funds

The City also has several enterprise funds, which are self-supporting and function much like a business in that fees are charged for services in order to cover the cost of operation and equipment replacement. Enterprise

funds within the City include the airport and flightline operations, the solid waste utility (collection and landfill), the water and sewer utility (water treatment & distribution, wastewater collection & treatment, and stormwater management facilities), and the Parking Authority operation.

Special Revenue Funds

Several operations of the City also operate as special funds, including the arena and exhibit functions of the Civic Center, the public library, specific function of the roads and streets department (snow gates and street lighting/traffic signals), and specific law enforcement activities. The lodging and restaurant tax used for capital construction and government grants are also budgeted as special revenue funds. The City of Bismarck is an entitlement community for Community Development Block Grants. These funds must be used for projects that benefit low and moderate income households, eliminate slums and blight, or mitigate a life-threatening situation. In Bismarck, CDBG funds may be utilized for infrastructure improvements in low and moderate income neighborhoods. The City is also eligible to apply for a variety of state and federal aid programs including funding for transportation plans, airport improvements, stormwater management studies, and qualified capital improvements.

Development Impact Fees

Although development impact fees are not utilized extensively in Bismarck, they should be considered. Developers would set aside funds to be used as a portion of required future infrastructure (such as roadways, traffic signals, stormwater management facilities, etc) benefiting the property being developed. This would help eliminate the need for general taxpayer subsidy of improvements that primarily benefit specific properties.

DEMOGRAPHICS

Documents Reviewed include:

- Mobility 2017- Community Profile and Demographics
- Long Range Transportation (LRTP-2010-2035)

The following is a review of existing demographics and future growth trends.

Relevance to the Subarea Study:

Market and demographics information provides a snapshot of the community demographics and historical development trends within Bismarck.

Analysis: According to the 2010 U.S. Census, the population of Bismarck is 61,300. Bismarck is a regional center of government, health care, education, and retail. Recently, Bismarck experienced considerable population and employment growth in its northern neighborhoods, mostly due to new and expanding retail centers along or north of I-94. Between 1990 and 2010 population growth remained relatively stable, at or around 10% per decade.

Major Employers and Projected Growth

The employment breakdown of major employers includes:

- The State of North Dakota employs 4,400 people
- Sanford, the St. Alexius Medical Center, the City of Bismarck, and the United States Federal Government
- Kirkwood Mall, City/County government and Bismarck Public School
- By the year 2035 employment growth, for the most part, will be concentrated in locations outside of downtown; in areas north of I-94 and along U.S. Highway 83 in Bismarck
- Growth is also anticipated to be high in eastern parts of Bismarck, along the East Bismarck Expressway and points further east

Existing Population and Projected Growth

Population density is highest in and around downtown Bismarck. The neighborhoods surrounding the State Capitol Building and Hillside Park average 5,000 people per square mile and greater. Densities are also high both north and south of I-94 along North Washington Street, as well as in the areas to the north and south of the Bismarck Expressway, along South Washington Street. By the year 2035, the Bismarck-Mandan MPO anticipates that household growth will be:

- Highest in locations outside of already urbanized Bismarck and Mandan, to the north, east, and south of the downtowns of both cities
- The areas north of I-94 will see the highest rates of growth, along U.S. Highway 83 in Bismarck, and along Highway 1806 in Mandan
- West of Bismarck Municipal Airport along East Burleigh Avenue and South Washington Street, the areas immediately east and west of the Bismarck Expressway
- In areas in the southeast corner of Bismarck- land along both sides of the Missouri River south of I-94

**Bismarck-Mandan Area
Major Employers**

Sector	Company	Number of Employees
Government	State of North Dakota	4,400
Healthcare	Sanford Medical Center- Bismarck	3,038
Healthcare	St. Alexius Medical Center - Bismarck	2,176
Education	Bismarck Public Schools	1,714
Government	The U.S. Federal Government	1,200
Government	City of Bismarck	864
Energy	Dakota/Great Plains Gasification Plant	700
Telecommunications	Aetna	618
Education	Mandan Public Schools	572
Energy	MDU Resources Group - Bismarck	537
Healthcare	Missouri Slope Lutheran Care Center	530
Education	Housing Industry Training (HIT)	501
Energy	North American Coal - Coteau Mine	500
Telecommunications	Coventry Healthcare	475
Retail	Dan's Super Markets	470
Healthcare	Mid-Dakota Clinic	465
Energy	Basin Electric Power Cooperative	459
Education	University of Mary	435

MEETINGS/PUBLIC WORKSHOP #1 SUMMARY

Meeting Summary

Downtown Bismarck Subarea Study
Meetings & Public Workshop #1
March 13 & 14, 2013

1

Summary

The first of the Downtown Bismarck Subarea Study committee and stakeholders meetings and public workshop were held on March 13th and 14th, 2013. During that period, the consultants presided over a total of 10 one-on-one hour-long meetings with a Technical Advisory Committee, a Steering Committee, and stakeholder groups at the City/County Building located at 215 5th Street.

The Public Workshop #1 was held on March 13th from 5:30-7:00pm at the Civic Center Room 205 and was attended by approximately 30 community members.

Meetings and Workshop Purpose

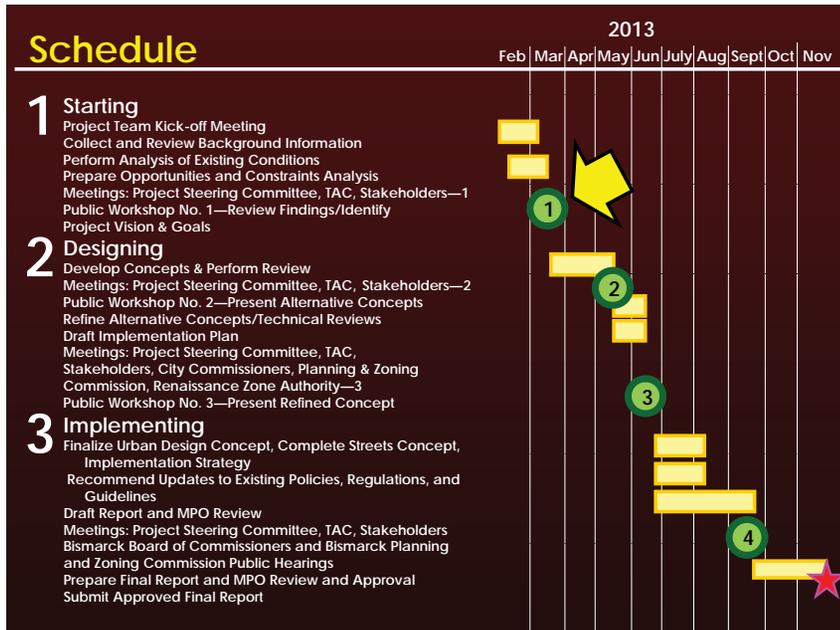
The purpose of the meetings and workshop was to:

- Inform the committees, stakeholders, and citizens about the planning process
- Obtain thoughts on key issues that need to be addressed
- Answer questions

Each meeting began with the consultants presentation of the project process and schedule (shown above right) and analysis of the opportunities and challenges of the study area. Following the presentation, attendees discussed the issues and concerns over table maps. The workshop portion concluded with table reports from each group summarizing the discussion. Written response sheets (shown below right) were provided to all with the purpose of documenting the top issues and concerns as well as the areas presenting special design opportunities.

The following pages include:

- Project Goals
- Response Sheet Tally- Issues, Concerns, & Opportunities
- Areas for Special Design Opportunities and Concern
- Consultant Identified Constraints & Opportunities



Project Process & Schedule

Response Sheet

Downtown Bismarck SubArea Study Meetings & Public Workshop #1 March 2013

1



Opportunities and Constraints

On the map above, please note areas you believe:

- Present special design opportunities
- Present design constraints

Issues and Concerns

List your top three issues and concerns:

1. _____
2. _____
3. _____

Additional Comments

Please note additional comments on the back of this sheet.
Name (optional): _____

If you need additional time to respond, please return your comments through:
MAIL: 520 SW Yamhill, Roof Suite 4, Portland, OR 97204 **WEB:** www.DowntownBismarckStudy.com/public-meetings

Project Goals

A summary of the response sheets' issues and concerns form the basis for creating the project goals.

In all, sixty-four response sheets were collected and fifteen project goals were identified as indicated below.

These goals will guide the planning and development of the Subarea Study concepts throughout the planning process.

Goal	Times Mentioned
Create a Public Square or Gathering Space	36
Ensure A Pedestrian-Friendly & Walkable Downtown	28
Develop a Downtown Parking Strategy that Maximizes Existing & Future Ramps	28
Strengthen the Downtown Shopping & Entertainment Experience	25
Increase Downtown Housing	17
Introduce More Greenery into Downtown Streets	14
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Response Sheet Tally - Issues, Concerns, and Opportunities

concerns, and downtown opportunities mentioned in the response sheets collected from the committees and stakeholder meetings, Public Workshop #1, and online submissions via the project website. Comments are grouped under the over arching project goal.

Create a Public Square or Gathering Space

1. Steering, Technical and Stakeholder Meetings

- Green space- Centralized location is key (lot south of City/City building)
- Open space- if I live downtown I need a place for dog
- No space downtown- how it was designed
- Establishing a public gathering space-focal point
- Green space: places to gather, relax, enjoy time- Beautiful space!
- Downtown Plaza with performing arts space
- The need for a new gathering green space in downtown- downtown 'plaza'
- Lack of Green space, trees
- Need a destination park in downtown
- No green space/outdoor activity space

2. Public Meeting

- Green space/town square
- Civic square
- Civic Plaza on 5th street – Mall on side and Heritage center on other
- City plaza/square- green space that may be highly programmed
- Civic Plaza
- Plaza
- Civic square= do a kid play fountain
- Plaza at Broadway- 5th and 6th street
- The community space;/living room// benches
- Turning the depot lot in to a plaza would be beautiful but strain on parking which is already distressed (would impact older customers
- The plaza– absolutely needed, everything will build around this! (5th and Broadway)

- Need city plaza that attracts all demographics
- Green space/gathering/fountain
- Public green spaces/pedestrian ways/gathering spaces
- Park at 5th to 6th on Broadway
- Park/plaza at Fiesta Villa

3. Online Submissions

- Green space-2
- Green space in the downtown; there is no pet place; Enjoy the outdoor while enjoying the downtown; obviously space is an issue so I think closing 1 street to get that green space
- No green space for people to gather
- No place to gather for outdoor events
- Central gathering place
- The parking area across from the Belle Mehus and World War Memorial buildings would make a PERFECT green space/Plaza/city center.
- While I hear we need "green space" we just don't have the space for it anywhere. Feels like downtown is coming alive again, love it.
- A central gathering space to visit neighbors, "hang out," listen to music, connect community
- A fountain! Specifically, a cool one that you can sit around the edge on a bench (the fountains wall), like one in Europe!

Ensure A Pedestrian-Friendly & Walkable Downtown

1. Steering, Technical and Stakeholder Meetings

- Improved walking routes- features to encourage walking and meeting downtown
- People don't walk 'as the crow flies' or in a radius. Need to consider a walk signal and no waiting at intersections- this is a BIG deal in winter!
- How to widen the streets to accommodate more activity
- Focus on pedestrian transportation- BOTH walking and biking (expand to river)
- Lack of complete streets- need safe routes for walkers & bikers
- As a retail owner who's many customers are younger people, I have concerns about safe walking and biking routes into the downtown area.

Response Sheet Tally - Top Three Issues and Concerns (Cont.)

- Lack of ambience/look & feel throughout CBD; only in Chancellor square does a pedestrian feel like they are 'downtown'
- Coordinated snow removal from sidewalks and streets; need a Business Improvement District

2. Public Meeting

- Make Main and Rosser pedestrian friendly
- Improve streetscape/infrastructure
- Our streets and sidewalks are awful: cracks and broken sidewalks
- ADA/children accessibility
- Pedestrian comfortable areas, sidewalks, crossings
- We need beauty on the streets- fixed sidewalks will help
- Walkability
- Eliminate the parallel parking on 4th street (east side): Widen sidewalks to include benches, include place for families
- Added pedestrian transportation, especially walking the entire ½ mile radius
- People places and one character-pedestrian friendly

3. Online Submissions

- Like the idea of a corridor along the railroad tracks; I can see that it could diverge from the tracks near the Bank of ND and to the river there. Not only is this ideal for walkers and bikers, but perhaps there could be a QUIET motorized transit to and from the zoo and downtown. I think an artist's drawing of how the corridor can be protected from the train is needed, for when I mention this to people they have this idea of the train taking someone's arm off!!
- Can a tunnel of sorts be built into the CC renovation? I can imagine this tunnel going under Front Avenue not only to avoid pedestrian vs. vehicle traffic, but for protection from the elements. Perhaps an escalator could bring the people up on the north side of Front but south of the tracks, into a building with retail and coffee, or into a new parking ramp between the tracks the building on Fifth and Front (BSC Nursing?). In addition to the previous reasons, this walk from the CC to downtown is uphill and this tunnel/escalator idea would really help with that concern.

- Have special road signage, light poles and stoplight infrastructure for the downtown area, making a classy and warm feel. Similar to downtown Albuquerque.

Develop a Downtown Parking Strategy that Maximizes Existing & Future Ramps

1. Steering, Technical and Stakeholder Meetings

- Parking-3
- City owned lots aren't open to public parking after 5pm
- Parking enforcement: huge concern in the downtown area
- Parking other than surface parking
- Trend of surface parking
- Lack of parking

2. Public Meeting

- Free up space by building parking ramps
- Parking ramp usage
- Street parking time limitations of 90 minutes does not allow time to use/get services therefore patrons do not visit retail stores for services/ food etc. longer than 90 minutes
- City parking lot not paved behind the Radisson
- We have less of a parking issue than a willingness to walk issue
- Diagonal parking on 5th street
- Need more diagonal parking
- Concrete jungle surface parking (current situation)
- Parking safe and simple
- New parking ramp on 6th and Thayer- STOP
- Parking is NOT an issue that will ever be solved...maybe add public transportation if people can't walk!
- Parking- street and ramps
- Parking ramp use should be encouraged
- Snow/cold discourages use of ramps to the degree found elsewhere
- Parking ramp at 5th and Ave A

Response Sheet Tally - Top Three Issues and Concerns (Cont.)

3. Online Submissions

- PARKING!!!-2
- The gravel parking lot/garbage collection area between the Radisson and Main Ave has a ton of potential. Right now that is the worst!
- Parking is an issue
- Ramp parking is an issue
- Diagonal parking

Develop a Downtown Parking Strategy that Maximizes Existing & Future Ramps cont.,

- Wish they would do a ramp between the 2 hospitals too, walkways to both. Space is there. Nice that the new building on Main is connected to the ramp, every time we go in that ramp it is almost full now, what will it be when the building opens.
- Like Pine's building with underground parking, we need more of that.
- Parking remains a problem
- PARKING! What about \$5 valet service? The parking garage.. although I haven't used it much I remember the walk down in heels was treacherous, it ends up in the alley... which had mounds of snow that I had to climb over to get to the sidewalk. Hopefully the new walkway will take care of that.
- Parking ramp on 5th and Broadway Ave known as the Parkade Ramp. I sit on the Parking Authority and we feel the need to upgrade this ramp to make it a 24/7 usage ramp. We are in the process of updating the lobby, stair tower etc for just under 1M. I believe and we have a concept of building an outside stair tower on the fifth street side or NW part of the building. Estimated cost as I remember is 2.5 to 3 Million to do this. This would allow 24/7 access to the ramp and high visibility to the service of parking. The issue is encroachment on the sidewalk, funding the project and timeline with all that is going on in downtown right now. This is an FYI
- A parking ramp should also be constructed west of the Civic Center. The ramp could service a dual function of event parking for the civic center.

- New Parking ramp design: Make it to help with these goals- a unique, friendly design that has other functionality to it i.e. a Transit Station or a main level shopping

Strengthen the Downtown Shopping & Entertainment Experience

1. Steering, Technical and Stakeholder Meetings

- Need more retail- entertainment to attract all generations to want to come downtown
- Parking ramps: retail on ground level
- Strengthen shopping experience
- Rehab of existing buildings
- Make downtown an experience

2. Public Meeting

- Downtown retail growth
- We need more street activation! Dining!
- Our city leadership needs to be more open to change. They don't want sidewalk dining and are resistant to change. Bismarck is on a threshold and it needs to move forward now or it will forever be left behind!
- Outside eating is illegal
- Sidewalk/Outdoor dining-2
- Retail/zoning- no more banks and furniture stores; need book stores, coffee shops, restaurants, markets
- 6th Street Parking ramp with retail level- it's not too late to update plans!
- Have 6th street new ramp include retail spaces below
- Trans corridor at 5th and Broadway has opportunity for moving people across town and be desirable retail zone; various stops would shorten the walk
- Family-friendly retail- bookstore (old/new)
- Retail/grocery store
- Empty storefronts not appealing- more attractive charm

Response Sheet Tally - Top Three Issues and Concerns (Cont.)

- Storefront improvement example project to demonstrate what we can have
- Parking ramps that don't look like parking ramps- 1st level business 3. Online Submissions Issues and Concerns
- It would be nice to get the office businesses off of the bottom floor of the downtown buildings turn into retail space

Increase Downtown Housing

1. Steering, Technical and Stakeholder Meetings

- If I live downtown where do I get groceries- shop?
- Housing-2
- Housing opportunities- not 'program' residents

2. Public Meeting

- Housing-7
- Living quarters
- Patterson building as more than 60 affordable housing
- Development of lot north of post office "Row houses"

3. Online Submissions

- Increase of residential units in the downtown but parking is an issue for sure
- Love the idea of more downtown living Introduce More Greenery into Downtown Streets

Increase Downtown Housing cont.,

- A CORNER STORE!!! with basic needs such as groceries and cleaning supplies

Introduce More Greenery Into Downtown Streets

1. Steering, Technical and Stakeholder Meetings

- Green spaces and planting areas for trees and landscape areas
- Common thread for downtown, streetscape, trees, benches, garbage receptacles

2. Public Meeting

- Green spaces

3. Online Submissions

- Green spaces and natural features
- Putting planting containers into the alleyways, a great way to make them more friendly
- Too much concrete, not enough green! It would be great to have more trees and natural elements
- Downtown could use areas with small flower gardens or trees along the street to add some atmosphere
- Beautifying the downtown
- The city needs a way to make sure our west downtown area (2nd and west) is a greener area where people can relax, specifically on the south sides of roads, avoiding sun and requiring less work.
- Pocket Parks!

Encourage Biking Downtown

1. Steering, Technical and Stakeholder Meetings

- BOTH walking and biking (expand to river)
- Lack of complete streets- need safe routes for bikers
- As a retail owner who's many customers are younger people, I have concerns about safe walking and biking routes into the downtown area
- Bike lanes and more bike racks

2. Public Meeting

- Bike lanes
- Bike parking
- Bikeway- protected/designated
- Bike traffic not too important- not a college town

3. Online Submissions Issues and Concerns

- Safe bike routes

Response Sheet Tally - Top Three Issues and Concerns (Cont.)

- Want to be able to walk and bike safely
- Walking, biking inclusion in plans
- Bike Racks that have a unique tie in to the block, or building they are in front of!
- Posters promoting biking downtown, with bike lanes on EVERY STREET!

Improve Traffic Access In & Out of Downtown

1. Steering, Technical and Stakeholder Meetings

- Traffic flow- getting people in and out of the downtown area
- Access and traffic patterns
- Access to the City center by people from rural communities
- Traffic flows north/south not east/west

2. Public Meeting

- Continued arterial roadway connections
- Traffic speeds unfriendly to pedestrians
- Traffic flow
- Connections to downtown from outside downtown- transportation, etc
- Main Avenue competing as both east/west traffic hub and pedestrian retail

3. Online Submissions

- Improve traffic flow
- I think the parking area to the south of the Civic Center and the parking area to the north side of Kirkwood Mall, along with the cross street in between is really a congestion. It doesn't flow well. The one way travel on 7th limits your options to exit to the north. And you have cars passing through the mall parking lot to go from 3rd street to 7th street sometimes. It just seems like there is room for improvement to this area
- Making a nice entrance to downtown coming from the Memorial bridge on main. (Trees, Signs. Adding an LED Hue system on that pedestrian overpass would be a nice touch, because it gets dark there

Preserve and Enhance Historic Buildings

1. Steering, Technical and Stakeholder Meetings

- Maintain our historic surroundings

2. Public Meeting

- Need better historic preservation
- Protection of historic building
- Preserving/highlighting historic buildings/areas
- Repurpose Patterson

3. Online Submissions

- Keep it pretty & cool looking. Preserve the historic buildings. Encourage any buildings to maintain the atmosphere and style of current downtown.

Develop a Downtown Multi-Modal Transit Hub

1. Steering, Technical and Stakeholder Meetings

- Developing a transit hub
- Ability for transit Bus's having access through downtown

2. Public Meeting

- Maybe 2 buses that do loops
- Transit
- Multi-modal transit/walking/parking

3. Online Submissions

- Transit is an issue: two way streets
- A transit hub should be constructed west of the civic center. A parking ramp should also be constructed at this location. From this hub workers in the downtown could park during the day and take shuttles to the places of employment such as the medical centers.

Response Sheet Tally - Top Three Issues and Concerns (Cont.)

Improve the Linkage between Downtown, the Civic Center & Kirkwood Mall

1. Steering, Technical and Stakeholder Meetings

- 5th street corridor
- 5th Street- Not a thru street- slow traffic down
- 5th and Broadway- main connector to Government, shopping, medical
- Traffic flow up 5th street
- Railroad safety- flow north to south to connect to Civic Center, Mall, New development
- Concrete street pavement deterioration; need a street rehab project (5th south and north)

2. Public Meeting Issues and Concerns

- Incorporate Civic Center and shopping mall with downtown
- Connectivity between shopping center, civic center, downtown and Hotel/arts/medical
- 5th street corridor from Kirkwood to downtown

Incorporate Public Art into Streets & Public Spaces

1. Steering, Technical and Stakeholder Meetings

- Downtown arts center for 12 month use
- Public art
- Display of public art

2. Public Meeting

- Art murals- eliminate the horrible scary alleys

3. Online Submissions

- An "art wall" where a side or back of a building is open to the art of graffiti, or a brick show where kids or artists can paint a brick for fun, with a storyline/plot
- Re-Painting of murals on the Anderson Building
- Street Lamp Knitting

- A park that can be brought in! like a deck on wheels that can move around (they have them in Portland, and maybe San Francisco)
- Installing LED lights on the 7th, 9th, and Washington underpasses that change colors, and make it friendlier for pedestrians Google the Albuquerque NM Underpass, it'll stick out! its the first choice, and it cost the city barely nothing since they'll never burn out!

Convert Chancellor Square to Two-Way

1. Steering, Technical and Stakeholder Meetings

- Correct chancellor square
- Changes to the one-way street system in Chancellor square

2. Public Meeting

- How friendly are one-way traffic streets in downtown especially partial blocks?

3. Online Submissions

- Opening Broadway to 2-Way traffic, indicated on Leaf Design Studios Website

Identify Opportunities for New Downtown Development

1. Steering, Technical and Stakeholder Meetings

- Essential Services
- Opportunities for growth in business development
- Mix uses- balance of retail, educational, health, greenery, economic development, arts (year around)
- We need more diversity mixing retail, and housing

2. Public Meeting

- New development west of civic center
- We need to reduce the open space between public functions. We as a region are so hung up on parking at the front door of everything we restrict usable space for businesses, residence and public gathering space.

Response Sheet Tally - Top Three Issues and Concerns (Cont.)

3. Online Submissions

- Do not let parking concerns/limitations be the main deterrent to progress and making the downtown area more appealing. Nothing wrong with walking a bit, especially if less parking lots mean there will be more shops/places to eat/nightlife/greenspace to offer people.
- Increase of multi-use buildings in lieu of surface parking lots

Locate a Downtown Trolley Route

1. Steering, Technical and Stakeholder Meetings

- Possibility of a historic trolley going through town
- Trolley connectivity

2. Public Meeting

- Trolley system

3. Online Submissions

- Trolley or shuttle from Kirkwood mall, downtown hub, BHS, and the capitol.
- I really want the streetcar.

Other

- The parking area across from the Belle Mehus and World War Memorial buildings would make a PERFECT green space/Plaza/city center. With the activities that go on in those two buildings to have that green space instead of a parking area would compliment that whole area. Plus Urban Harvest's open air Market could move off of the corner or 4th and Broadway and set up there. Farmer's Market people could have an area on their days to vend all summer and would be a local 'grocery' suppliers for the summer. I was in Denver and they had a city center right outside the Denver Art Museum and at lunch time all sorts of entertainment would set up and food trucks would pull up and tables set up and it was really nice to have that. There was some grass on the outside area of that where one could lay out a blanket and eat under the shade of the trees. This idea would go well in that spot. Parking for those events could be done in the Parking ramp that is by the Hotel there just across the street. Please look into this area and think about my idea.

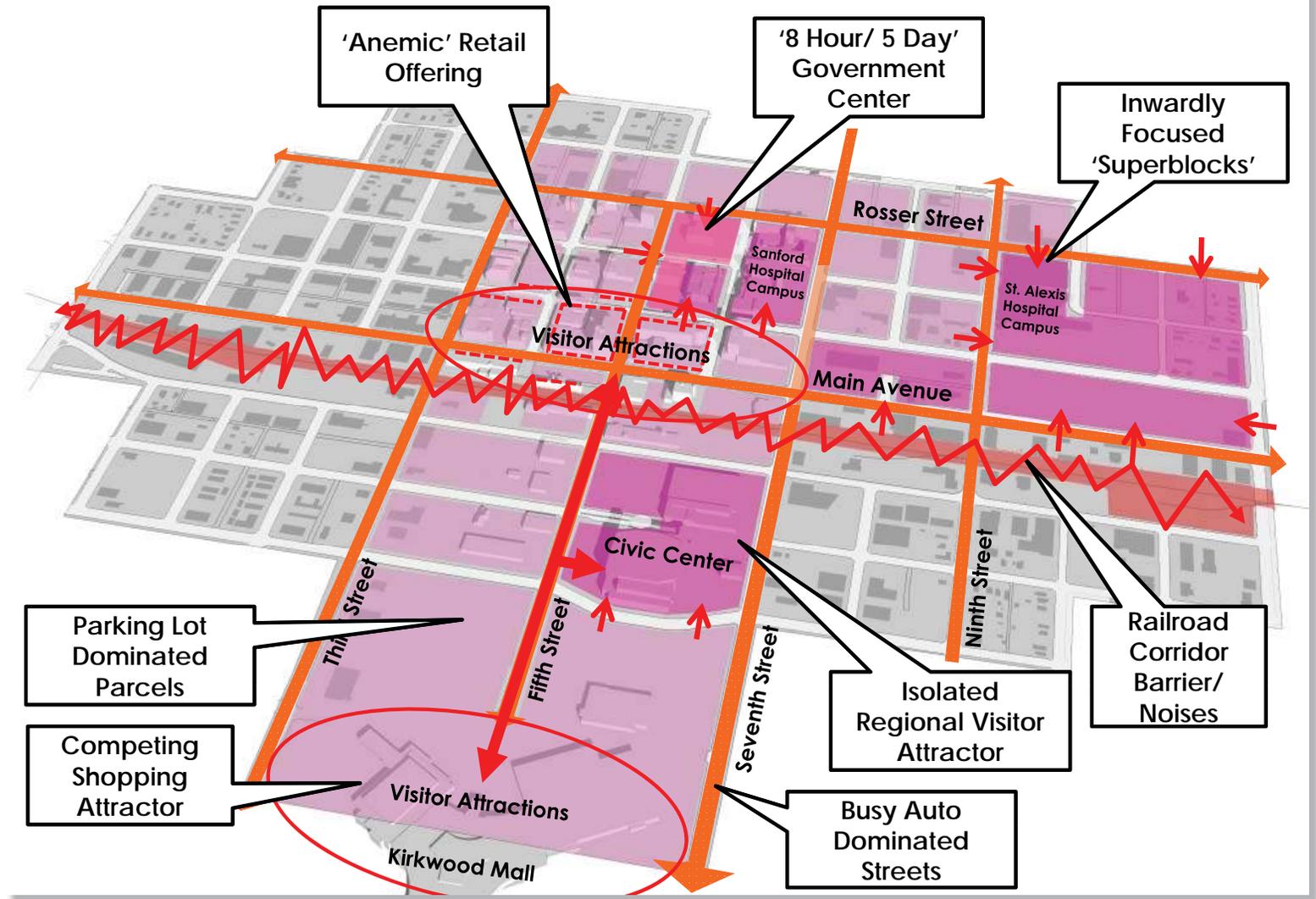
- See four anchors (golden cross) running N/S and E/W
- Skyway from civic center to ramp and medical will provide access to shopping
- The train horn noise- quiet rail is coming
- Quiet rail will be great
- Railroad quiet rail
- Bismarck needs to link downtown to the Kirkwood Mall with a pedestrian corridor. A transit hub should be constructed west of the civic center. A parking ramp should also be constructed at this location. From this hub workers in the downtown could park during the day and take shuttles to the places of employment such as the medical centers. The medical centers could then utilize their surface parking spaces for expansion of services in lieu of moving to the fringes and decentralizing the community. The ramp could also service a dual function of event parking for the civic center.
- Implementation- how to make sure the plan comes to life
- Meeting with the Downtowners and using our group as much as possible
- We as the Downtowners Association would love to meet with you as a group
- Recommendations for the re-use of the World War Memorial Building
- Adequacy of our infrastructure (water, sewer, storm sewer)
- Water supply infrastructure
- Balance need for parking, pedestrians, and through traffic
- Public Safety-2
- Security in the area- Bike patrol etc, public safety
- Utilization of the Civic Center and Exhibit Hall
- We need a coordinated downtown business owner by-in
- Remove/bury power lines/substation!
- A tie to the Missouri River
- Compatible design guidelines with existing character

Response Sheet Tally - Top Three Issues and Concerns (Cont.)

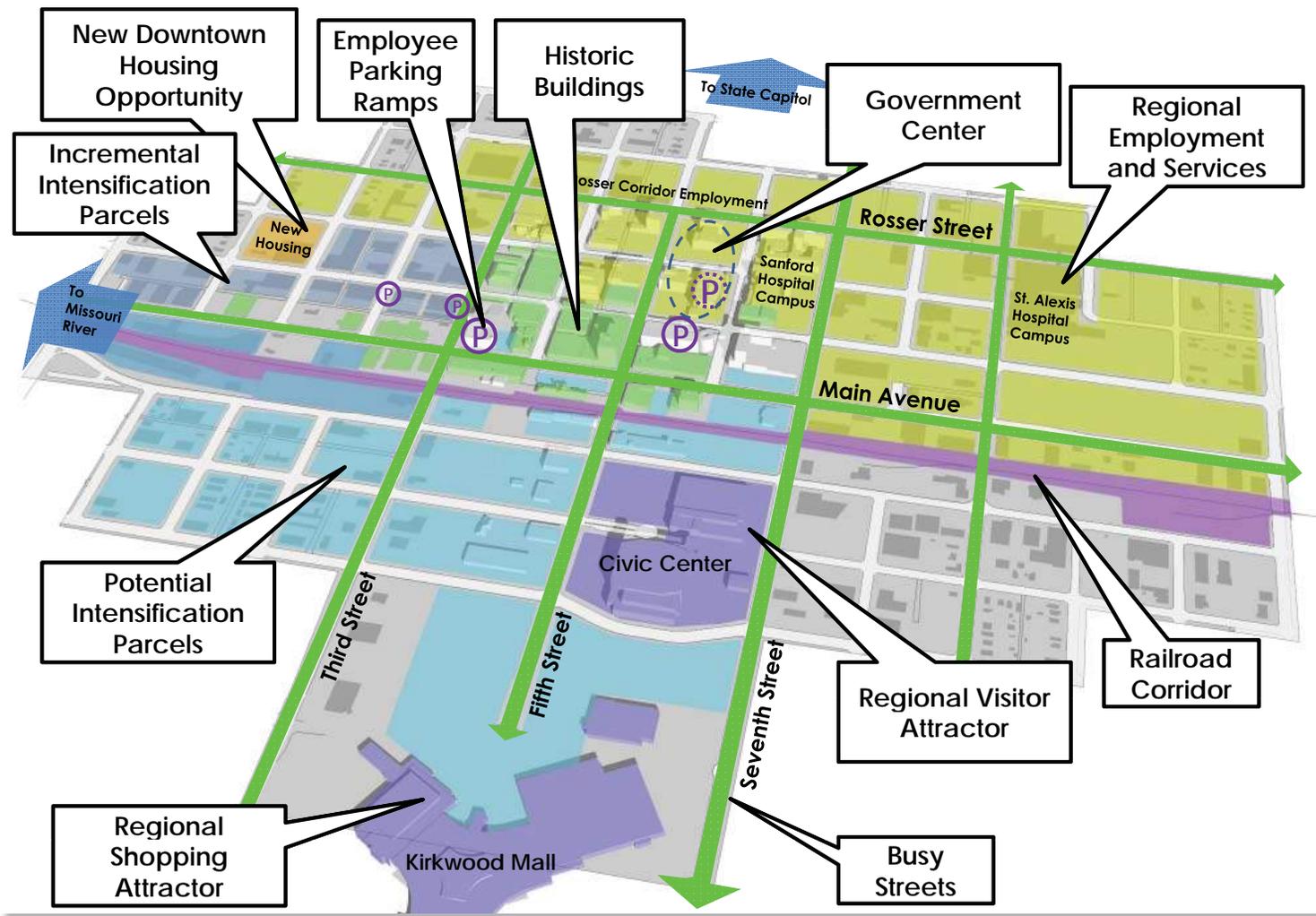
- City invests in downtown with infrastructure changes- snow removal etc.
- Meet with the Downtowners Association- we talk and encourage downtown to change perception of this community. Currently many believe downtown is highly subsidized. That is not the case. Rather we need community buy-in and education so the masses realize how crucial a healthy downtown is for the entire community: no downtown or unhealthy downtown= dying city
- Vibrancy constantly- this is our mission and enjoyment
- Strong communication of vision
- Kid friendly
- Events to attract people to downtown
- Concern halfway house on 7th between main and Broadway
- Attract younger demographic to live downtown
- I would like to take the time here to say that I'm so happy that the City Council Members are a group of hip visionaries although your silent sometimes, having heard what the consultants had to say was VERY encouraging and I hope that everyone sees what they see and what can be done in Bismarck to make Bismarck more awesome than it already is. I'm all for the Downtown, I live up 5th Street so I walk to Downtown. So happy you have decided to keep the Civic Center project going.
- Signage (LED Underpasses and special road signage for downtown)
- The close proximity of the downtown, civic center and Kirkwood Mall Shop Center provide the city with a central core that needs to be expanded and enhanced.
- Police patrolling on foot or by bike in good weather months
- Downtown is becoming trendy and cool (for the lack of a better word). Encourage this.
- Thank you for your efforts! Keep moving forward!
- Have Re-Parking not mandatory for food trucks and portable boutiques, or a No parking area from say 11:00 AM to 1:30 PM for food trucks
- Parking ramp with "Green Roof" and Transit Hub underneath the 2nd layer (one side have main floor parking, and when the ramp floors start moving upward, a bus (or Streetcar) station, with a raised curb for easier access unto the bus or STREETCAR) as you can tell I really want the streetcar.
- Re-Done Library parking situation, tying the library plaza into a small farmer market
- Wiring Custer Park so it is suitable for performances (The performer can talk as loud as possible and still not be heard)
- Using the train park (with the church and old train) as a farmers market area (I guess there could be a travelling one that goes on during urban harvest)
- Re-Purposing the World War Memorial Building into an event center, with skylights over the main gym, one main floor court with local-team colors all around, and adding people friendly bleachers. This would open that building to be used in its original light.
- Downtown Park and Ride. A downtown park and ride would allow workers to park in a large area, say, Kirkwood mall, and hop onto a transit bus. The bus would need to have A higher curb, lessing the need for the busses to kneel, a shelter with a heater that is only on when a button is pushed, and possibly adding a future permanent transit system, similar to a trolley or trolley bus. Fun fact-Trolley busses are built with onboard batteries, so electrical infrastructure would not be as much of an eyesore. For example, In Seattle, the electrical cables hang between stoplights. So front and 5th could have electrical from one stoplight, across the intersection and to the other. The busses sustain enough power to move with the batteries. The busses really would get power when they are at stops, say a kirkwood mall, downtown hub, BHS, and a capitol. A capitol stop would help by allowing busses only to go under the steps, controlled by a hydraulic pole that raises and lowers. The busses would be an appealing option because the traffic is growing and the capitol parking is getting worse, especially around legislative meetings.

Constraints and Opportunities

Consultant identified Subarea Study constraints and opportunities are identified.



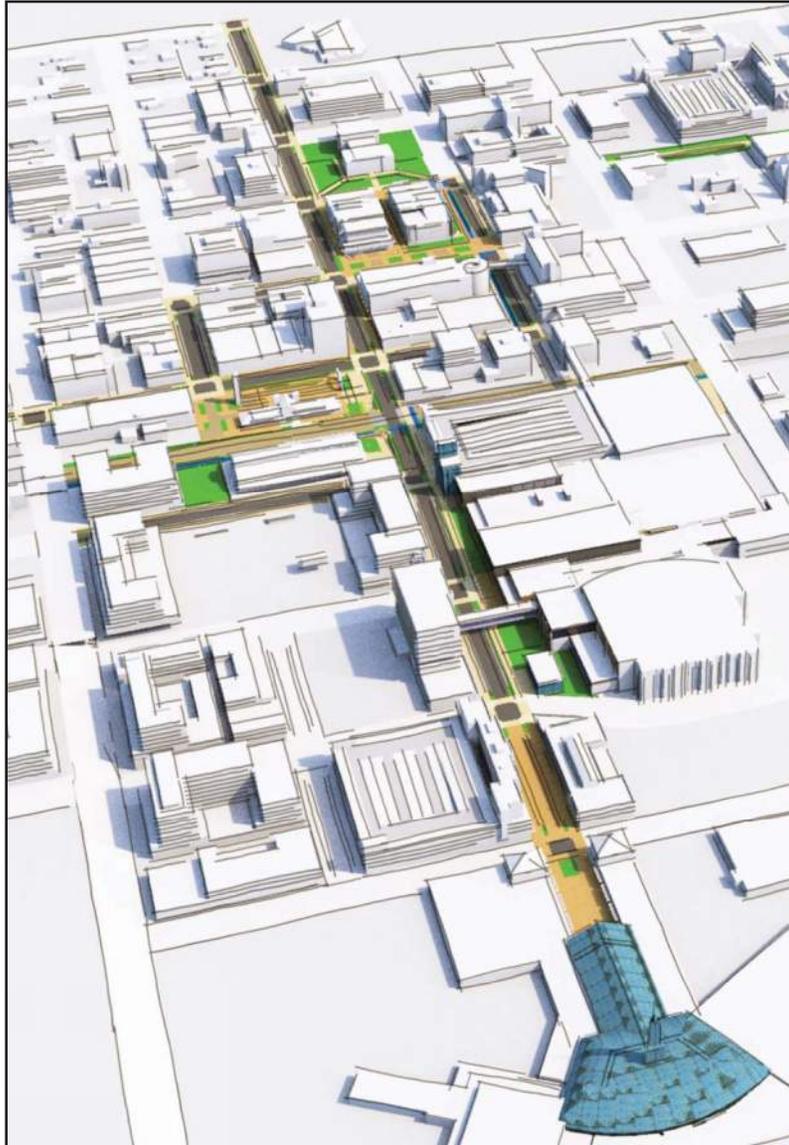
Key Study Constraints



Key Study Opportunities



CONCEPTS AND DESIGN ALTERNATIVES



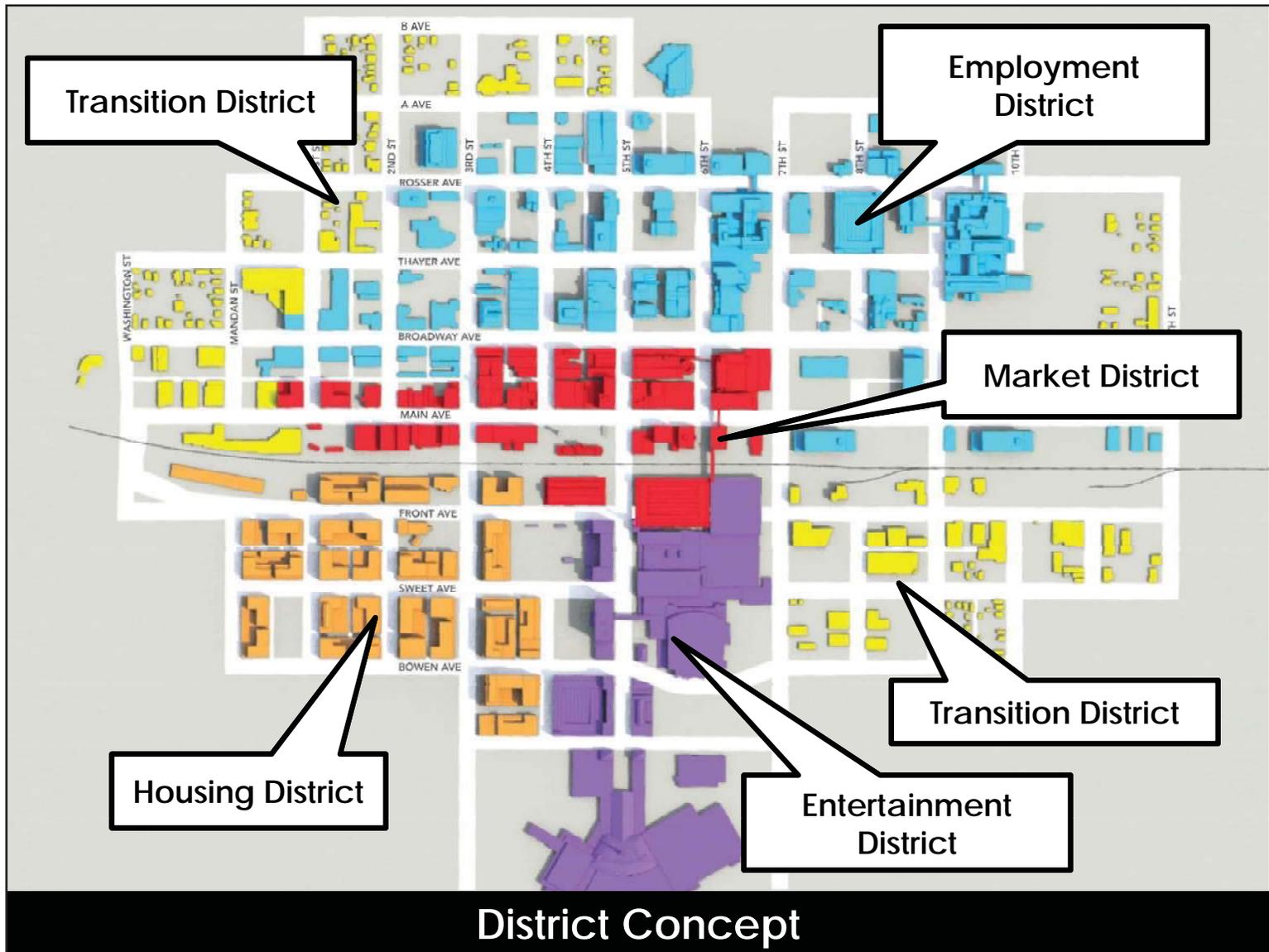
Downtown Bismarck Subarea Study

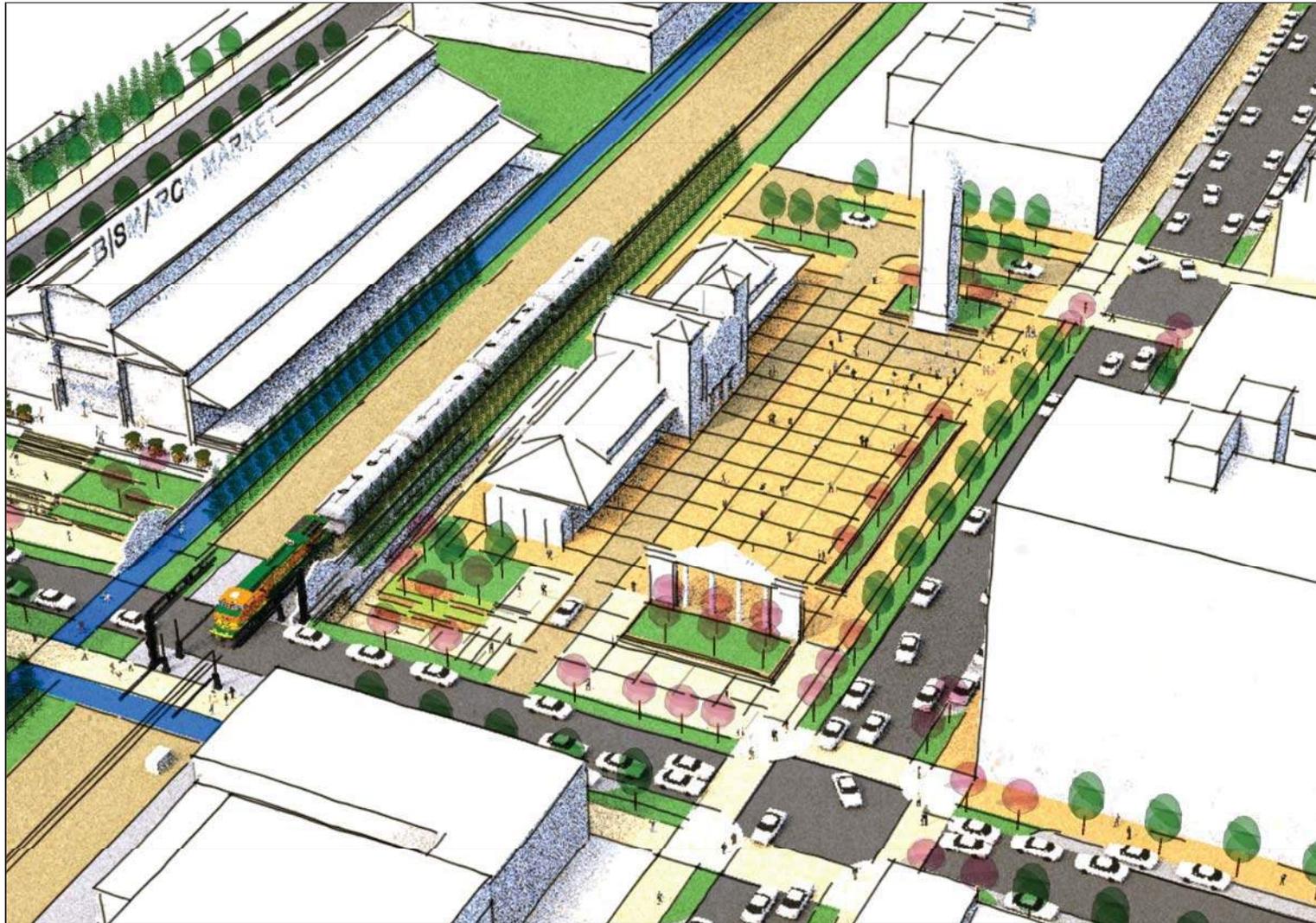
Bismarck-Mandan MPO
City of Bismarck, North Dakota

MAY 23, 2013

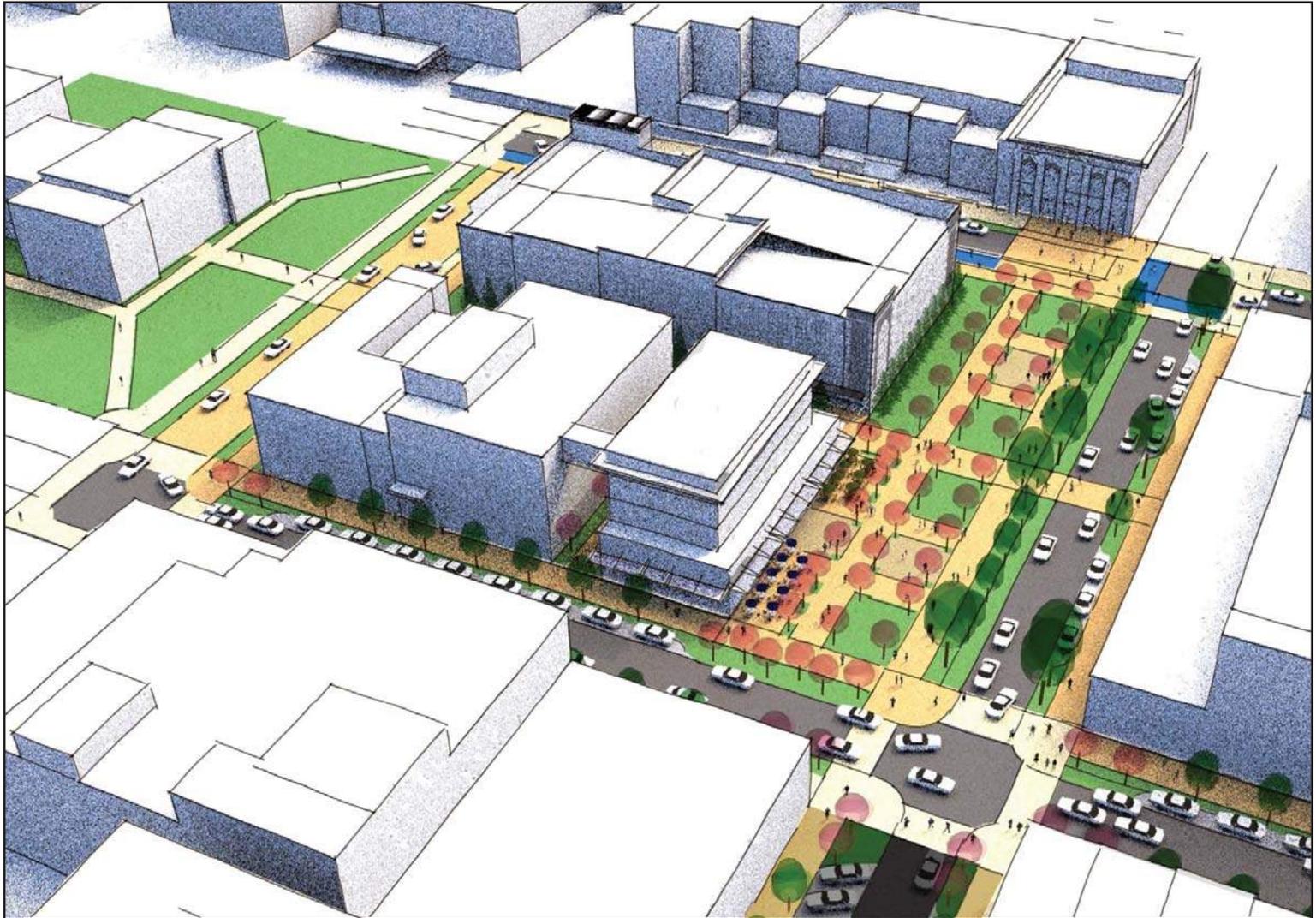
CRANDALL ARAMBULA
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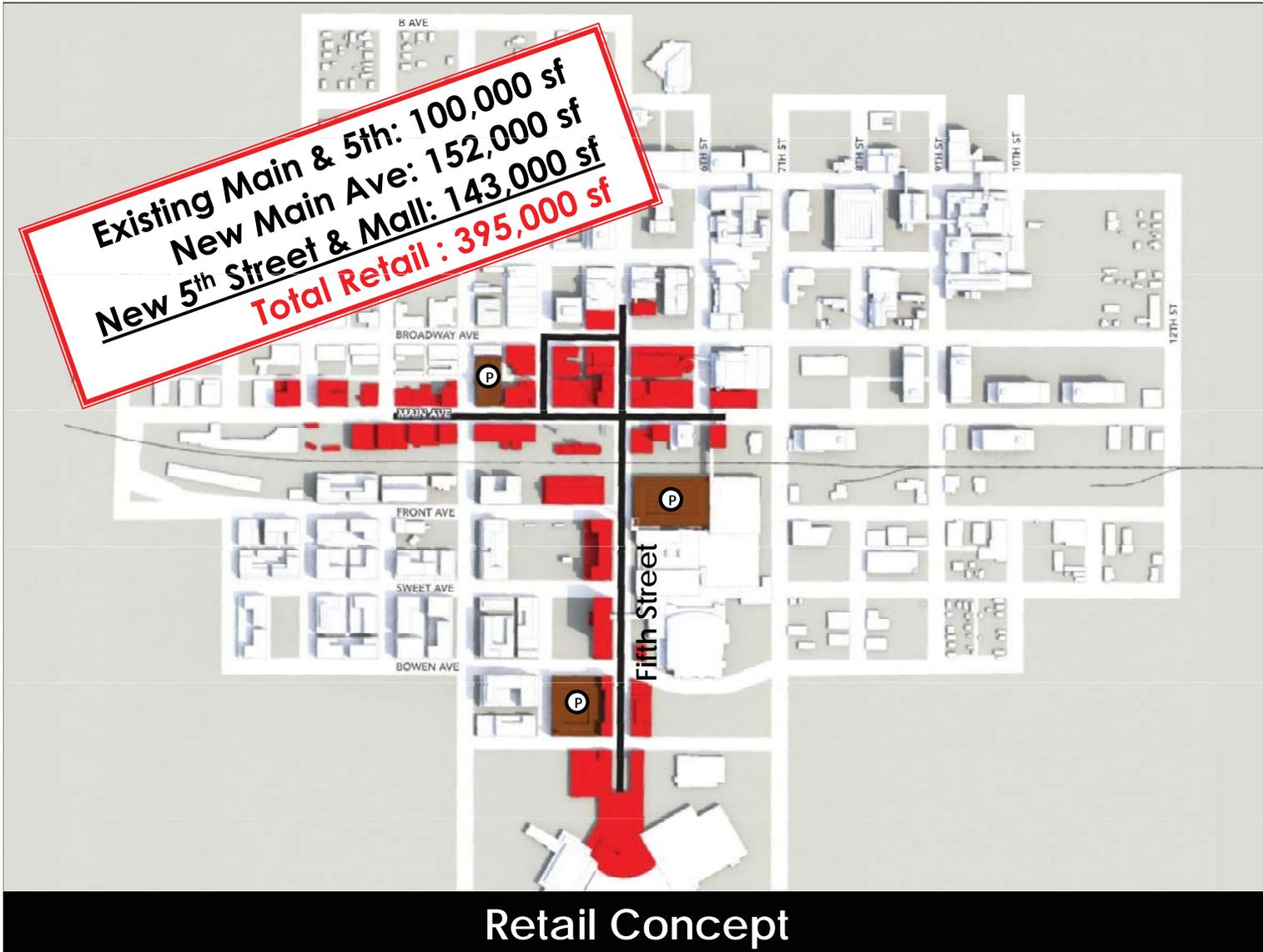


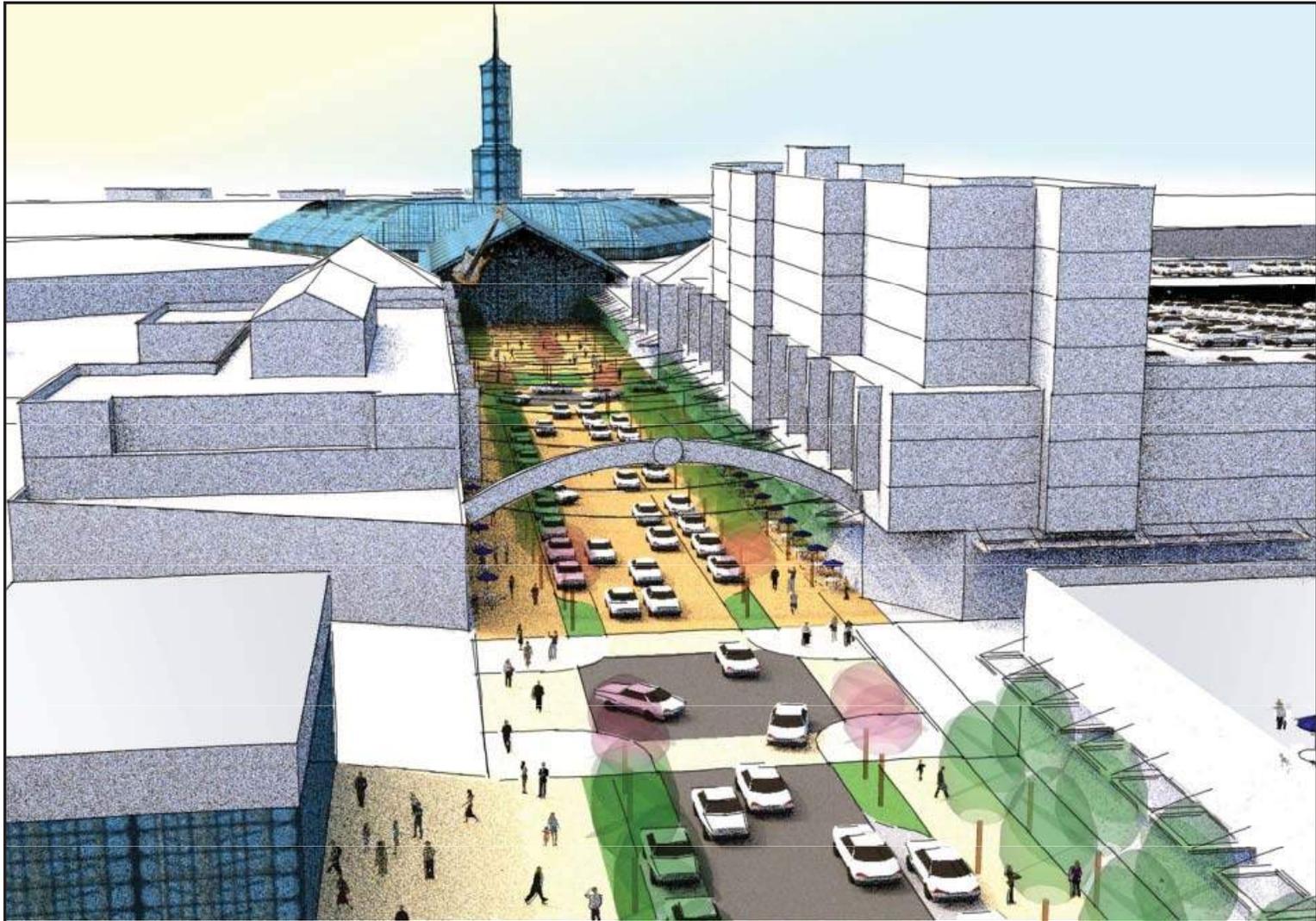


Depot Plaza

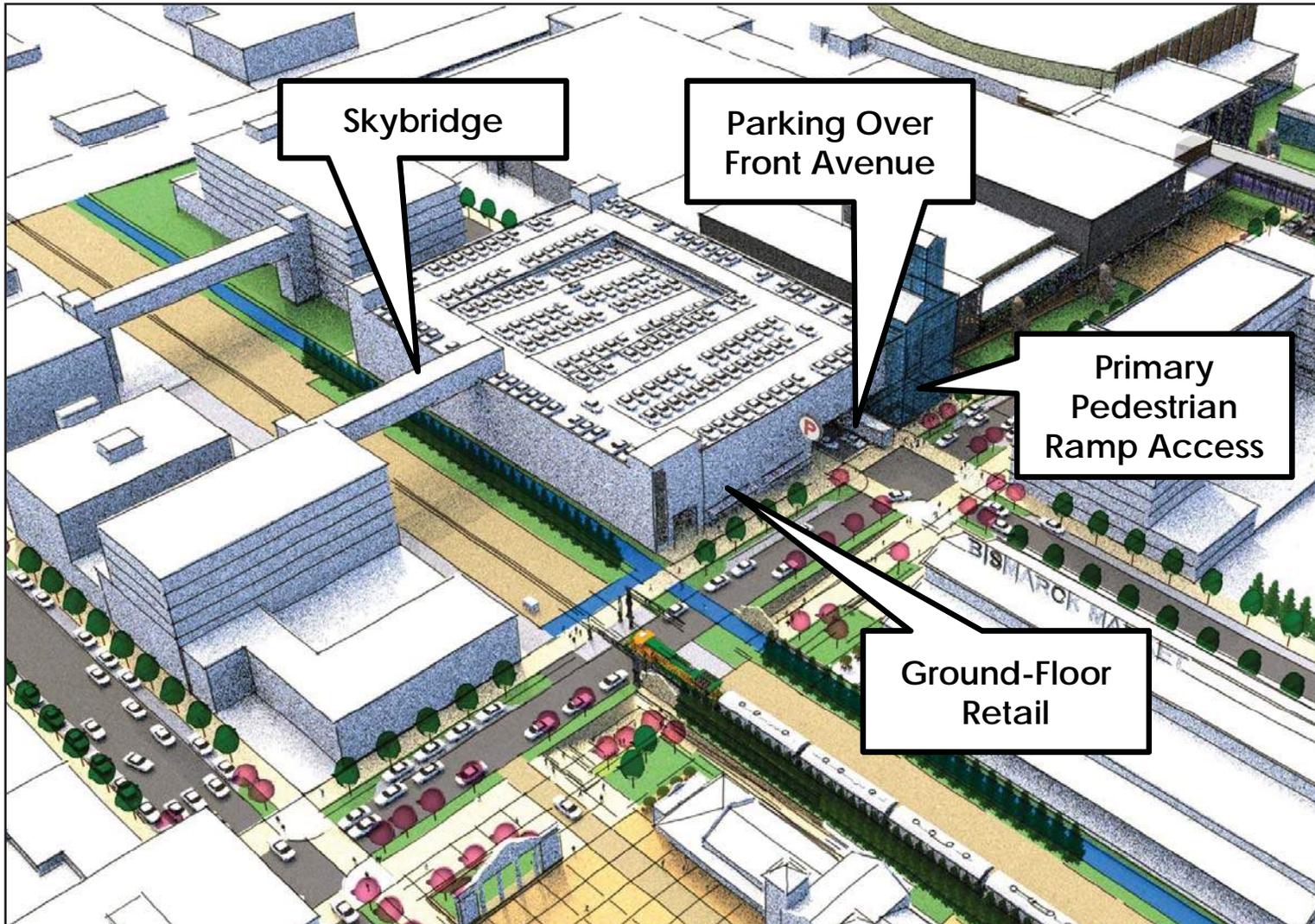


Mehus Commons

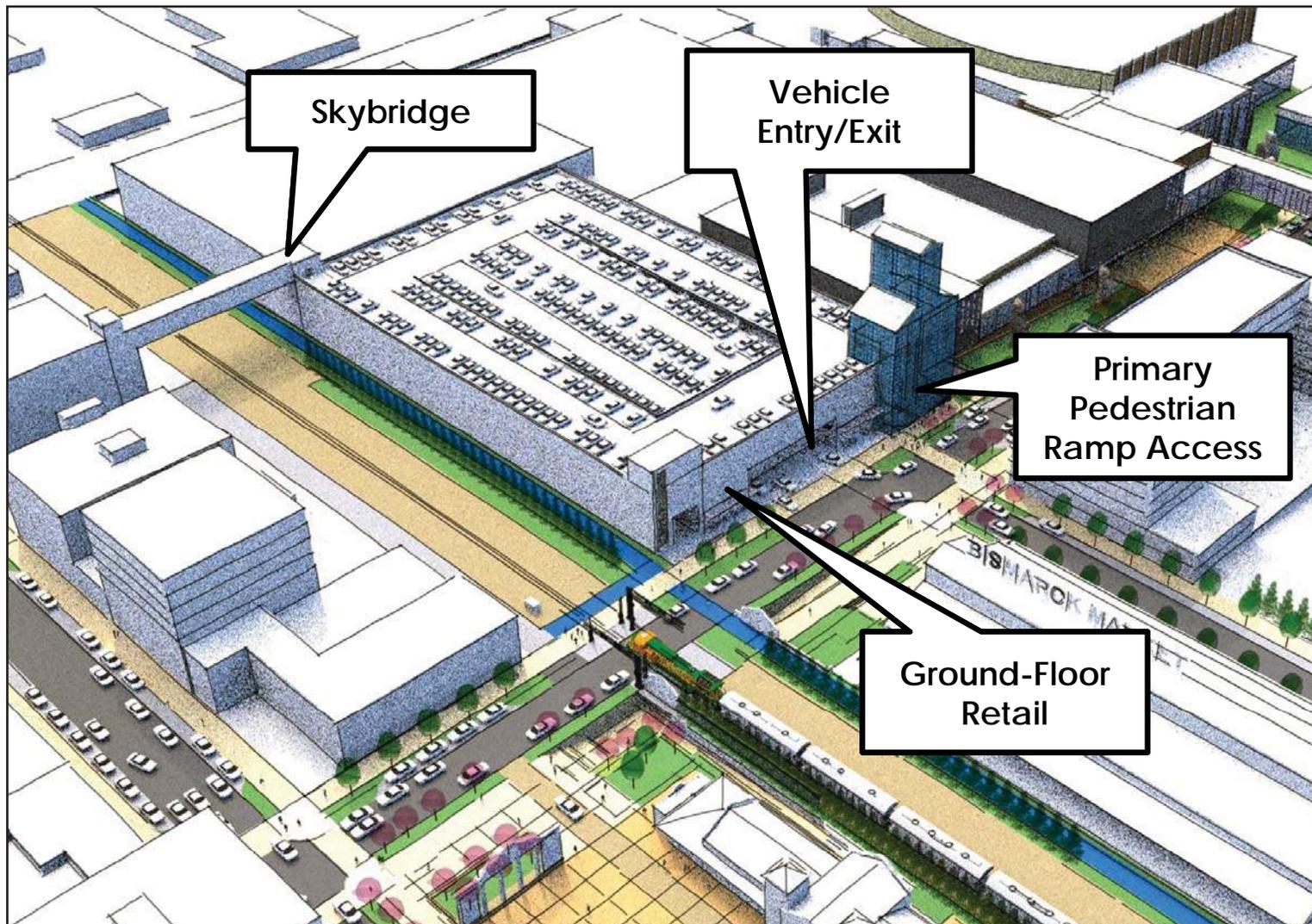




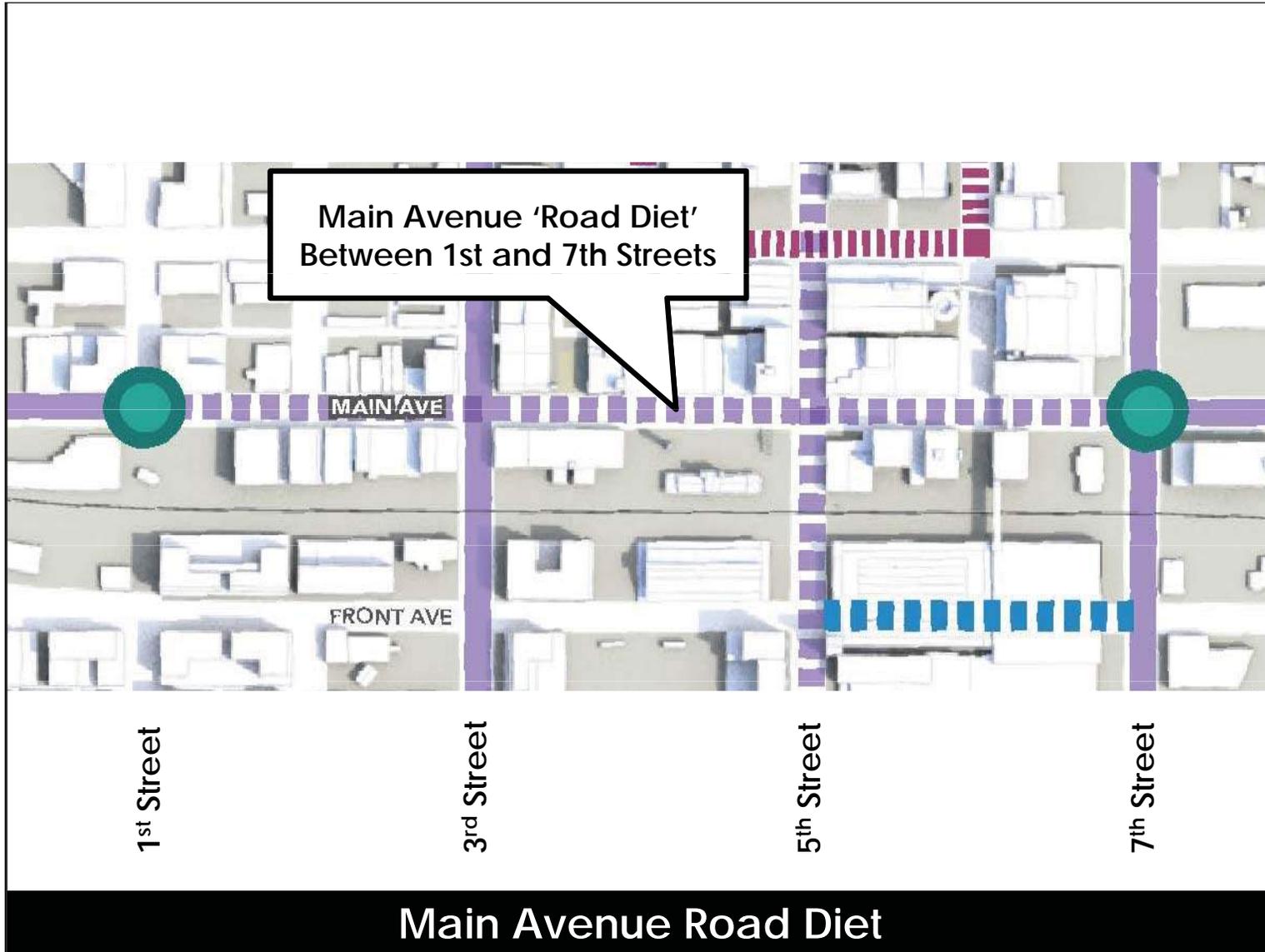
Retail Concept



Retail Parking Ramp (Alternative 2)



Retail Parking Ramp (Alternative 3)

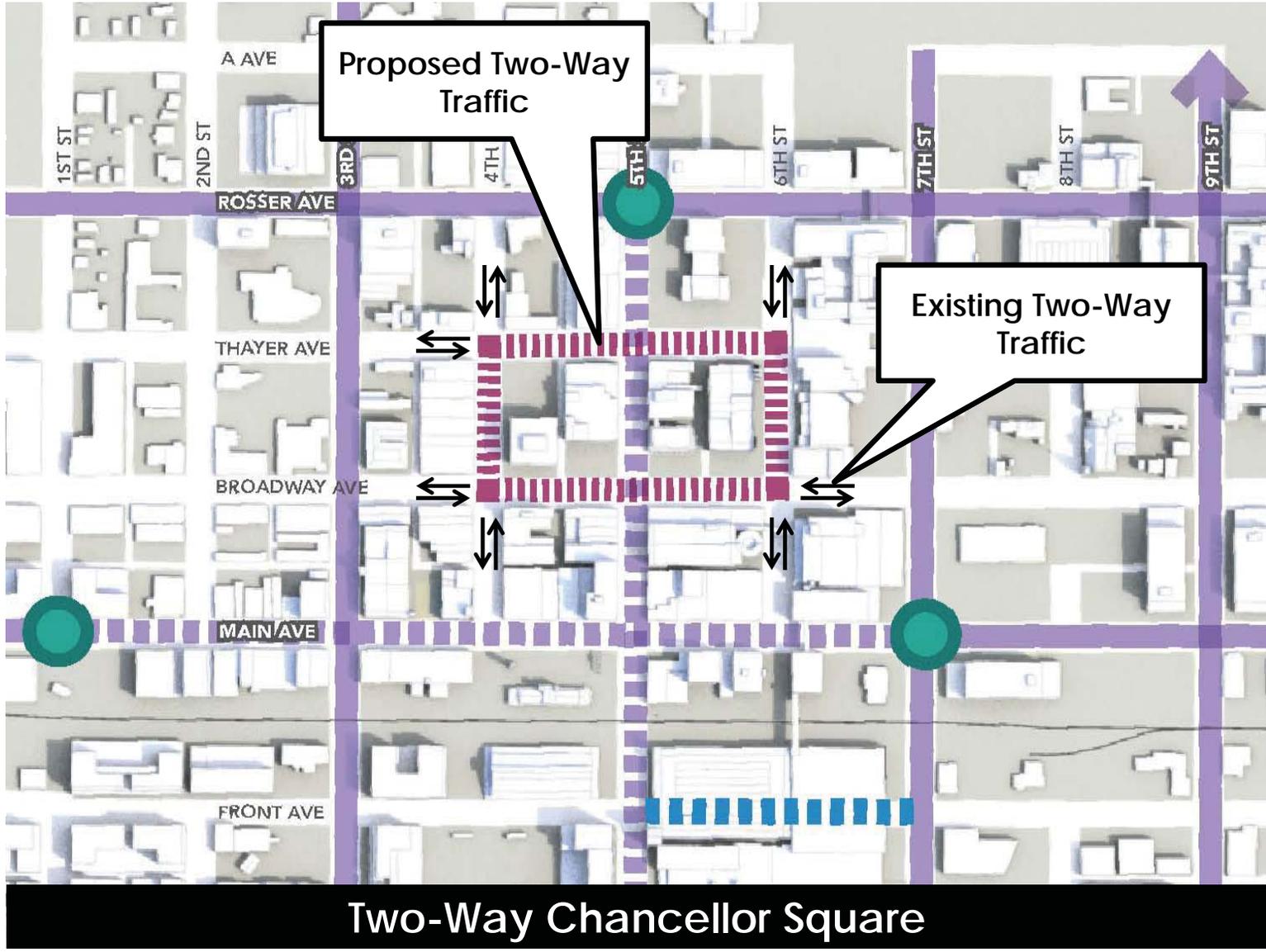


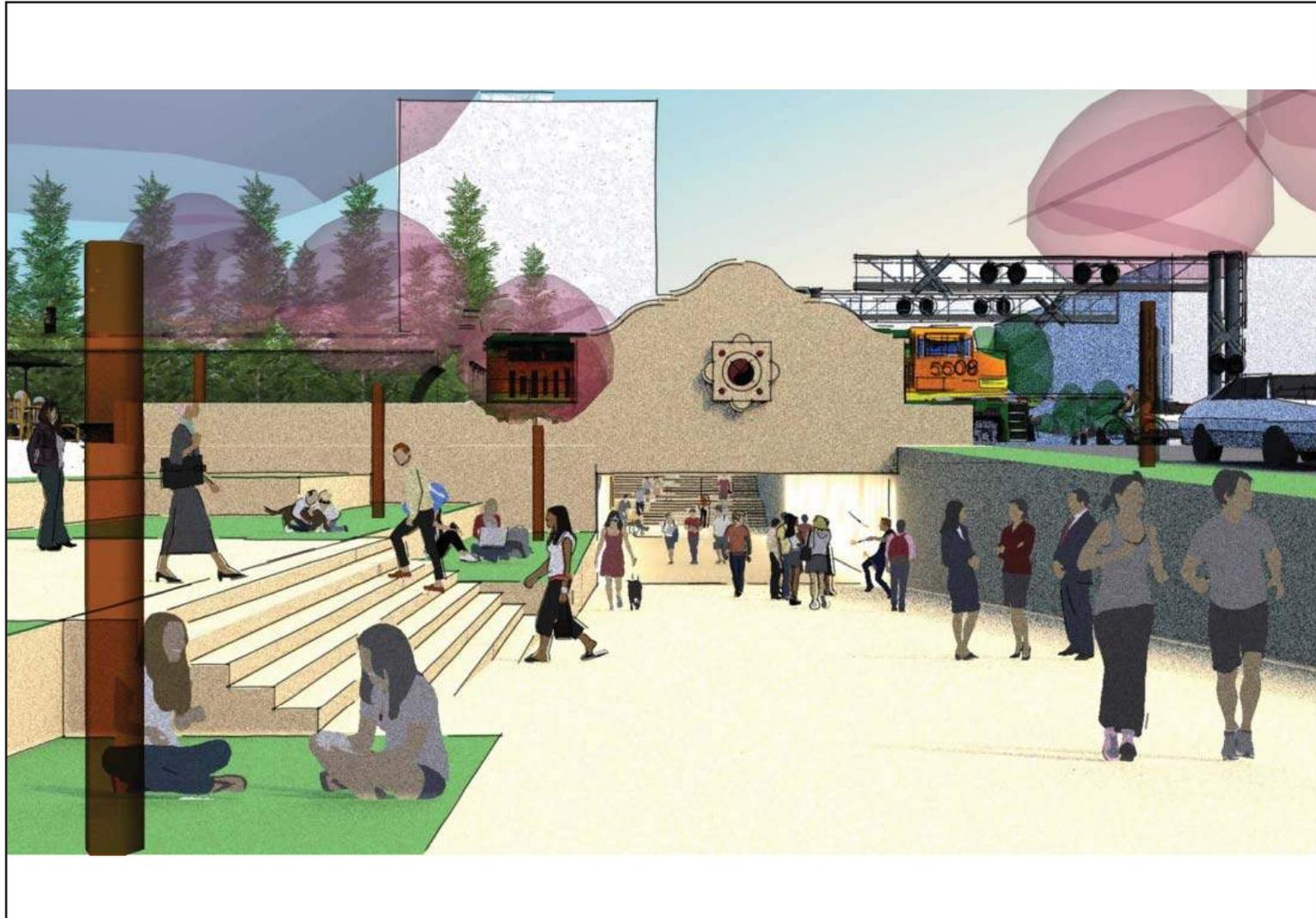


Existing Four Lane

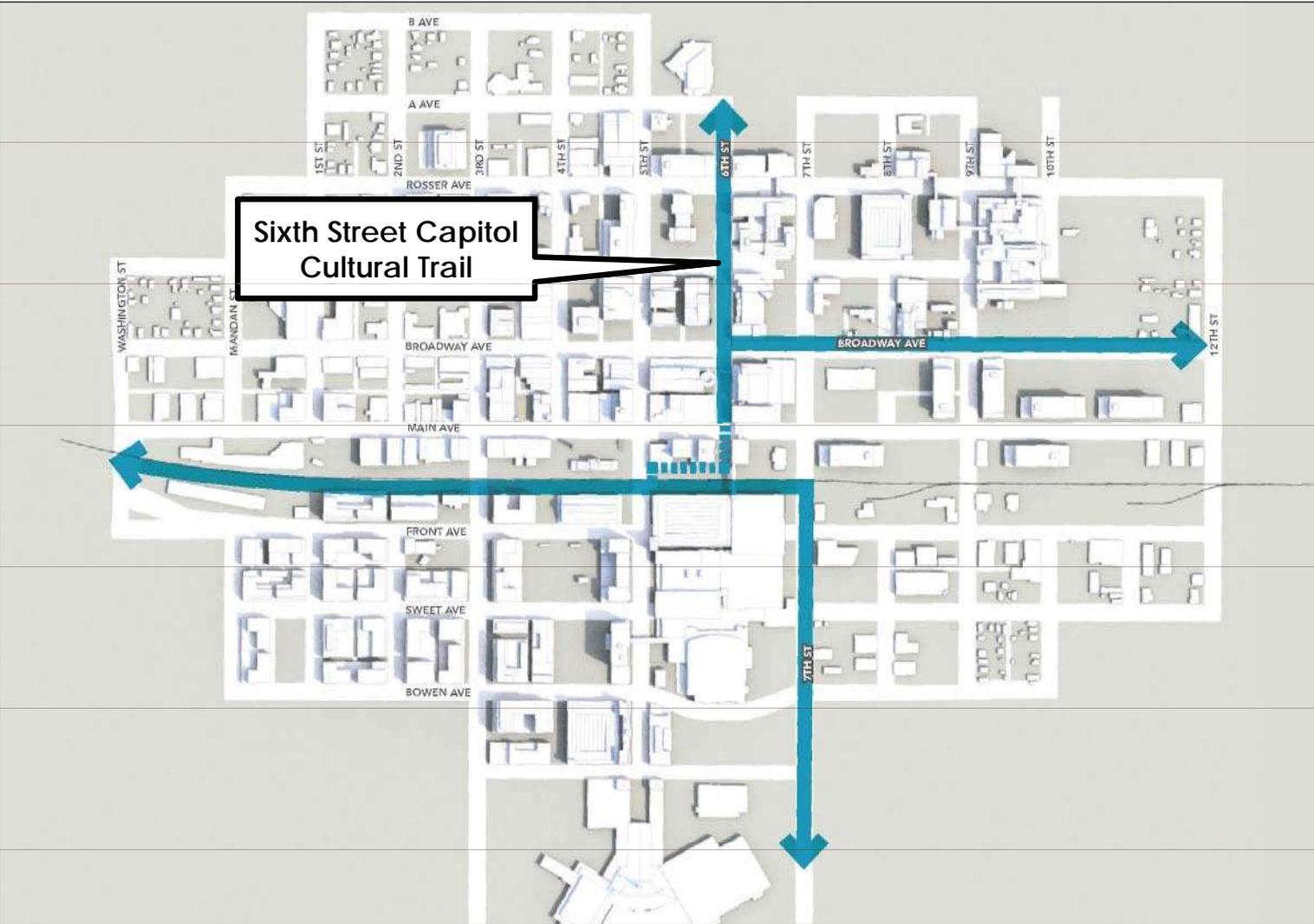
Proposed Three Lane

Main Avenue Road Diet





5th Street Pedestrian Underpass (Looking North)



Sixth Street Capitol Cultural Trail

Sixth Street Capitol Cultural Trail



Sixth Street Capitol Cultural Trail

MEETINGS/PUBLIC WORKSHOP #2 SUMMARY

Meeting Summary

Downtown Bismarck Subarea Study Meetings & Public Workshop #2

May 22 - 24, 2013

2

Summary

The second of the Downtown Bismarck Subarea Study committee/stakeholders meetings and public workshop were held May 22nd to May 24th, 2013. During that period, the consultants presided over a total of eleven meetings that included the Technical Advisory Committee, Steering Committee, the Planning Commission and numerous stakeholder groups at the City/County Building located at 221 N. 5th Street.

The Public Workshop #2 was held on May 23rd from 5:30-7:00pm at the Civic Center Prairie Rose Room 205 and was attended by approximately 50 community members.

Additional public input was collected via on-line response sheets- up to June 14th . The on-line response included an additional twenty-four response sheet submittals and comments. The tally on the following page is a summary of all public response sheet submittals.

Meetings and Workshop Purpose

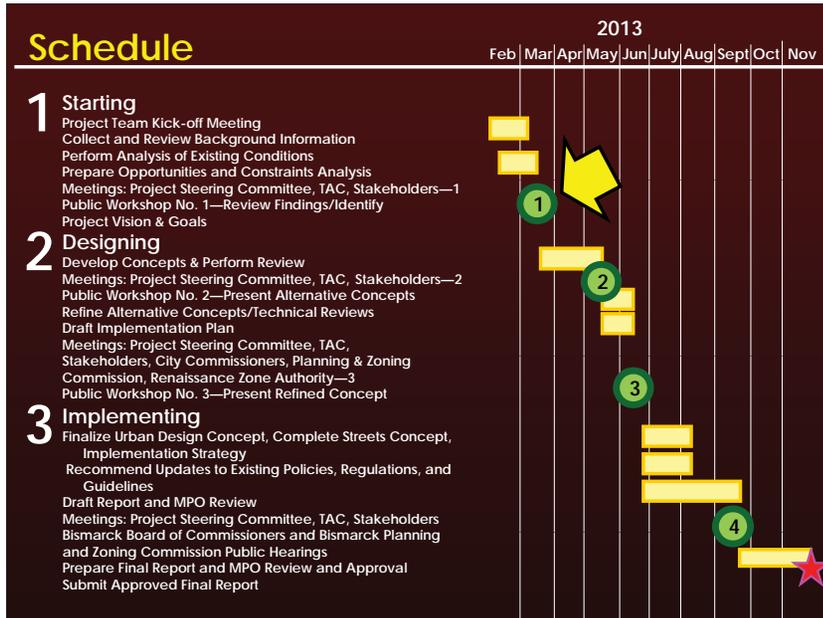
The purpose of the meetings and workshop was to:

- Review Public Workshop #1 Findings
- Present Urban Design and Complete Streets Concepts
- Answer Questions
- Identify Preferences for Urban Design and Complete Street Concepts

Each meeting and the Public Workshop began with the consultants presentation of the project process and schedule (shown above right), a summary of Public Workshop #1, and a review of urban design and complete streets concepts. Following the presentation, attendees discussed the concepts over table maps. The workshop portion concluded with table reports from each group summarizing the discussion. Written response sheets (shown right) were provided to all with the purpose of documenting preferences for eight urban design and complete streets concepts.

The following pages include:

- Response Sheet Tally (Meetings, Workshop and On-line submissions)
- Response Sheet Comments
- Table Reports



Project Process & Schedule

Response Sheet

Downtown Bismarck SubArea Study
 Meetings & Public Workshop #2
 May 22-24, 2013

2

Urban Design Concepts

	Check Preference		
1. District Concept	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Other
2. Green Space Framework			
a. Depot Plaza	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Other
b. Mehus Commons	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Other
3. Retail Concept	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Other
4. 5th & Front Retail/Civic Center Parking Ramp	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Other

Complete Streets Concepts

	Check Preference		
5. Main Avenue Road Diet	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Other
6. Two-Way Chancellor Square	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Other
7. 5th Street Pedestrian Underpass	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Other
8. 6th Street Capitol Cultural Trail	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Other

Comments

Please note comments below or use the back of this sheet:

Name (optional): _____

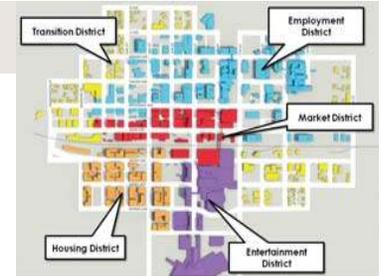
If you need additional time to respond, please return your comments through:

MAIL: 520 SW Yamhill, Roof Suite 4, Portland, OR 97204 WEB: www.DowntownBismarckStudy.com/public-meetings

Response Sheet Tally - Urban Design Concepts

1. District Concept

60 Yes **2** No **1** Other



2a. Green Space Framework- Depot Plaza

62 Yes **3** No **1** Other



2b. Green Space Framework- Mehus Commons

62 Yes **0** No **3** Other



3. Retail Concept

59 Yes **1** No **5** Other



4. 5th & Front Retail/Civic Center Parking Ramp

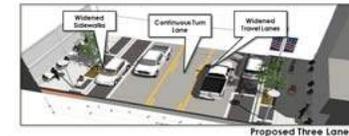
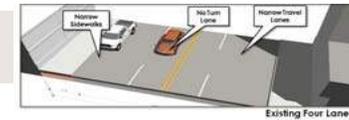
55 Yes **5** No **4** Other



Response Sheet Tally - Complete Streets Concepts

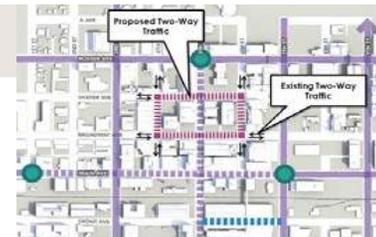
5. Main Avenue Road Diet

63 Yes **0** No **5** Other



6. Two-Way Chancellor Square

58 Yes **1** No **5** Other



7. 5th Street Pedestrian Underpass

54 Yes **4** No **7** Other



8. 6th Street Capitol Cultural Trail

51 Yes **2** No **8** Other



Response Sheet Comments

Below is a summary of the comments mentioned in the response sheets collected from the committees and stakeholder meetings, Public Workshop #2 and additional online submissions via the project web site. were added after June 14, 2013.

Comments are grouped under the over arching topics of Urban Design, Complete Street, Implementation and Other.

Urban Design Concepts

1. Steering, Technical and Stakeholder Meetings

- Assume the district concept allows some bleed-over of usages and is not exclusive in each district
- Will the district concept consider areas for youth (skate park/rink, basketball, tennis courts, etc)? Make theater? Rec Center? Pool?
- Like the Mehus Commons concept
- I think Burleigh mall alley should be pedestrian only
- 6th Street Ramp- why no retail on ground level?
- Concerned about taking away potential retail space at NE corner of 5th and Broadway
- Don't close Front Avenue; it is a truck route
- Is there another option for the 5th and Front parking ramp?

2. Public Meeting

- District concept needs to be clear that housing is incorporated throughout the districts
- Affordable housing measures!!
- Affordable housing— please consider for physical activity
- Need really affordable housing
- Residential access to downtown
- Housing: need availability of middle-income housing
- We see downtown growing to the west and north and lesser

emphasis to the south. How would the housing district tie into downtown since the natural barrier of the Front Avenue hill obstructs the line of site for the new residential district?

- The housing project would help urban sprawl to the country
- Wow! Love the Depot Plaza and many of the concepts
- Depot Plaza—too dangerous
- Plaza safety-2
- Where will people park for Fiesta Villa if the Depot Plaza is built?
- Belle Mehus square great!
- I like the emphasis on Mehus
- Not 100% sold on logistics of Mehus Commons but I like the idea
- Courthouse alley great (Burleigh Mall)
- Having everything spread out across Main & 5th is great; it needs to and should happen
- Retail needs to be established in west downtown not just Main Avenue and 5th Street
- Concern of weather- (for night-life district especially) maybe instead of strictly 'night life' it could be more mixed-use (retail on bottom and housing on top) or make it restaurant/bar (something that visitor's at the convention hotel can do and spend money)
- Concerned about retail concept and cold weather
- Please make architecture promote and respect historical and traditional themes (Retail concept)
- I love the idea of having an anchor downtown and to the mall
- Need a market downtown; Combo of 'farmer's market and coop grocery for other grocery essentials
- Arts corridor great!
- After civic center events- late
- The hotel is needed if the civic center gets used more

Response Sheet Comments

- I would love a GAP for retail
- I know what you mean by 'grandma-friendly' but wish you'd find another term; it implies that older women are particularly vulnerable
- Leave Front Avenue open and build ramp over the top—2

3. Online Submissions

- I would be concerned that parking ramps won't be constructed. Bismarck badly needs to use the empty space that has been left by the parking lots. There replaced beautiful and functional older buildings and it has been seriously detrimental to the downtown district in terms of getting around, aesthetics, weather protection (heat and cold!), businesses and I'm sure a host of other areas.
- Thank God someone has finally taken control
- I am very excited for these changes to occur. It will be great for Bismarck!
- I dont see districts in most downtowns, usually mixed use
- I think it's a smart and healthy use of our current under-utilized space in downtown Bismarck. I have been waiting for a plan like this for decades. It makes so much sense to me and most of my friends
- The idea for districts is an interesting one. Although, in my opinion, one of the appeals of a downtown is the random mix of uses. It gives a small tract of land a very heterogeneous mix which lends itself to unique spaces and architectural "surprises". By breaking the downtown into zones, you are creating a predictable pattern that mimics modern development in North Dakota. This takes away from the unique appeal that our states downtowns offer
- I'd like to see more green space in the plaza possibly
- I like it. Will the Market be year-round? It should!! I'm a market vendor and I sell baked goods. I would love to be able to do that year-round in Bismarck
- A Plaza would be a great place for families, friends and anyone crossing on west side of fiesta, too
- As long as the Civic Center area is connected and redesigned like the rest of the plan suggested, then this area would make a lot of sense as a plaza. I don't know how it would work with Fiesta Villa currently occupying that building, but I believe it makes a lot of sense, especially if you connect it to the other green zones, like the one in front of the Belle Mehus, and then use that to bring that same outdoor public space atmosphere into the rest of downtown Bismarck. Broadway needs some attention and thought, as well, so the Depot Plaza could be a nice center to develop out from. It makes sense to me only if the Belle Plaza and the Civic Center projects are also done along with it.
- Very exciting idea (Depot Plaza). Would really pull together the street and all the exciting new business along it
- I really like this (Depot Plaza). But you need to do something with that damn train backing up traffic during PEAK hours (like lunch time). Sometimes the train comes through and screws up the entire downtown area with backed up cars for nearly a mile during LUNCH TIME. Make the train go under the ground
- I LOVE it! (Depot Plaza)
- Very intelligent use of the Depot parking lot (Depot Plaza)
- Fantastic (Mehus Commons). Kokkler's Jewelers won't be happy though. More greenery! I think the overhead canopy suggestion for the courthouse walkway may be right for a warm-weather city, but since we get long, sever winters it would be nicer to have sculptural

Response Sheet Comments

art that serves to enhance the beauty and solemnity of a courthouse walk throughout the seasons

- I am very supportive of these ideas (Mehus Commons)
- This is essential in my opinion (Mehus Commons). And I like the idea of turning the Alley into a usable space, like the pedestrian street in some of the examples. But I go to the Belle Mehus often and it really does need a green or promenade in front of it. This is a part of Bismarck that is historic and needs to be highlighted
- The courthouse needs to be expanded, maybe more government building space. The Supreme Court has to rent office space from private companies because of a lack of space
- I really also liked the "sculpture alley" concept for the alley way in this block
- Great concept. I would be so happy to see Kokkellers, that eyesore of a building, come down and stop detracting from our downtown
- Really like this idea. I shop at Kokklers though :-)
- Retail Concept- YES, YES, YES!
- Yes to parking ramps and more buildings. Yes to compulsory under living. Promotions for "green buildings"? Encouraging block-center courtyards?
- Very supportive of this (Retail Concept)
- We could make a cool downtown this way (Retail Concept)
- Been waiting many years for the downtown of Bismarck to come to life. Great work!
- Need to clean up all the trashy and ratty looking parking lots and run

down businesses. Too many shifty looking people walk Fifth street to the mall

- I do like the idea of linking downtown to the mall through walkable retail. I also like the concept of linking the civic center to a hotel on the west side of 5th. I do wish you would update any plans around the civic center to include the currently proposed expansion. These planning exercises are worthwhile, but they need to be realistic and encompass projects that are already in motion
- Love it (Retail & Civic Ctr Parking Ramp). Would prefer to build parking ramp over 5th rather than removing street
- I love it (Retail & Civic Ctr Parking Ramp). I think that skywalks are essential here in ND and can only serve to encourage free movement in the downtown area. Enclosed parking facilities can only help keep the streets clean and clear and less cluttered. As it is now and has been for the last 30 or so years, we have been calling Bismarck "Parking Lot, USA". We've joked for years that you could address an letter Mr. and Mrs. John Doe, Parking Lot, ND, USA and it would arrive. Help!
- We need more sky bridges in this cold climate. It will help attract people to various activities and shopping all winter long
- More tie ins to central downtown
- It's about time this takes place for Bismarck! I'm SO happy about the Green Spaces/Public Squares. You have come up with ideas that I never would of dreamed of. I do however dream of a Museum in your proposed concept. Thank You for your open mind thinking. :)
- I will be moving to Bismarck in a month and am very excited to see possible changes on the horizon. Compared to other cities in the upper Midwest - even Fargo- Bismarck is not very pedestrian

Response Sheet Comments

friendly. I also think that Bismarck should pay more attention the young families and their need for activities.

- Restrictions on new offices, to have a nice look and retail floor
These plans are great! Also, I know that NDSU has moved many of its classes into downtown Fargo. I would like for Bismarck become more of a college town, maybe bringing some of it into downtown areas. BSC is not far from this area, couldn't they grow BSC energy programs in this downtown area? Just a thought. Thank you.

Complete Streets Concepts

1. Steering, Technical and Stakeholder Meetings

- Need more technical data to support the Road diet and Two-way Chancellor Square concepts
- The road diet is probably a good alternative to solve the issue where cars going straight have to wait for left turning cars; Any considerations for roundabouts for bustier intersections?
- As a retail store owner, who has a store on Main Avenue, I like the concept of wider sidewalks; I would like to see bike racks incorporated into sidewalk widening
- Not sure about the underpass
- Pass on Capitol cultural trail
- Don't know enough about the Capitol Cultural trail concept to comment in favor or not

2. Public Meeting

- Complete streets- a must! Increase access to and opportunities within Downtown
- Main concern is congestion during parallel parking
- Main Avenue diet would provide safer crosswalks for older people and wider sidewalks for newer landscape
- Please analyze opportunities to use planted medians in areas to slow traffic and allow

- Pedestrians to wait while crossing streets (retail concept and continuous turn lanes)
- Not sure two-way Chancellor square would make it any easier
- Underpasses are scary for women alone or with children; even with lighting, there are many places for people to hide; If there are crowds around when a person first enters an area, the thought of how secluded the underpass will feel as they exit may keep a woman from wanting to shop in the area.
- Capitol corridor to library and north
- Capitol to downtown access great
- Capitol cultural trail did not really draw me in Implementation

3. Online Submissions

- Please don't put in "bike" paths. We have a terrible time in the winter with the city not plowing the streets or simply throwing dirt/sand down (no salt). I work in an office building on 4th street, people are maniacs whizzing down the roads.
- I highly agree with the road diets. Consider 3-lane roadway on 5th Street with roundabout at 5th/Bowen. Consider roundabouts at all intersections where there is a traffic signal now. Need Quiet Rail now! Overall, these are good concepts and I applaud the city for doing this study
- Excellent. Love it. (Main Ave Road Diet)
- Great. (Main Ave Road Diet)
- Very supportive of this. (Main Ave Road Diet)
- Add nice trees that are columnar, to increase visibility while adding green. (Main Ave)
- The road diet is a great idea. It will make it safer for pedestrians as well as vehicles
- Needed. (Two-Way Chancellor Square)
- One way traffic is very confusing to out of towners. Should all be 2 way

Response Sheet Comments

- Yes, (Two-Way Chancellor Square) but the streets need to be completely re-done
- Absolutely! (5th Street Underpass)
- I'm somewhat ambivalent about the pedestrianized underpass. The snow and ice can be treacherous in ND and combined with a howling wind that tears through a tunnel, I wonder if it would become a dangerous no-go zone. It would have to be very well-lit and frequently cleaned. (5th Street Underpass)
- This is one of the best ideas to come from this entire meeting. (5th Street Underpass)
- Make the retaining wall less closing in, by angling it or making it slanted, so it doesn't feel so closed (5th Street Underpass)

Implementation

1. Steering, Technical and Stakeholder Meetings

- As you discussed at the beginning of your presentation, implementation is key. I like your concept and ideas, but implementation of some form of these concepts will be difficult. I await the time when concepts are further evaluated to see if they are implementable.
- Main Avenue, Depot plaza and pedestrian underpass are critical!

2. Public Meeting

- Main Avenue Road Diet should be first priority
- Priority is the complete streets! Multi-purpose trail next to the rail!
- Prioritize Depot Plaza, then Retail Concept, then Main Avenue road diet
- Removing historic building will be a big obstacle, can the buildings be incorporated?

3. Online Submissions

No submissions

Other

1. Steering, Technical and Stakeholder Meetings

- The city will be making repairs to infrastructures in the next several months— how do we address these maintenance needs when there could be 'big picture' changes in the next several weeks?
- I don't like the potential impacts to historic structures. Has this been taken into consideration? Has redevelopment of these structures been part of your concept instead of building new?

2. Public Meeting

- Plan reflects my values and vision: public space, bikeways, walking space, green space
- Many excellent ideas
- This revitalization program would greatly encourage a sense of community; it would prevent some of the urban sprawl; we would be the envy of other cities
- Strategic plan for the future! I love it and we don't have enough
- This type of project would be a destination for downtown Bismarck
- Need to do a lot of communication/education
- Need to overhaul total bus routes
- Beef up transit
- Tie Capitol to ramps downtown via transit
- I want public transit and energy efficiency to be a major criteria/goal for all phases
- More skywalks to connect major buildings for the cold months
- I would like to see a section of downtown (at least one street) with NO cars; outdoor restaurants do not fit with cars!
- Bring BAGA, street art, both State and City libraries into greater focus in the plan
- As we implement the plan, I'd like us to build on the deco

Response Sheet Comments

architecture of the Capitol and carry it through as a theme on new elements like signage, buildings, the underpass, etc

- Include ice skating rink!-2
- Have snow removal sites and design for function
- Very little mention of 'west-side' downtown development is detailed- this must be addressed!
- The city MUST change its snow removal and clearing of sidewalks for this to happen successfully
- The public library is a major gathering place; can the plan extend to include it?
- What about controlled commercial signage and more harmonious building fronts?

3. Online Submissions

- I loved every idea. You guys have just nailed it! Everything I saw here is everything I have been wishing and wanting for Bismarck for a long time
- I've seen city-center revitalisations happen in other countries where an economic boom is taking place. I have also seen them fail miserably and although beautiful to look at, they remain empty. The nice thing about this plan is that it is taking an existing area of interest, one that used to be vibrant and attractive to local residents and making it the place to go once again. Bismarck became fragmented because of poor planning, badly placed shopping centers and too many open spaces. The old mentality was to tear down buildings, chop down trees and make room for so-called practical things like parking lots, to move forward no matter the direction and no matter the cost to the town residents. The old downtown had "anchor stores" mixed in with small family-run bakeries, small shops, jewelers and grocery stores to keep the center active and a place with amenities. We have plenty of the

large-scale grocery stores now, but there is room for specialty shops, restaurants, cafes, etc. and this plan supports the return of those kinds of services to the downtown area. I sincerely hope that this plan goes through, but I have my doubts. My family have lived in the downtown area since 1976 and we've seen the destruction and irreversible damage done to many beautiful houses and buildings. We're all glad that this plan is being formed and see it as a realistic course of action. The greater public reaction to this remains to unfold and there will be factions who will protest it mightily, preferring that unorganised expansion continue until Bismarck breaks into three or four completely separate and decentralised burroughs, without a cohesive connection between them. One can hope. Good luck and you have my full support.

- I have been to Seattle, Portland, Vancouver WA, Missoula, and others and always said, why can't Bismarck do this? If you can get corporate to kick in the majority it would be a big yes
- This is an amazing concept that will turn a drab downtown into a modern, cultural asset. Let's do this!

Table Reports

Below is a summary of the comments presented by members of each table during Public Workshop #2.

TABLE 1

- Unanimously excited
- Agreed with pedestrian/bike focus and green spaces
- Issue with flooding for residential zone
- Support road diet on Main Avenue
- Underpass good way to bridge gap north/south
- Concern use of plaza 300 times a year
- Would like more info on Public Market; indoor with outdoor feel

TABLE 2

- Not enough off-street parking in downtown; don't eliminate parking in Fiesta Villa
- Three lane traffic is ten times better than four lanes; add angle parking
- We like outside elevators on garage
- Chancellor Square biggest bottleneck in Bismarck! Put Two-way traffic back
- The State of ND is #1 with a property tax reduction! You can't lose a nickel here!

TABLE 3

- Do you have history with educating or selling the idea that we can walk and experience downtown like we do when we travel?
- We all need info to digest!

TABLE 4

- Loved the concepts- support an ice skating rink
- Keep in mind the culture of the area for plaza (not too modern, build off history)
- There is a need for affordable and medium-income housing
- In terms of public health we like the access plan for pedestrians
- Good idea connecting Mall/Civic Center to Downtown

TABLE 5

- We unanimously agree with Main Avenue road diet
- Support the underpass! Important
- It is time for two-way on Chancellor Square; Glad to see street tree and landscape improvements
- Plaza is a game changer: no doubt!
- Mehus Commons is also very important
- Implementation: should include the private investment that goes in concert with parking improvements

TABLE 6

- In process of Civic Center Exhibit Hall; the roof top of Exhibit Hall will be useable outdoor gathering space

TABLE 7

- Main Avenue congestion will be helped
- Expand pedestrian area to Library and north
- Worried about Plaza safety
- After events dealing with congestion and getting folks out of downtown quickly

TABLE 8

- Enjoyed whole plan; I grew up here and we haven't had a plan
- Our concern is the weather: long cold winters; spread down 5th with retail/restaurants
- Can we get covered walkways? Design for our climate

PRELIMINARY TRAFFIC OPERATIONS REVIEW MEMO



Memorandum

SRF No. 8022

To: Jason Graf, Associate Principal
Crandall Arambula PC

From: Matthew Pacyna, PE, Associate

Date: May 1, 2013

Subject: Downtown Bismarck Subarea Study - Tr

Introduction

As part of the Downtown Bismarck Subarea Study, several concepts were developed to help revitalize the downtown core and provide a framework for redevelopment. To help revitalize the area, it is important to consider the existing transportation infrastructure, which can have a significant impact to the perception associated with a downtown area. Therefore, SRF was tasked with reviewing concepts developed as part of this study from a transportation perspective to help identify any potential issues, improve connectivity for all modes of travel, and provide recommendations to maintain and/or enhance parking, operations, and safety. It should be noted that the draft project goals that mostly pertain to the transportation review include the following:

- Create a pedestrian-friendly and walkable downtown
- Develop a parking strategy that maximizes existing and future ramps to best serve downtown
- Introduce more greenery into downtown streets
- Encourage biking downtown
- Improve traffic access in and out of downtown
- Develop a downtown multi-modal transit hub
- Improve the linkage between downtown, the Civic Center, and Kirkwood Mall
- Convert Chancellor Square to two-way
- Locate a downtown trolley route

Based on discussions with project staff regarding the initial concepts developed, the following specific improvements were considered as part of this review:

- Road diets along 5th Avenue and Main Avenue (reduction of roadway capacity to three-lanes)
- Closure of Front Avenue (from 5th Street to 7th Street)
- Conversion of Chancellor Square from one-way to two-way operations
- Civic Center Parking Ramp Location

It should be noted that these concepts were reviewed independent of one another, when possible, to help quantify impacts associated with each improvement.

Existing Conditions

To establish a baseline condition in which the impact of the concepts developed can be compared, existing traffic volumes, roadway characteristics, and capacity were reviewed. The following information provides an overview of the existing conditions within the downtown subarea:

- Existing (year 2012) Average Daily Traffic (ADT) volumes provided by NDDOT were collected and summarized in Figure 1.
- The existing roadways within the subarea were categorized into the one of the four roadway types:
 - 2-lane roadway (no turn lanes)
 - 3-lane roadway/2-lane roadway with turn lanes
 - 4-lane roadway (no turn lanes)
 - 5-lane roadway/4-lane roadway with turn lanes
- Initial review of the existing roadway network (illustrated in Figure 2) indicates there does not appear to be a consistent roadway network within the downtown core (i.e. several roadways have varying cross-sections).
- For each of these roadway types, the typical planning-level ADT capacity ranges and the approaching capacity ADT volumes are shown in Table 1.
 - These volume ranges are based upon guidance from the Highway Capacity Manual and professional engineering judgment.
 - This information will be used as a guide to help identify roadway capacity impacts associated with the proposed concepts.

Table 1
Planning-Level Roadway Capacities by Facility Type

Facility Type	Daily Capacity Ranges (ADT)	Approaching Capacity (85% of ADT) ⁽¹⁾
Two-lane undivided urban	8,000 - 10,000	8,500
Three-lane undivided urban ⁽²⁾ (two-lane divided with turn lanes)	15,000 - 17,000	14,450
Four-lane undivided urban	18,000 - 22,000	18,700
Five-lane undivided urban ⁽²⁾ (four-lane divided with turn lanes)	28,000 - 32,000	27,200

⁽¹⁾ Values based on the upper limit of daily capacity range

⁽²⁾ The lower value of the daily capacity range is applied to the undivided section; the upper value of the daily capacity range is applied to the divided section.

- Comparing the existing ADT volumes with the capacity ranges in Table 1, all of the existing roadways within the study area have adequate capacity.



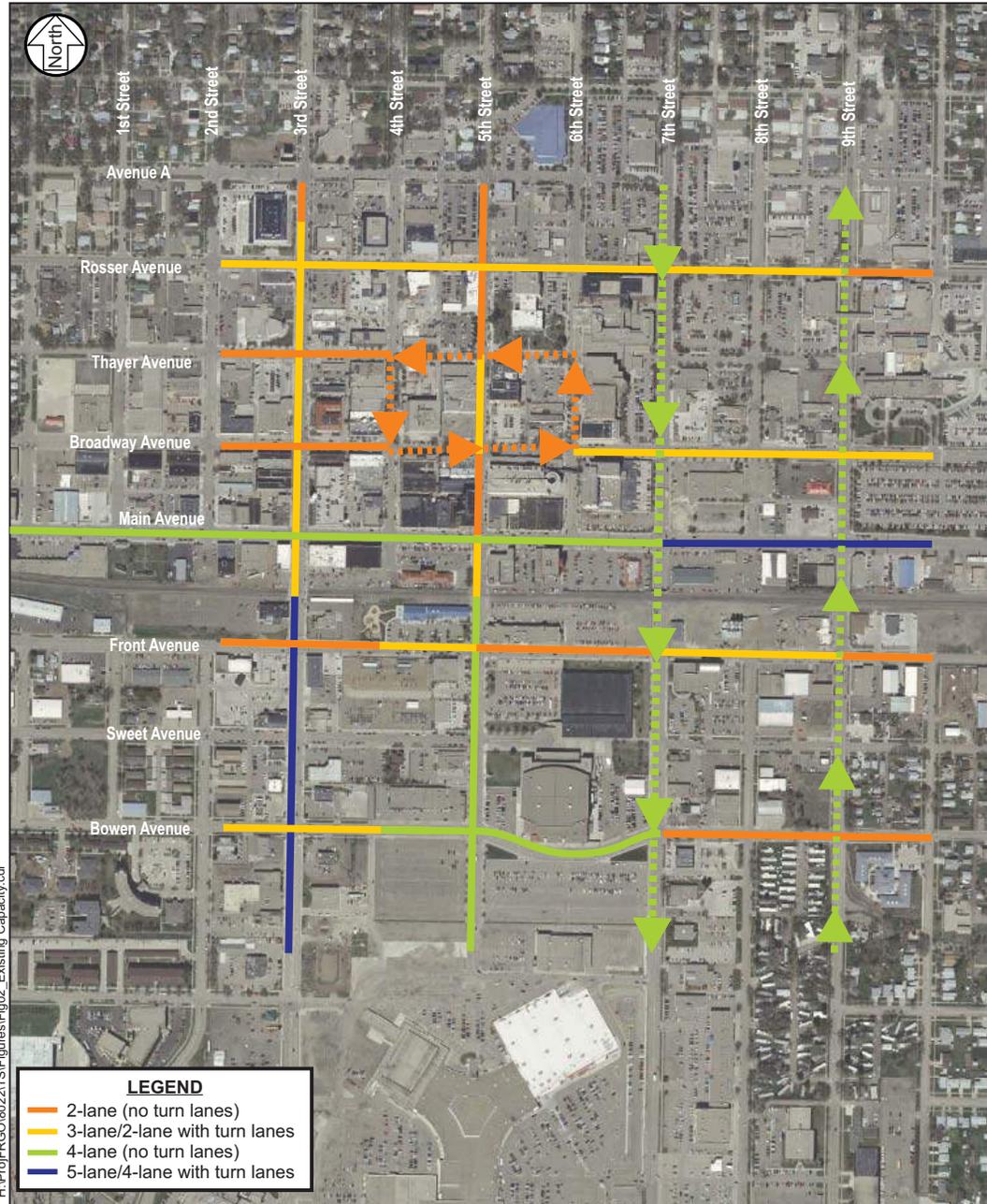
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Existing (Year 2012) Average Daily Traffic Volumes
 Bismarck Downtown Area Transportation Study
 Bismarck, North Dakota

Figure 1

0138022
 May 2013



Existing Roadway Network
 Bismarck Downtown Area Transportation Study
 Bismarck, North Dakota

Figure 2

0138022
 May 2013

Concept Review

As previously mentioned, SRF was tasked with reviewing the concepts developed from a planning level traffic operations perspective. Therefore, the following information provides a summary of the proposed concepts, their impacts, and potential improvements to consider. The approximate location of the improvement concepts are illustrated in Figure 3.

5th Street Road Diet

Currently, 5th Street from the Kirkwood Mall to Front Avenue is a four-lane roadway with no turn lanes. However 5th Street north of Front Avenue is a two-lane roadway with turn lanes only at Main Avenue and Thayer Avenue. To reduce vehicular speeds, promote pedestrian activity, and create a “downtown center”, the subarea study proposed to convert 5th Street to a two-lane roadway with turn lanes and a landscaped median, if possible. The following information provides a summary of the impacts of this proposed change.

- The existing ADT volume along 5th Street ranges from 3,000 to 4,000 vehicles per day (vpd).
 - No volume diversion is expected due to the proposed modification of 5th Street.
- There is sufficient capacity to convert 5th Street to a three-lane facility (or a two-lane facility with turn lanes and a landscaped median) based on the current traffic volumes and the capacity ranges identified in Table 1.
 - The ADT volumes indicate a two-lane roadway without turn lanes would also likely provide adequate capacity and reduce pedestrian crossing distances.
- It is important to ensure adequate capacity at the Main Avenue/5th Street and 5th Street/Front Avenue intersections to prevent queues from extending to the railroad tracks.
 - To reduce potential impacts, dedicated turn lanes (i.e. left, through and right) and/or use of a signal preemption system to clear any queues prior to a train arrival should be considered.

Main Avenue Road Diet

Main Avenue from 1st Street to 7th Street is a four-lane undivided roadway with only select right turn lanes and no dedicated left-turn lanes. There are several private driveways/access locations within this segment, which also impact the capacity and traffic flow. To reduce vehicular speeds, promote pedestrian activity, and create a “downtown center”, the subarea study proposed to convert Main Avenue to a three-lane roadway from 1st Street to 6th Street. The following information provides a summary of the impacts of this proposed change.

- The existing ADT volume along Main Avenue range from 7,000 vpd to 13,000 vpd.
 - Although a four-lane undivided roadway can easily handle this daily volume, it is beginning to approach the capacity of a three-lane roadway in certain segments.
- If the conversion to a three-lane roadway is constructed, existing motorists along Main Avenue are expected to divert to other routes
 - The motorists that are expected to divert due to increased delays (associated with the three lane conversion) would primarily be “through” type trips, which are currently using Main Avenue.
 - This diversion is expected to create some additional capacity for downtown destination type trips.



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Improvement Concepts

Bismarck Downtown Area Transportation Study
Bismarck, North Dakota

Figure 3

- The roadways that are most likely to be impacted by any diversion from Main Avenue would be Front Avenue and Rosser Avenue.
 - To aide this diversion, improvements should be considered at various locations to help make a smooth transition (i.e. reduce delays on the preferred diversion routes).
 - These improvements include turn lanes, traffic controls (i.e. protective left-turn phases), grade separations, and wayfinding.
- Due to the amount of driveways/access locations along Main Avenue in the downtown area, it is recommended that the three-lane roadway section begin west of Washington Street and closer to Rosser Avenue, if possible.
 - This will help create a gateway into the downtown area and may help divert “through” trips to alternative routes.
 - The continuous two-way left-turn lane (TWLTL) will also reduce conflicts with respect to the access along Main Avenue.
 - As redevelopment occurs, access consolidation and/or closures should be considered.
- The three-lane facility along Main Avenue is recommended to end at 7th Street.
 - A second eastbound through lane along Main Avenue should be developed east of 6th Street, while the westbound through lane at 7th Street should trap into a westbound left turn lane.
 - A potential additional improvement is to construct dual westbound left-turn lanes at 7th Street to encourage “through” vehicles to use alternative routes other than Main Avenue.

Closure of Front Avenue

As part of the subarea study, there is the potential for a Civic Center Expansion and associated parking ramp. Although the exact location of the parking ramp is not determined, there are two potential parking ramp location options (see Figure 3), both of which would close a portion of Front Avenue between 5th Street and 7th Street. Currently, Front Avenue is a two-lane roadway with select turn lanes within the study area and is used as a parallel reliever to Main Avenue (from the Missouri River to 9th Street/12th Street). The following information provides a summary of the impacts of this proposed change.

- The existing ADT volume along Front Avenue is approximately 5,000 vpd.
- Closing this section of Front Avenue will likely cause diversion to other parallel routes, including Main Avenue, Bowen Avenue, and Rosser Avenue.
 - Main Avenue would be the most likely diversion route as it is the closest parallel route. (However, if converted to a three lane facility, Main Avenue would already be approaching capacity prior to any diversion and is unlikely to be able to handle any additional traffic.)
- Therefore, if Front Avenue were closed, Bowen Avenue and Rosser Avenue would need to be improved to make them more attractive routes to motorists, including additional improvements along 3rd Street, 7th Street, and 9th Street.
- Ideally, the location of the potential parking ramp would be designed and located so that closure of Front Avenue would not be necessary.
 - This type of design has the potential to be a good transit hub location, serving downtown and the Civic Center.

If the closure of Front Avenue is desired, the following additional information with respect to Rosser Avenue and Bowen Avenue should be considered. It should be noted that these types of improvements could be implemented regardless of any closure along Front Avenue.

Rosser Avenue

- Rosser Avenue is a three-lane roadway from 2nd Street to 7th Street and a two-lane roadway to the west and east of 2nd Street and 7th Street, respectively.
- Similar to Main Avenue, there is a significant amount of private driveways/access locations along Rosser Avenue.
 - Conversion of Rosser Avenue to a three-lane facility from Main Avenue to 9th Street is expected to improve traffic flow, reduce potential conflicts, and provide continuity, but may also negatively impact on-street parking in some segments.

Bowen Avenue

- Bowen Avenue is a four-lane roadway from 5th Street to 7th Street and a two-lane roadway to the west and east of 5th Street and 7th Street, respectively.
- Existing ADT volumes along Bowen Avenue range from approximately 3,000 vpd to 4,000 vpd.
- If a portion of Front Avenue were closed and additional volume diverted to Bowen Avenue, a three-lane roadway would still be expected to accommodate area volumes.
 - However, depending on event traffic at the Civic Center, it may make more sense to convert Bowen Avenue to a four-lane roadway between 3rd Street and 9th Avenue.
- Regardless if Bowen Avenue is converted to a three-lane or four-lane roadway, the roadway section should be consistent to provide continuity.
- Additional turn lane and traffic control improvements (i.e. protected left-turn signal phases and traffic control changes) should be considered to further encourage motorists to utilize Bowen Avenue as an alternative route.

Chancellor Square (One-Way to Two-Way Conversion)

Chancellor Square encompasses a two block area bounded by Broadway Avenue to the South, Thayer Avenue to the north, 4th Street to the west, and 6th Street to the east. Currently, traffic flows around these two blocks in a one-way counter-clockwise direction. The roadways that make up Chancellor Square are all two-way roadways throughout the rest of the study area with on-street parking. The proposed concept is to eliminate the one-way operations and convert the roadways to two-way operations. The following information provides a summary of the impacts of this proposed change.

- The Chancellor Square conversion is not necessarily an operational issue as the ADT volumes range from 1,000 vpd to 2,000 vpd and can be easily accommodated with a two-lane roadway section.
- The current one-way operation limits the volume that may otherwise use Broadway Avenue and Thayer Avenue as east/west routes, which in turn directs more traffic to Main Avenue and/or Rosser Avenue.
 - The conversion to two-way operations has the potential to provide some diversion from Main Avenue.

- Two-way operations provide circulation benefits and make it easier to find your destination.
- Based on parking utilization surveys performed for the Downtown Bismarck Parking Study (2008) the area surrounding Chancellor Square is the most utilized parking area downtown (about 80 percent utilized).
 - The conversion to two-way operations is expected to significantly reduce the available onstreet parking, resulting in the need for additional parking to be considered.

Recommended Roadway Network

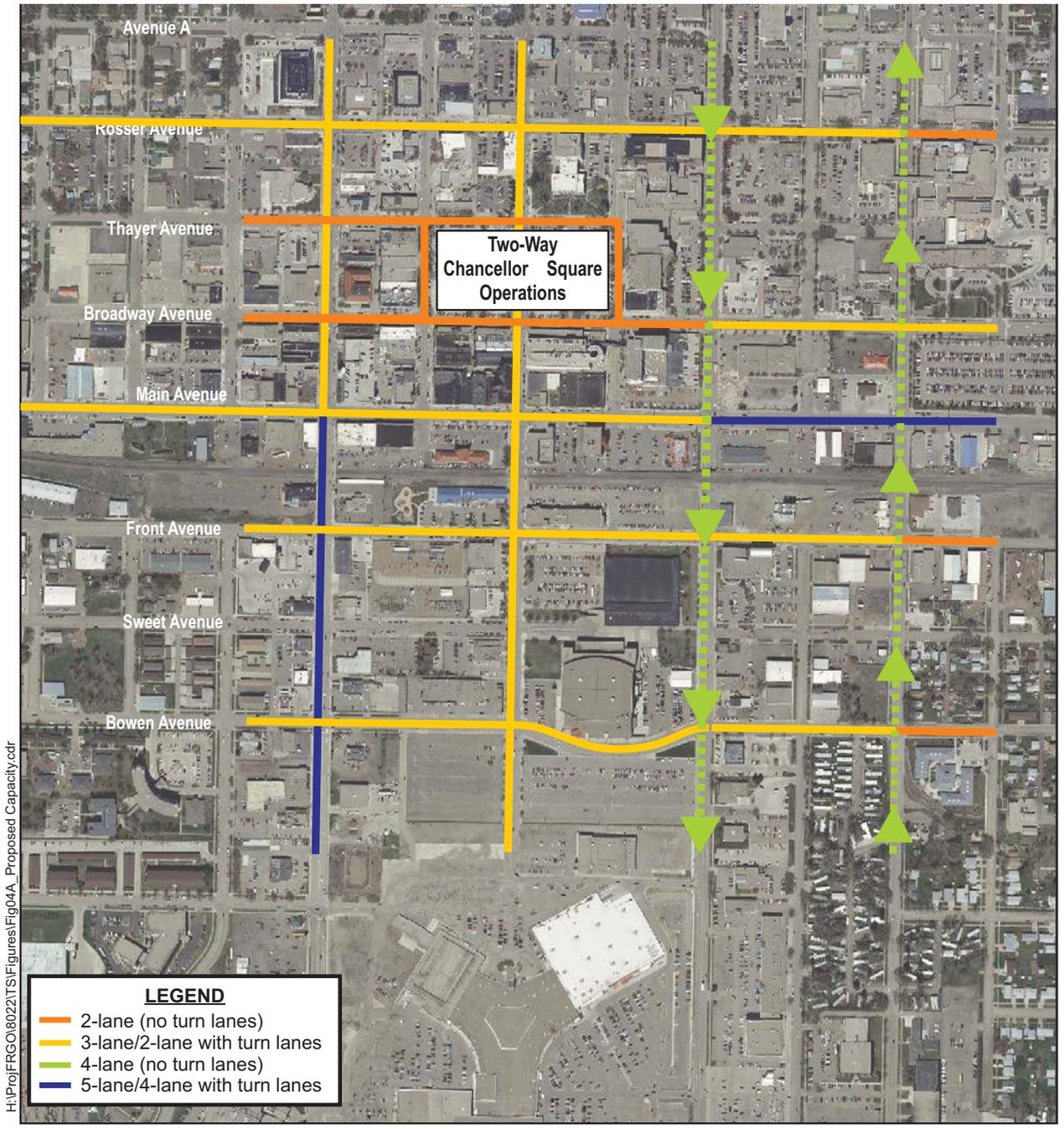
As previously mentioned, several study area roadways have varying cross-sections and there is not a consistent roadway network in the downtown area. Therefore, to provide more continuity and improve traffic flow within and around the study area, a recommended roadway network was developed from a capacity/cross-section perspective. Two recommended roadway network alternatives were developed due to the potential closure of Front Avenue to accommodate a parking ramp. Alternative A and Alternative B represent the roadway network with and without the Front Avenue connection, respectively. The recommended roadway networks, along with the corresponding capacity/cross-section are illustrated in Figure 4A and Figure 4B.

Potential Improvements

In addition to the recommended roadway network alternatives, several other turn lane, traffic control, and miscellaneous improvements were identified. These improvements are for discussion purposes and are considered preliminary. The intent of the majority of these improvements is to improve access to an alternative east/west corridor and help divert “through” traffic away from Main Avenue. The following information summarizes the potential improvements to be considered:

- Develop an eastbound off-ramp from Main Avenue to Washington Street.
- Install an eastbound right-turn lane at the Main Avenue/3rd Street intersection.
 - The specific design could be a typical right-turn lane or be a channelized right-turn lane that could feed into its own lane (i.e. a free right-turn movement).
- Install dual southbound left-turn lanes off Main Avenue to 7th Street
- Construct a grade-separation along 3rd Street and the railroad crossing.
- Install protected-permitted left-turn phasing/traffic control improvements at key locations within the downtown area.
 - It should be noted that the locations of the left-turn phasing/traffic control improvements are dependent on whether Front Avenue is modified.

Once again, two potential improvement alternatives were developed due to the potential closure of Front Avenue to accommodate a parking ramp. Alternative A and Alternative B represent the potential improvements with and without the Front Avenue connection, respectively. The potential turn-lane, traffic control, and miscellaneous improvements for discussion are illustrated in Figure 5A and Figure 5B.



Recommended Roadway Network - Alternative A
 Bismarck Downtown Area Transportation Study
 Bismarck, North Dakota

Figure 4A



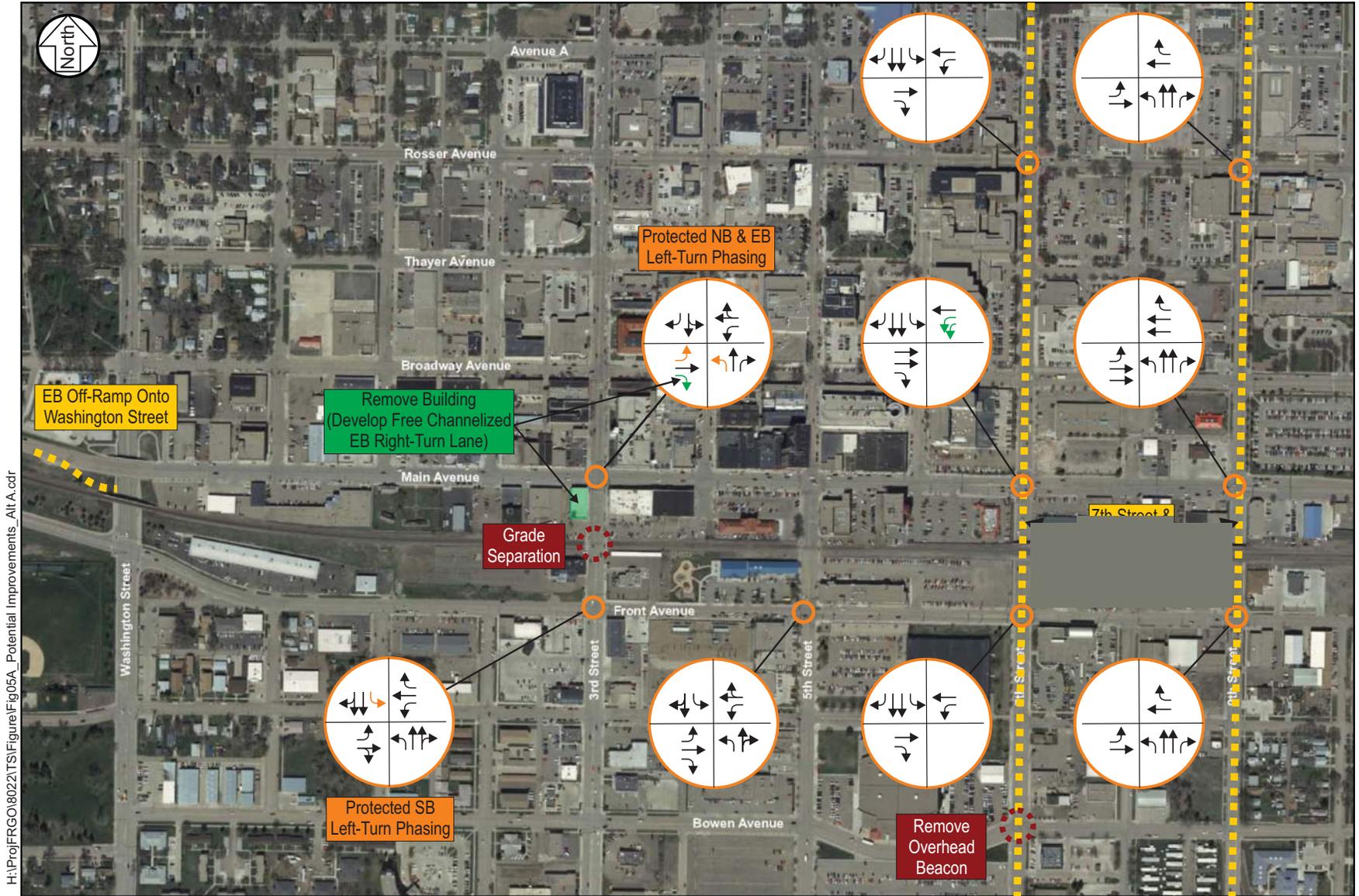
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Recommended Roadway Network - Alternative B

Bismarck Downtown Area Transportation Study
Bismarck, North Dakota

Figure 4B



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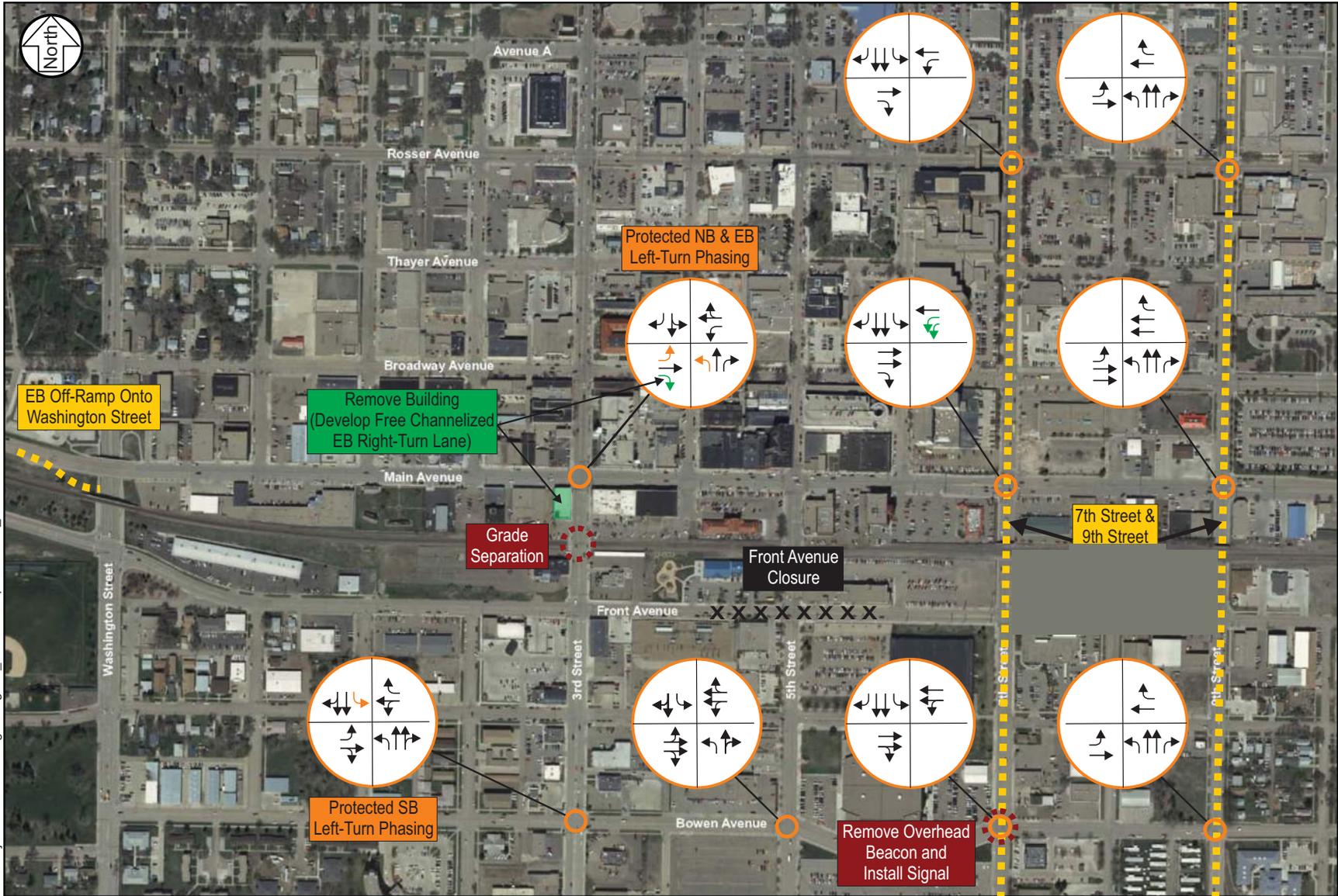
Potential Improvements - Alternative A

Downtown Bismarck Subarea Study
Bismarck, North Dakota

0138022
May 2013

Figure 5A

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Potential Improvements - Alternative B

Downtown Bismarck Subarea Study
Bismarck, North Dakota

0138022
May 2013

Figure 5B

BISMARCK DOWNTOWN TRANSIT CENTER: PROGRAM CONCEPT, LOCATION AND SITE ALTERNATIVES

This memo provides information regarding preliminary transit center locations being considered as part of this study. Based on the existing service and proposed service plan, a single downtown transit center is recommended for CAT operations. Three additional key transfer points will be located in downtown Mandan, in the northern part of Bismarck (vicinity of Gateway Mall), and a single intermodal transfer at the current Bis-Man Transit/CAT facility (allowing access to intercity coaches).

DOWNTOWN TRANSIT CENTER

Ultimately, the objective of a downtown transit center in Bismarck will be to elevate the visibility of transit, and improve the experience of being a transit user. It will allow for timed connections among routes, offer a comfortable passenger facility, allow CAT service to be expanded, and promote development and activity in downtown Bismarck. When transit centers have been developed in central urban locations adjacent to key activity centers and shopping areas, they have provided a steady stream of patrons to local businesses while people wait for buses and transfer between buses. In theory, a new transit center should:

- Have space to accommodate the next 20 years of growth.
- Have adequate boarding/alighting space, layover space and circulation space to ensure smooth operations.
- Provide safe and convenient access for pedestrians and bicyclists.
- Provide a pleasant atmosphere for passengers.
- Meet the needs of bus drivers (including driver amenities such as a restrooms and break room).
- Provide an operations outpost for the transit agency, allowing CAT riders to collect information about the service and talk with a customer service representative.

Basic Program

Crandall Arambula's downtown concept is for an off-street facility to be developed in one of six current parking lots/undeveloped lots. Nelson\Nygaard and Crandall Arambula staff met with representatives from the MPO, Bis-Man Transit and the City Engineer to discuss a preliminary approach to transit center layout and programs. Based on the discussion, the following program is proposed for a new facility in downtown Bismarck:

- Space for nine full-size (30-40 feet) buses to be shared by CAT and other regional providers that opt to drop off or pick up passengers in downtown Bismarck.
- A building that will house (1) an indoor waiting area for passengers with room for seating and an information kiosk, (2) an information desk/work space for two people, (3) a driver restroom, (4) a small break room (to accommodate eight people at a table), (5) a small storage closet, and (6) public restrooms. A vending machine could be outdoors or indoors. Based on these basic requirements, the consulting team assumes a 1,000-square foot building.
- 2-3 auto parking spaces near the building that can be used by CAT/Bis-Man Transit (not public parking).
- Space on-site or off-site for up to two taxis.
- A center island and sawtooth bus bays.
- Bus shelters and benches for each bus bay (and possibly a canopy over the entire site).

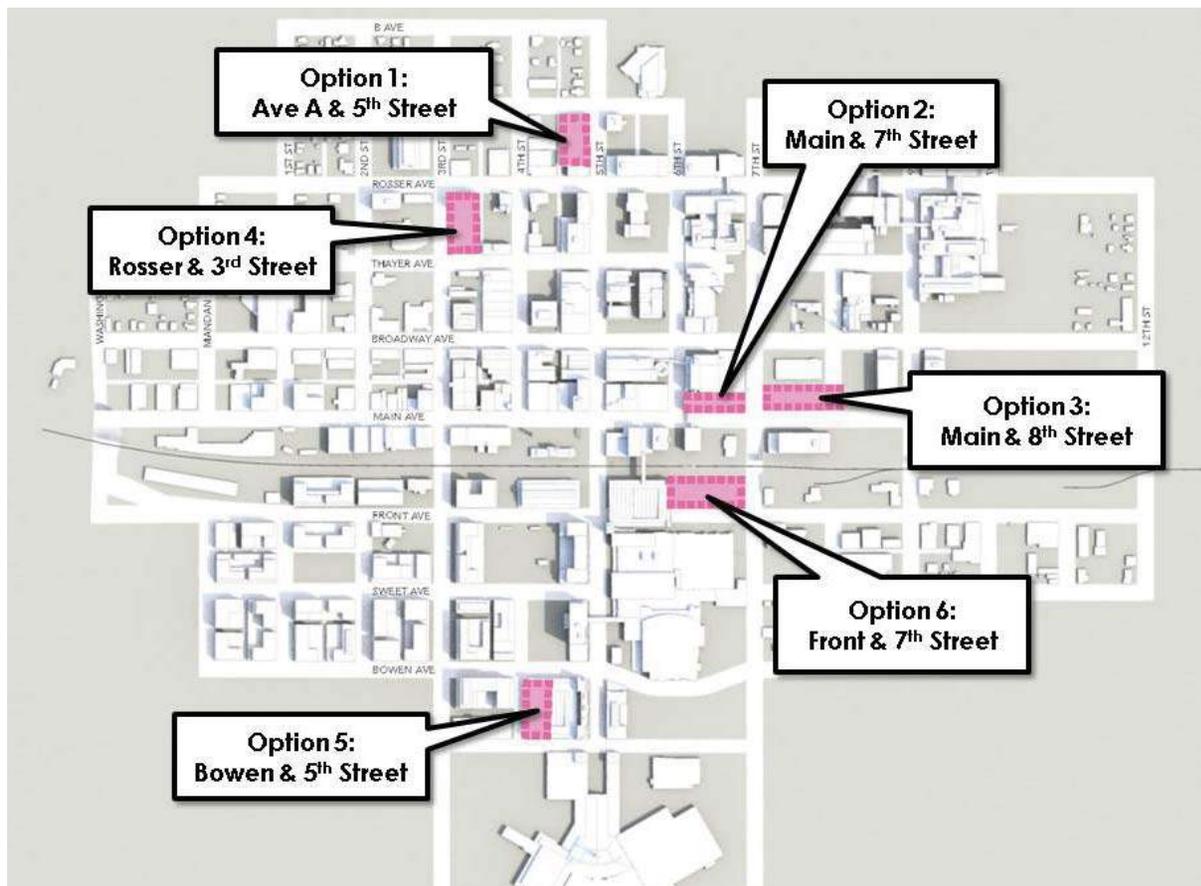
In addition, an array of other modest amenities might be included such as a public telephone, an information kiosk (system map, bus information), bus stop signage, bicycle racks and/or lockers, outdoor lighting, garbage cans/recycling, clocks/real-time arrival bus information, public art, and security cameras.

Potential Locations

Six potential sites in and around downtown Bismarck were selected by Crandall Arambula. These sites are as follows and are illustrated in Figure 1:

- Option 1 – Avenue A at 5th Street (412 N 5th St.), under a new parking ramp. By moving the access to the ramp to the south end of the site, the site can accommodate an island configuration. As a result of the ramp above, this facility would accommodate eight bays for CAT buses on-site, and one larger bay for non-CAT buses on 5th Street (See Figure 2). The 0.8-acre property is assessed at \$105,800, or \$2.99 per square foot and is owned by Burleigh County. It is possible this land could be purchased from the county, or traded for a comparable City-owned site elsewhere in Bismarck. Likewise, it is possible that portions of this property along 4th Street could also be incorporated into the site to make it larger, allowing for more buses to be accommodated off street.
- Option 2 – Main Avenue between 6th Street and 7th Street (630 Main Av., South of Radisson and existing ramp). This site is the somewhat constrained physically, but is in the most central location and would showcase a commitment to transit by the City of Bismarck. The proposed site design is a mix of on- and off-street bus bays, allowing for a center island configuration (See Figure 3). One of the primary advantages of this .6-acre site is that it is owned by the City of Bismarck and would therefore potentially be easy to acquire for a transit facility.
- Option 3 – Main Avenue between 7th and 8th Street (700 Main Av.). This site allows plenty of room for a plaza, a row of pavilions, or a garden that can offer “eyes on the street”. The conceptual design assumes that 8th Street is reopened to the north, allowing for access to Broadway. The site assumes an on-street taxi stand, but taxis could easily be accommodated on-site if desired by CAT (See Figure 4). This 1.03-acre site is assessed at \$313,800, or \$6.97 per square foot, making it the second-most costly site to acquire. Based on subsequent discussions between the land owner and project team, this site may not be available for acquisition due to plans to create a mixed-use office and housing development.
- Option 4 – ½ Block bounded by 3rd Street, Rosser Avenue and Thayer Avenue (311 N. 3rd St., 309 N 3rd St., 307 N. 3rd St., 302 Thayer Av., and 308 Thayer Av.). This site is large enough to offer a fairly straightforward island configuration, with the conceptual design offering two options for a taxi stand (See Figure 5). Although fully ½ of the site is publicly owned by the City of Bismarck, the .96-acre site would be very costly to acquire because several existing structures and businesses on the other ½ of the site. Combining all of the values of the parcels that comprise the site, the land is assessed at \$1,129,400, or \$26.89 per square foot (\$53.78 per square foot if only the privately owned lots/structures are used in the calculation).
- Option 5 – Between Bowen and Kirkwood Mall, west of 5th Street (portion of site at 410 S. 5th St.). This is a large site with flexibility to accommodate the program, depending on how the retail corridor along 5th Street is ultimately built. Based on a preliminary site development concept, the transit facility conceptual design assumes a parking ramp would be constructed on the site (assuming columns on 60’ centers, which is a double-loaded parking bay) and the transit center would be under about ½ of it. A facility/pavilion could be attached to the ramp. For purposes of this conceptual design, a new street at the south end of the site is presumed to be one-way eastbound, but the configuration could be two-way if necessary (See Figure 6). One of the primary advantages of this 3.02-acre site (approximately 1.2 acres as configured for the transit center) is that it is owned by the City of Bismarck and would therefore potentially be easy to acquire to use as a transit facility.
- Option 6 – At the corner of Front Avenue and 7th Street, south of the rail lines and adjacent to a future parking ramp. This is a large site, equivalent to approximately one half of a full city block. The site is owned by the City of Bismarck and would therefore potentially be easy to acquire to use as a transit facility. The complete lot is approximately 2.25 acres. Advantages of this site include its proximity to the Civic Center, being only one block from the planned Fifth Street development corridor, and adjacency to rail, which could offer future multi-modal passenger service, although no plans currently exist. The site is also bordered by a rail trail. Obstacles for this site include restricted pedestrian access for people with origins or destinations in downtown Bismarck (they must use below-grade crossings on 7th Street or 5th Street) and limited access for buses. The site would only be accessed via a single entrance/exit on Front Avenue.

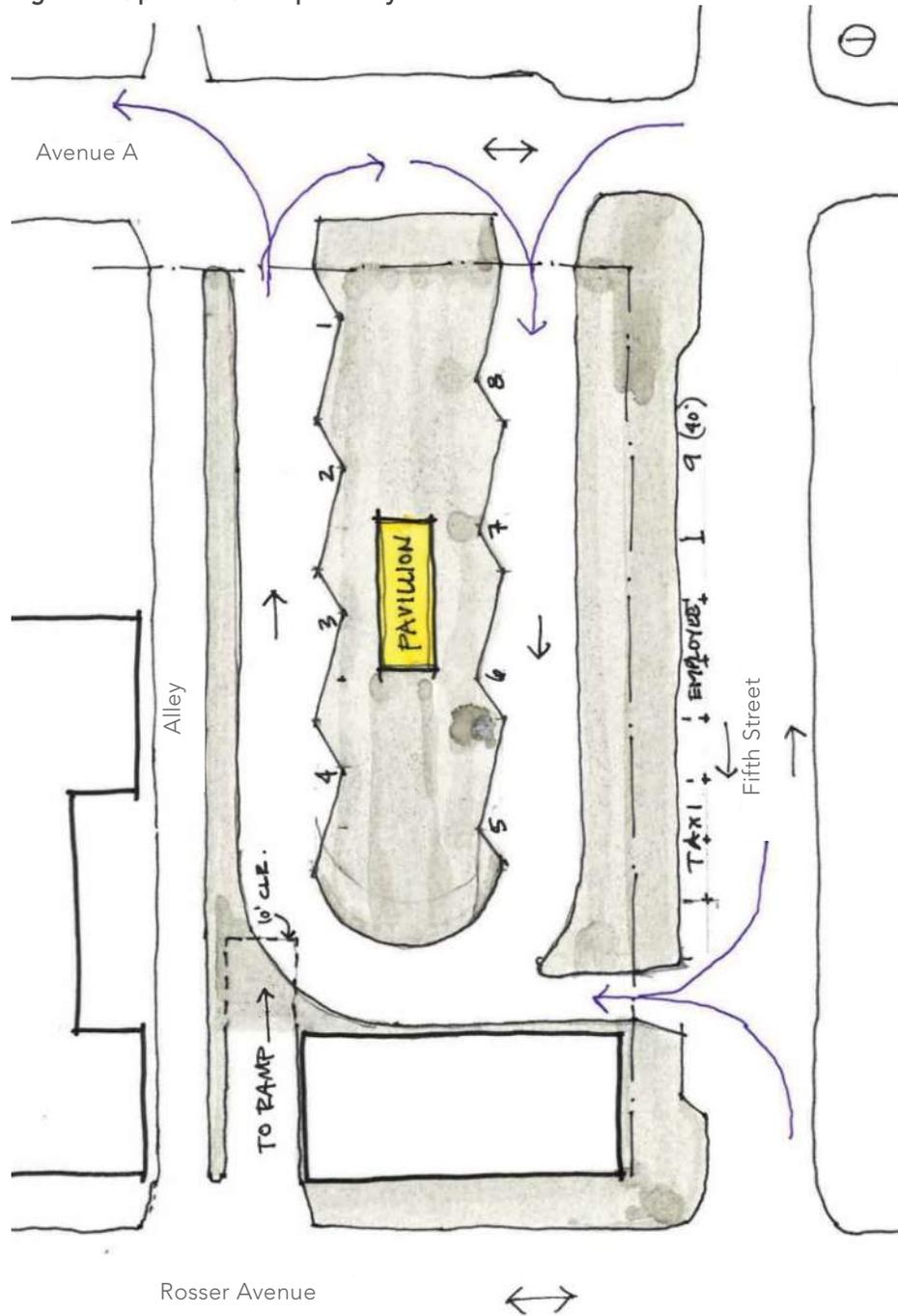
Figure 1 Sites Selected for Consideration



Potential Transit Center Concepts

Sketch layouts for each Options 1 through 6 are included in the figures on the following pages. In some cases more than one layout is offered in an effort to provide two design options, one for accommodation of existing CAT bus design vehicles and the other for accommodation of larger 40' bus design vehicles. Developing a concept for a larger design vehicle (40') allows for unanticipated changes to the type of bus serving transit in Bismarck, as well as, allowing other transit providers who utilize a larger design vehicle within the transit center.

Figure 2 Option 1 Conceptual Layout



* Layout Accommodates Existing CAT/Bis-Man Transit Design Vehicle

SITES LOCATION

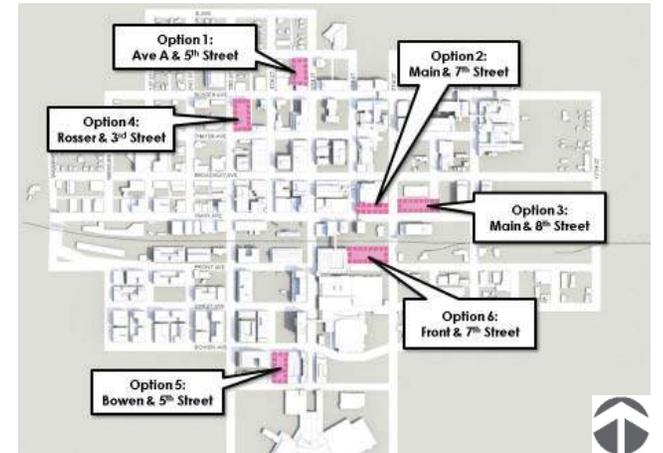
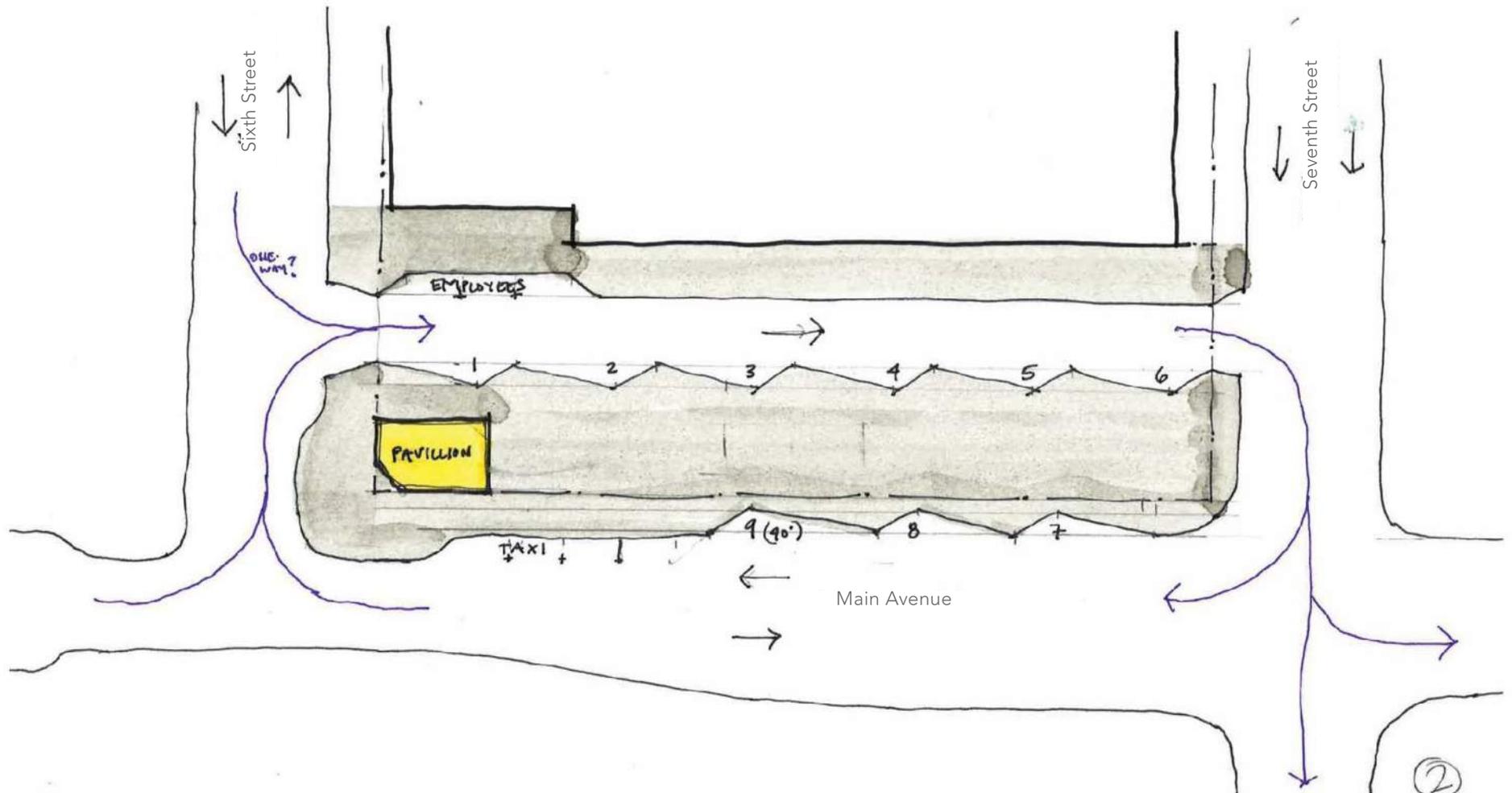
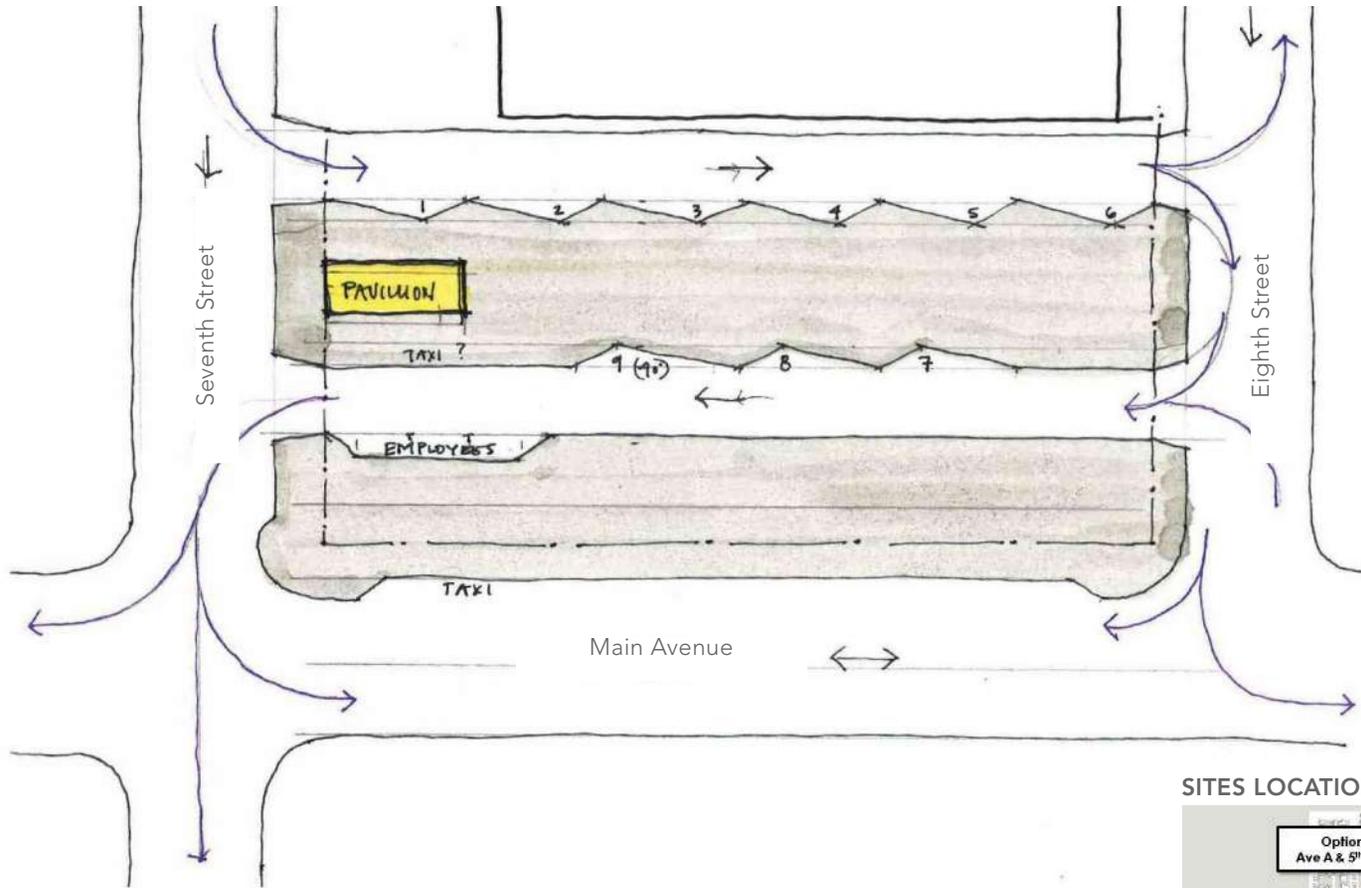


Figure 3 Option 2 Conceptual Layout



* Layout Accommodates Existing CAT/Bis-Man Transit Design Vehicle

Figure 4 Option 3 Conceptual Layout



* Layout Accommodates Existing CAT/Bis-Man Transit Design Vehicle

SITES LOCATION

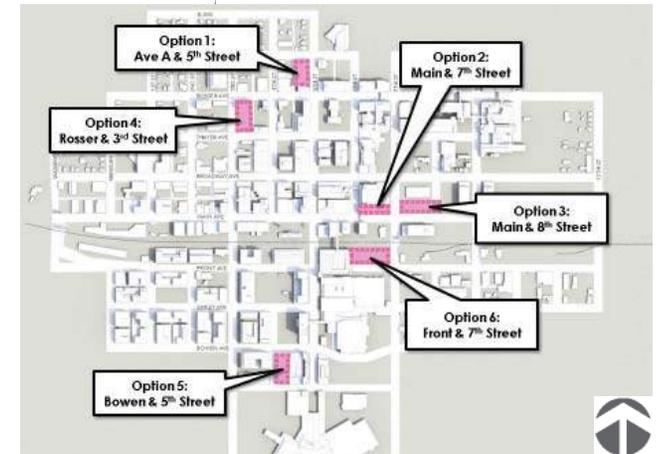


Figure 4 Option 3 Conceptual Layout



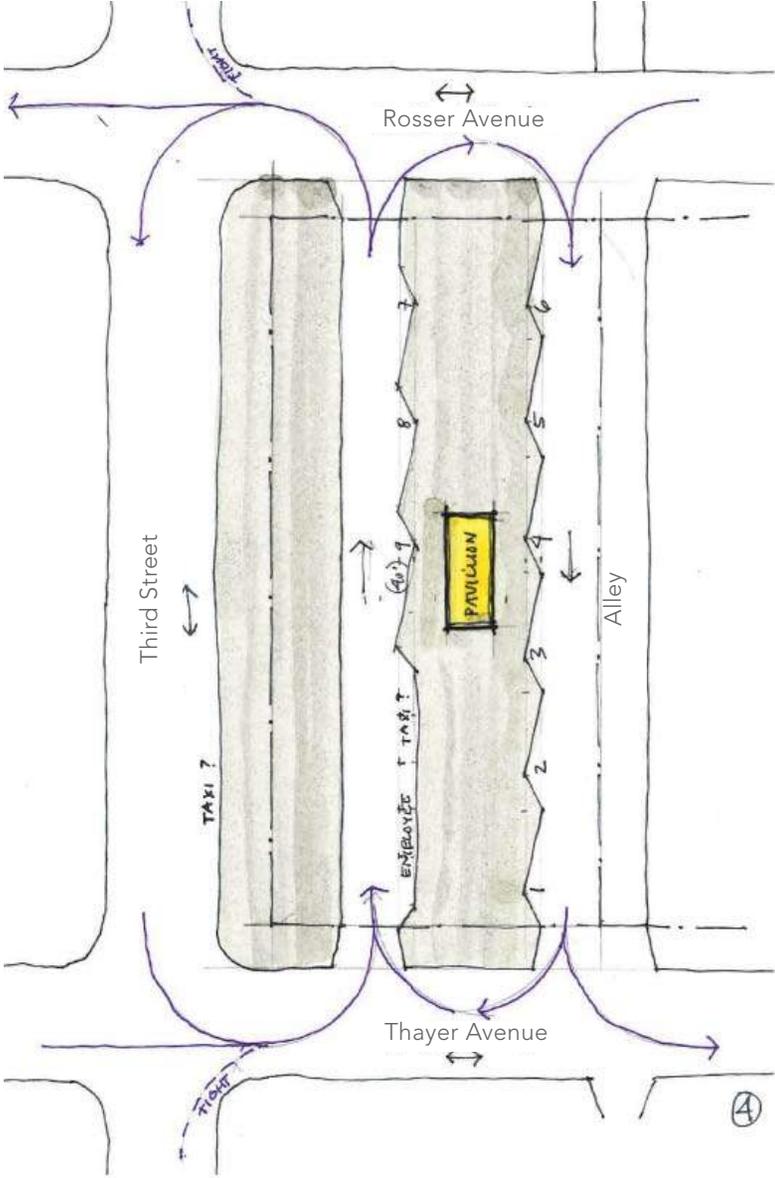
* Layout Accommodates 40' Transit Design Vehicle

Figure 4 Option 3 Conceptual Layout

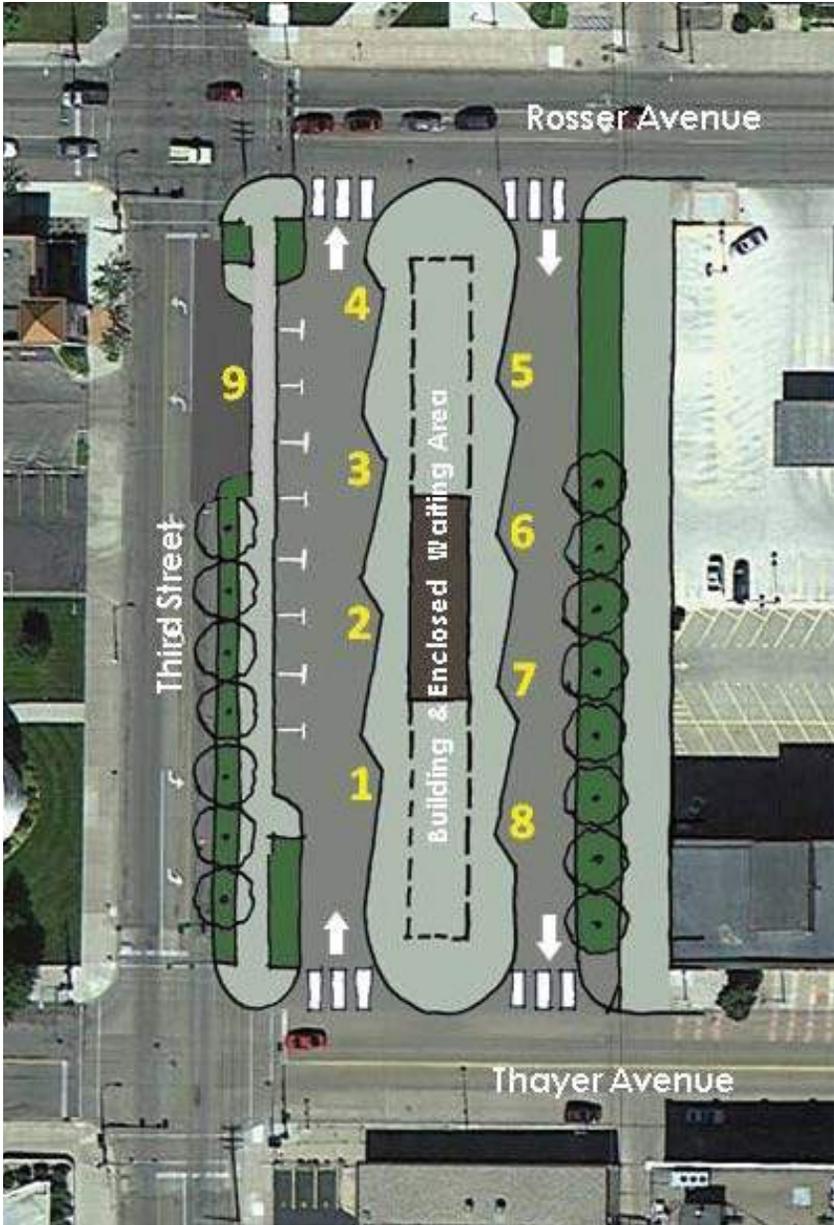


* Layout Accommodates 40' Transit Design Vehicle

Figure 5 Option 4 Conceptual Layout

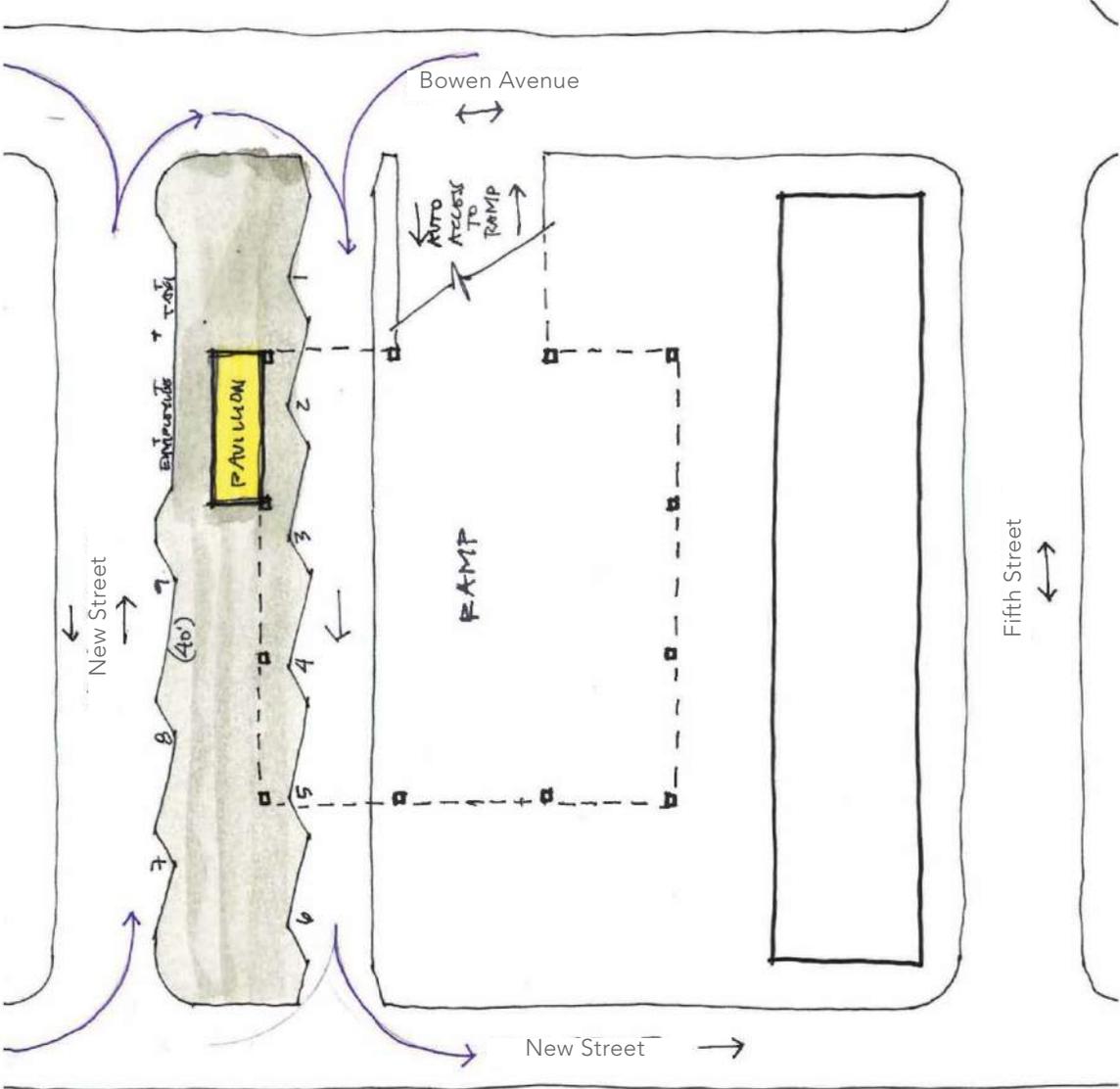


* Layout Accommodates Existing CAT/Bis-Man Transit Design Vehicle



* Layout Accommodates 40' Transit Design Vehicle

Figure 6 Option 5 Conceptual Layout



* Layout Accommodates Existing CAT/Bis-Man Transit Design Vehicle

SITES LOCATION

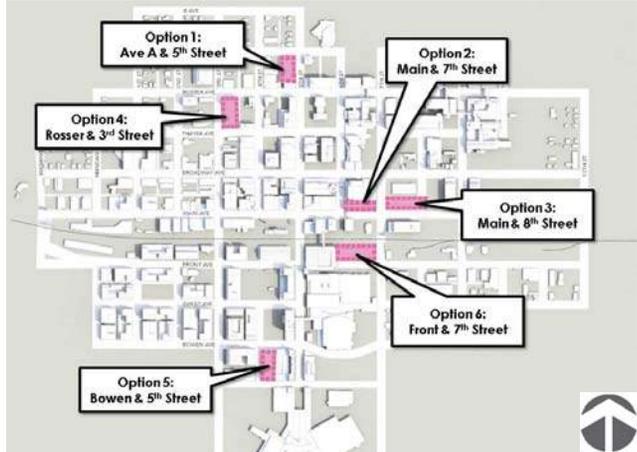


Figure 7 Option 6 Conceptual Layout



- | | |
|------------------------------|----------------------|
| A 12 Bus Bays (40') | E Rail Trail |
| B Center Platform & Building | F Rail Trail Ramp |
| C CAT/Bis-Man Parking | G Stair to Underpass |
| D Taxi | |

* Layout Accommodates 40' Transit Design Vehicle

Existing Transit Operations

A preliminary analysis included an evaluation of potential impacts on existing CAT vehicle in-service run times if the current routes were adapted to one of the proposed transit center sites. As shown in Figure 8, times represent travel only and do not include dwell or circulation at new transit centers. We assume an intra-parking lot travel time of one (1) minute for routes that currently serve Kirkwood Mall but will not in the future. This applies to all routes except B-1, B-2, and E-2, which are assumed to continue to serve the mall after a new transit center is constructed. (For the purposes of this analysis, we assume Route E-2 will continue to serve the 2nd/Indiana loop and Kirkwood Mall.)

Figure 8. Approximate Running Time Impacts for Existing Routes to Serve Transit Center Options

Route Area	Route #	Effect on Route Travel Time (in Minutes)					
		Option 1	Option 2	Option 3	Option 4	Option 5	Option 6
E Bismarck	A-1	- 3	- 3	- 3	- 2	- 1	-2
E Bismarck	A-2	- 6	- 4	- 4	- 5	- 1	-2
SW Bismarck	B-1	+ 6	+ 3	+ 3	+ 8	+ 4	+2
SW Bismarck	B-2	+ 9	+ 7	+ 7	+ 10	+ 1	+5
NE Bismarck	C-1	Routes do not serve Downtown or Kirkwood Mall.					
NE Bismarck	C-2						
NE Bismarck	D-1						
NE Bismarck	D-2						
Express Kirkwood to Gateway	E-1	- 5	- 3	- 3	- 4	- 3	-2
Express Gateway to Kirkwood	E-2	+ 2	+ 6	+ 6	+ 2	- 2	+4
Bismarck & Mandan	M-1	- 1	- 1	-	-	- 2	-2
Mandan & Bismarck	M-2	- 1	- 1	- 1	-	- 2	-2

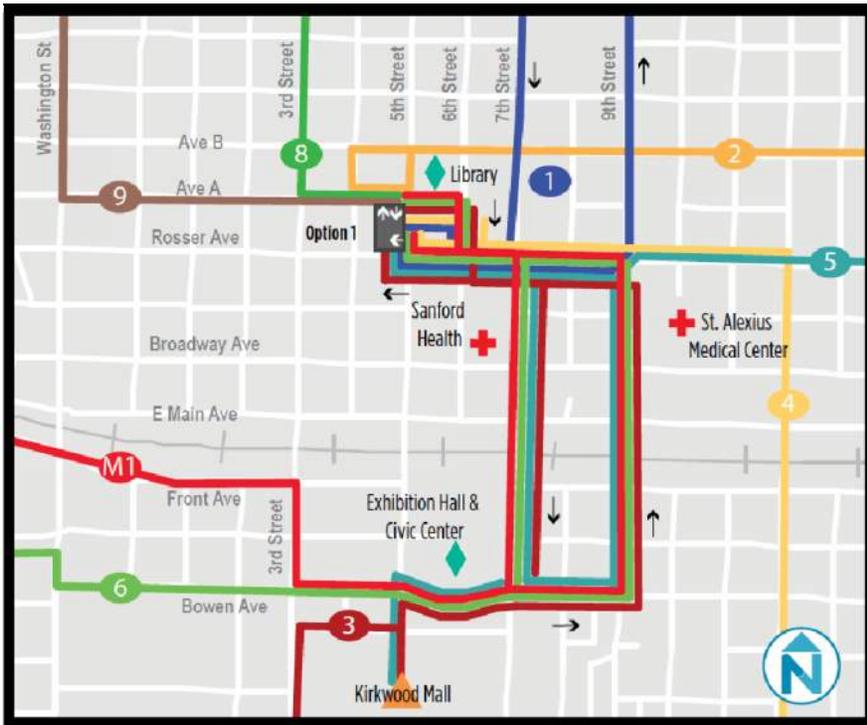
The calculations suggest that by interlining specific routes with routes that are different from their current pairings, route run times would generally be minimally impacted. For example, longer run times on the southwest Bismarck routes could be balanced with shorter run times on the east Bismarck routes and overall shorter run times from northeast Bismarck routes that currently do not connect to Kirkwood Mall. Some small portions of the existing routes could also be streamlined to allow for improved run times.

Future Transit Scenario

Based on the preferred (significant changes) modifications presented in Mobility 2017, Nelson\Nygaard evaluated possible ways to serve the new transit facility sites. The analysis shows that all of the facilities can be served without significant changes to the overall preferred transit scenario, although moderate changes will be needed to allow for access to primary downtown destinations. Option 6 presents the greatest challenge in terms of access to the site for buses, but this can be mitigated. These are shown in Figure 9

Figure 9 Future Transit Scenario, Possible Options to Serve Transit Centers

Option 1



Option 2



Figure 9 Future Transit Scenario, Possible Options to Serve Transit Centers

Option 3



Option 4



Figure 9 Future Transit Scenario, Possible Options to Serve Transit Centers

Option 5



Option 6



Preliminary Site Functional Evaluation

A cursory evaluation of these sites suggest all of them have opportunities to serve as functional facilities to meet at least the basic program proposed by transit staff. This evaluation is shown in Figure 10.

Figure 10 Preliminary Transit Center Site Evaluation

Transit Center Site Evaluation	Option 1 Ave A & 5 th	Option 2 Main & 7 th	Option 3 Main & 8 th	Option 4 Rosser & 3 rd	Option 5 Bowen & 5 th	Option 6 Front & 7 th
Location Criteria						
Access to existing multiple trip generators (jobs, services, and retail)	●	●	●	●	●	●
Compatibility with existing adjacent uses	●	●	●	●	●	●
Comfortable and safe environment	●	●	●	●	●	●
Connection with other modes of transportation	●	●	●	●	●	●
Site Criteria						
Ability to accommodate bus transit vehicles	●	●	●	●	●	●
Ability to accommodate support facilities (waiting areas, restrooms, driver facilities)	●	●	●	●	●	●
Ability to meet expanded demand and space to meet unanticipated future needs	●	●	●	●	●	●
Impact on operations (deadhead miles, re-routing, operating costs)	●	●	●	●	●	●
Impacts on ridership (access, convenience, comfort, personal safety)	●	●	●	●	●	●
Circulation Criteria						
Existing/future route service	●	●	●	●	●	●
Transit vehicle site access	●	●	●	●	●	●
Pedestrian and bicycle site access	●	●	●	●	●	●
Auto/truck impacts (signalization, travel lanes, on-street parking)	●	●	●	●	●	●
Financial						
Site acquisition cost	TBD*	●	●	●	●	●

Legend:

● Good - Highest Ranking ● Fair - Medium Ranking ● Poor - Lowest Ranking (Potential Fatal Flaw)

*Proposed site could be expanded to allow for all vehicles to be accommodated off-street. Costs must be determined for the purchase of additional land.

Conclusion

Based on this analysis, Options 2, 3 and 5 were identified to have constraints that may preclude them from consideration for a transit facility (including capacity and site acquisition costs). Option 1 costs are indeterminate at this time, depending on whether the City of Bismarck would be able to use the County-owned site and purchase an adjacent lot to allow for the full transit program to be accommodated on site.

Based on cost and operational factors, Options 4 and 6 offer some advantages to the City. If cost are not a significant factor, Option 4 offers perhaps the best combination of location, operations functionality, and future expansion potential among the six considered sites. Nevertheless, other factors such as land use goals, funding sources that allow for multimodal transit/garage construction, or environmental requirements/costs may suggest that Option 6 is better suited for a downtown transit center.

Option 6 has the capacity to accommodate up to twelve 40-foot vehicles, as well as, a future parking ramp. Located along an active rail line with the potential to serve future passenger trains, the site is in close proximity to planned development in Bismarck, and offers good bike access. Enhanced efforts will be required to improve pedestrian access to downtown.

The site's greatest challenge will be in route design and operations because (1) only 7th Street, 9th Street, and Washington Street offer a rail grade separation and (2) because the site is only accessible from Front Avenue. These challenges can be overcome through service planning, but may also merit some possible capital improvements, such as a new signal or turning lane on Front Avenue (at the entrance to the facility) or signal preemption at 7th and 9th Streets.

SIXTH STREET RAMP SUGGESTED CHANGES

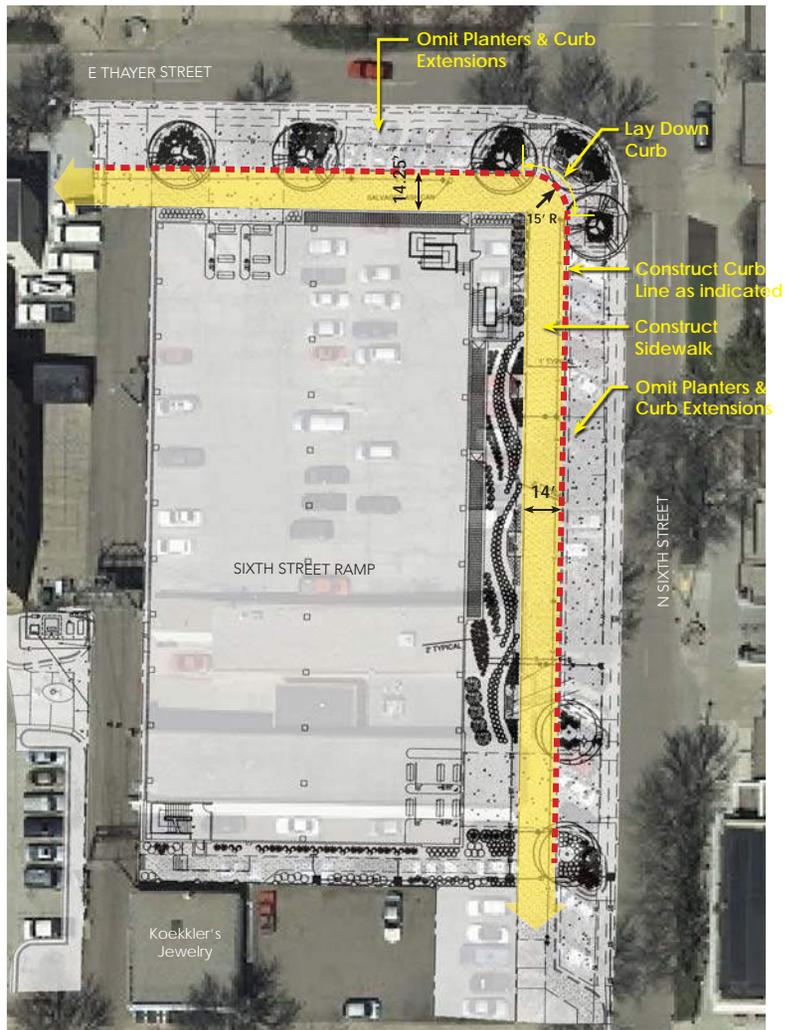


EXISTING ONE-WAY SIXTH STREET

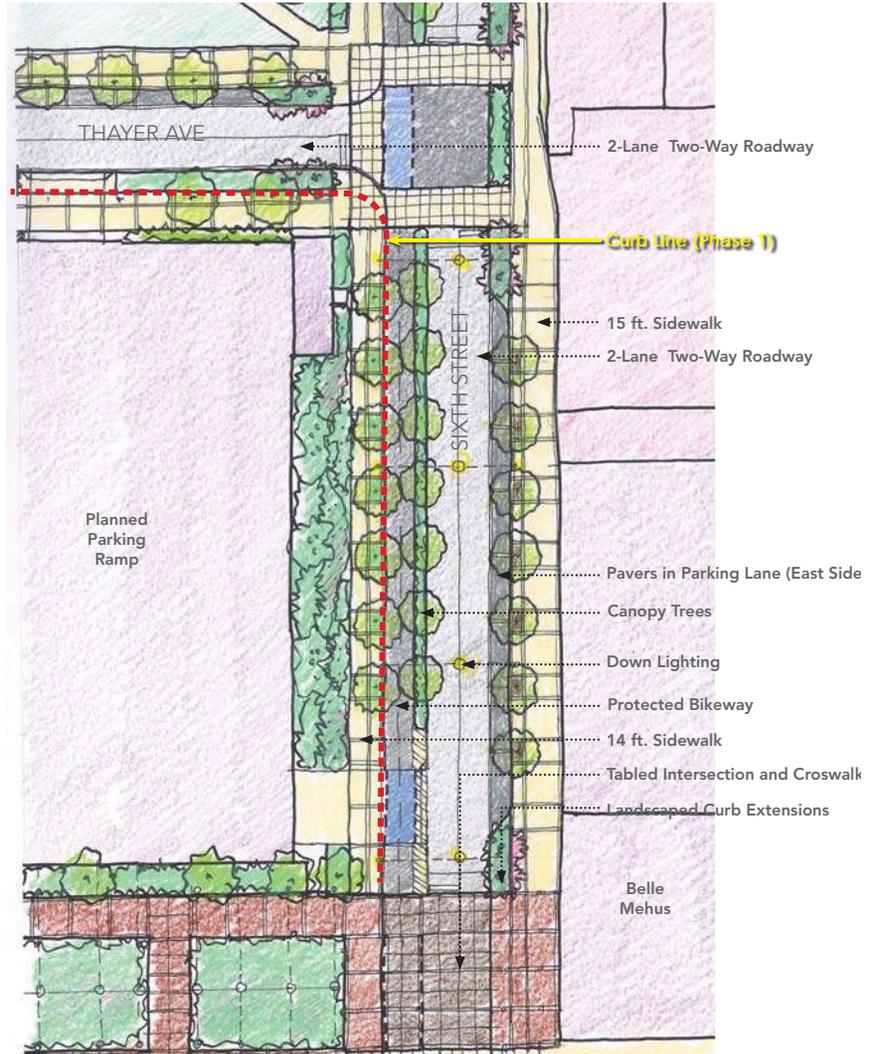


PROPOSED TWO-WAY SIXTH STREET

DOWNTOWN BISMARCK SUBAREA PLAN
PROPOSED BELLE MEHUS GREEN- GATHERING SPACE AND TWO-WAY 6TH STREET AND THAYER STREET

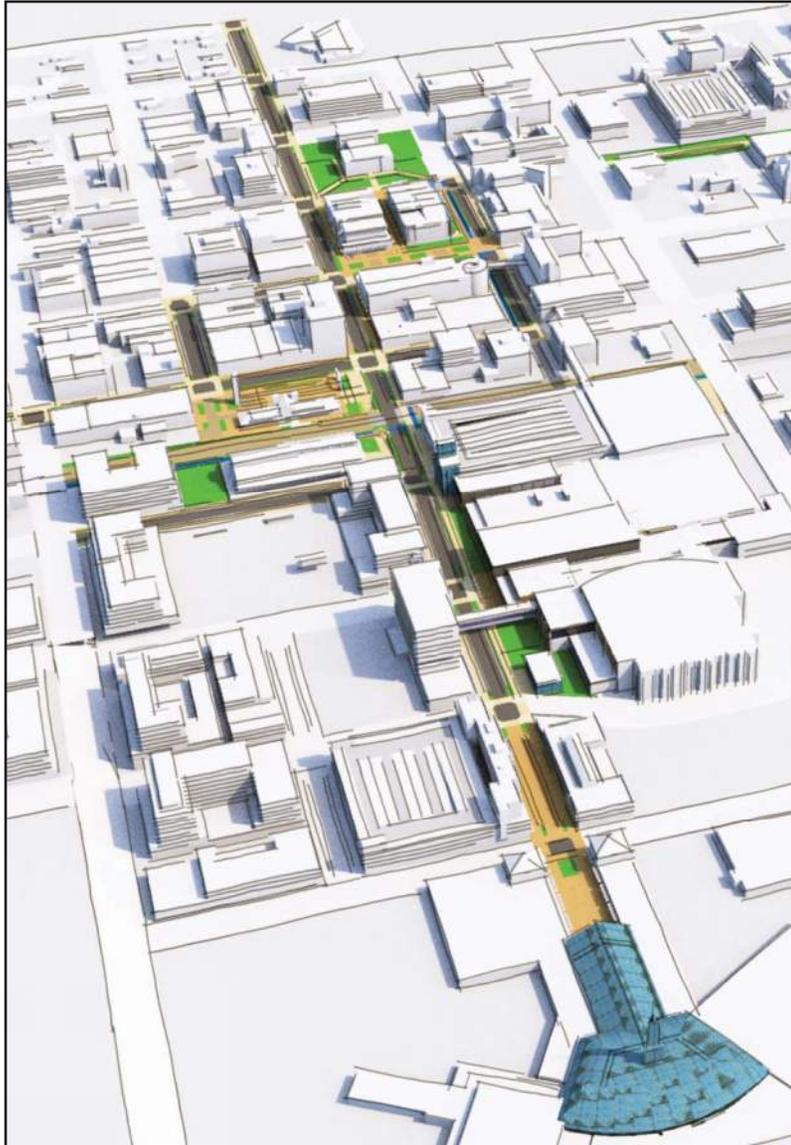


6TH STREET PARKING RAMP- SITE LANDSCAPE PLAN (ULTEIG)
RECOMMENDED PHASE 1- 6TH STREET AND THAYER STREET



DOWNTOWN BISMARCK SUBAREA PLAN
PROPOSED FUTURE 6TH STREET AND THAYER STREET

REFINED CONCEPT AND DRAFT IMPLEMENTATION



Downtown Bismarck Subarea Study

Bismarck-Mandan MPO
City of Bismarck, North Dakota

August 13-14, 2013

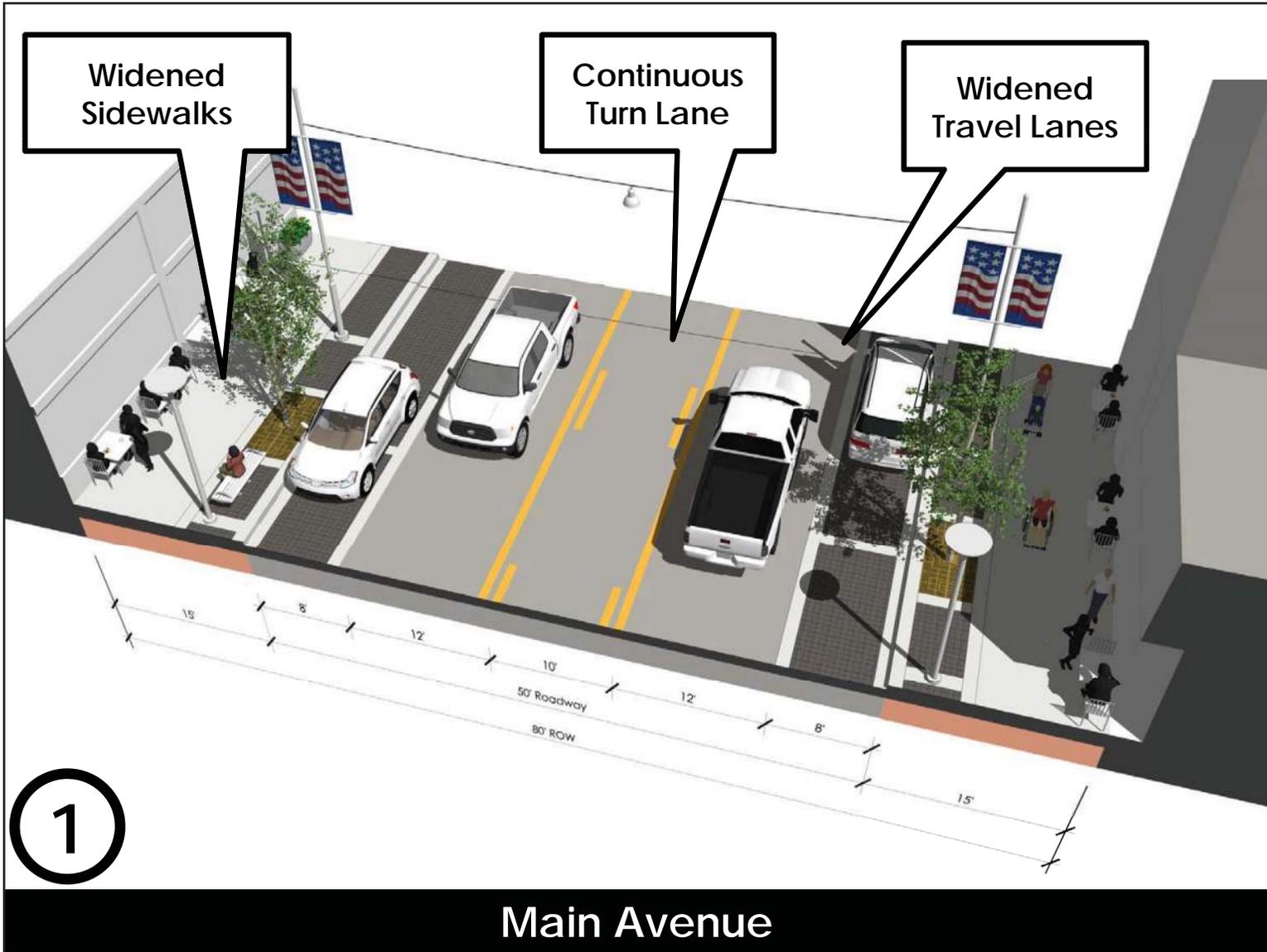
Refined Concept and Draft Implementation Plan

CRANDALL ARAMBULA
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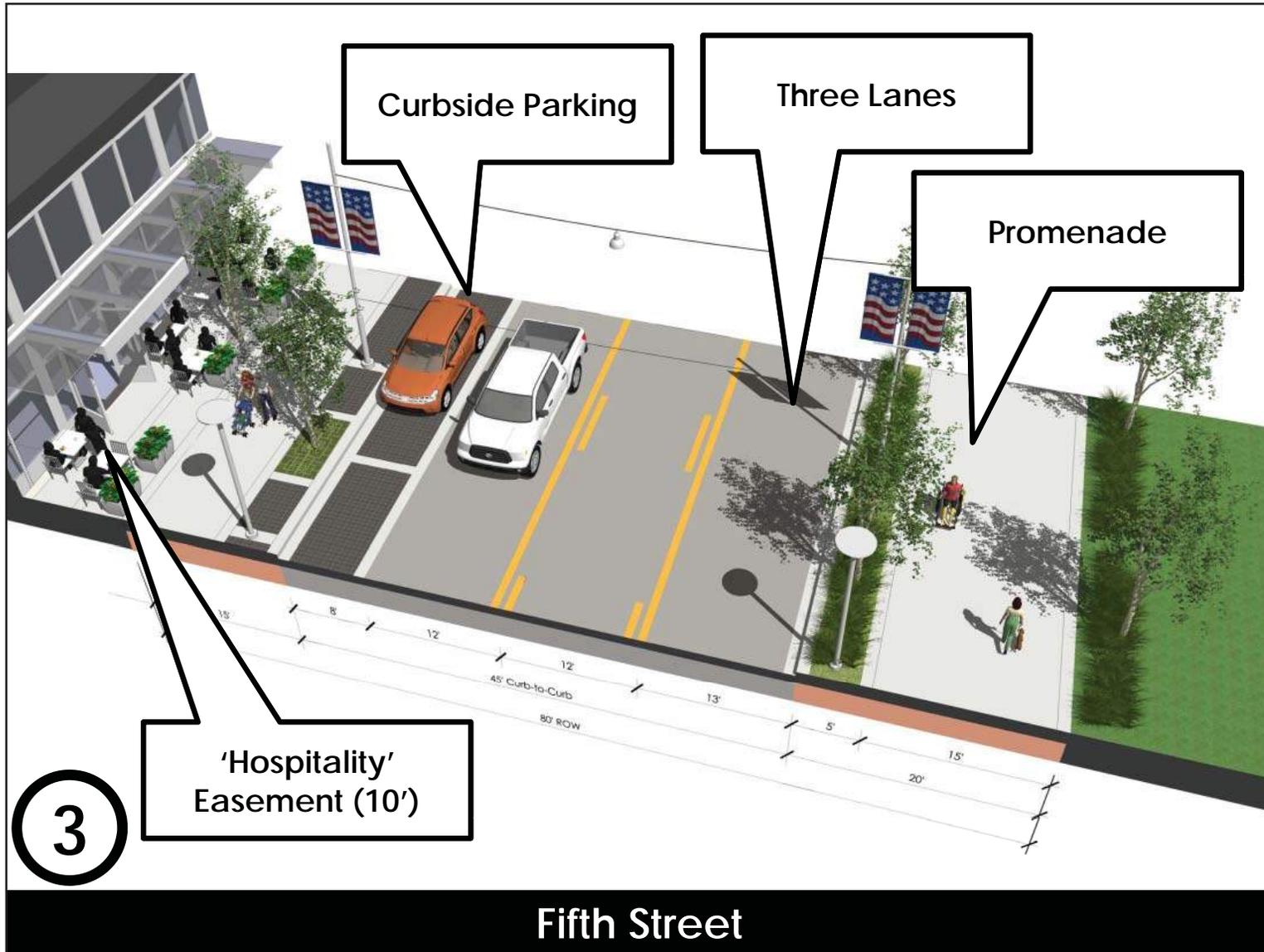
Game Changers





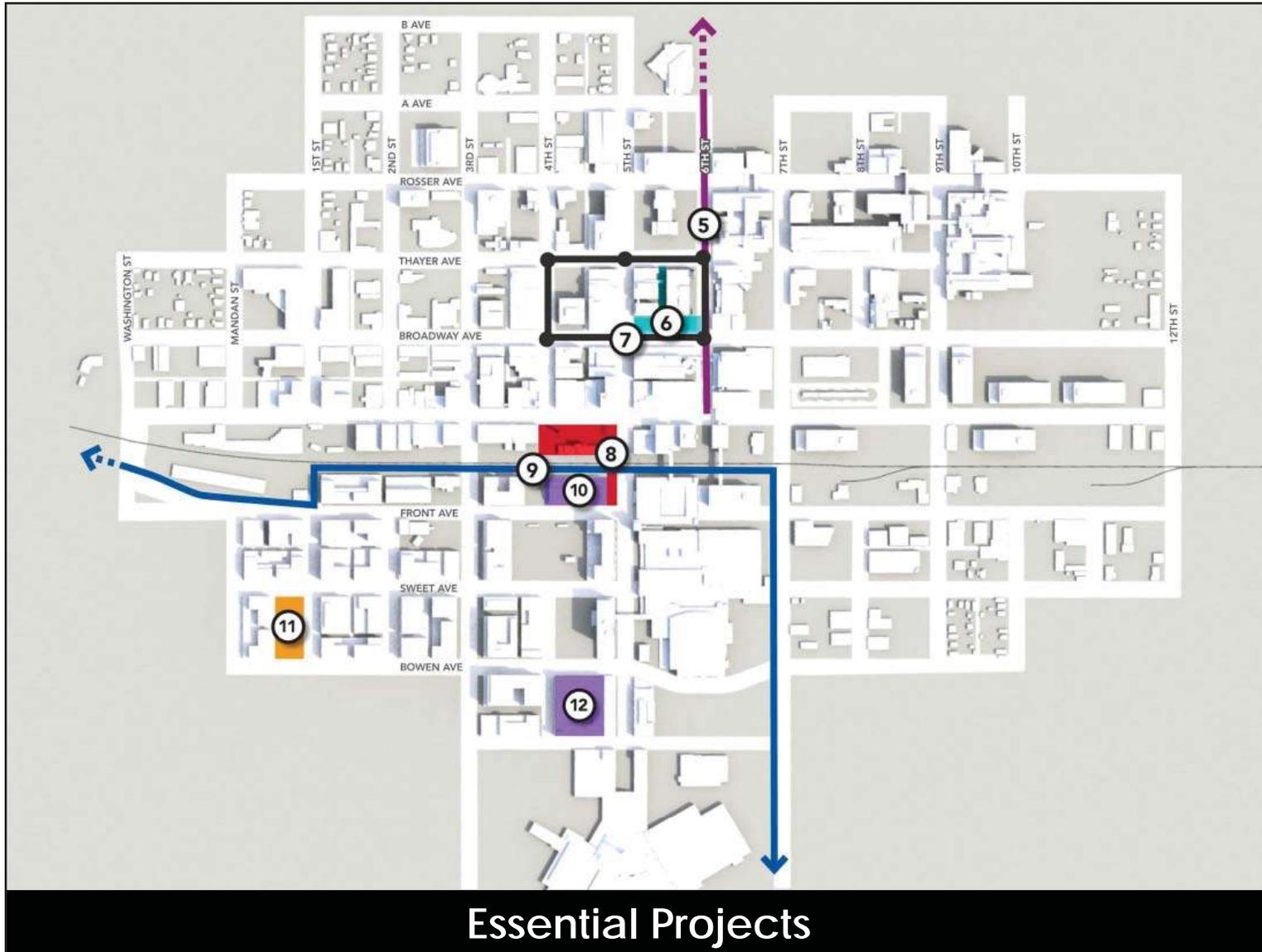
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Depot Plaza



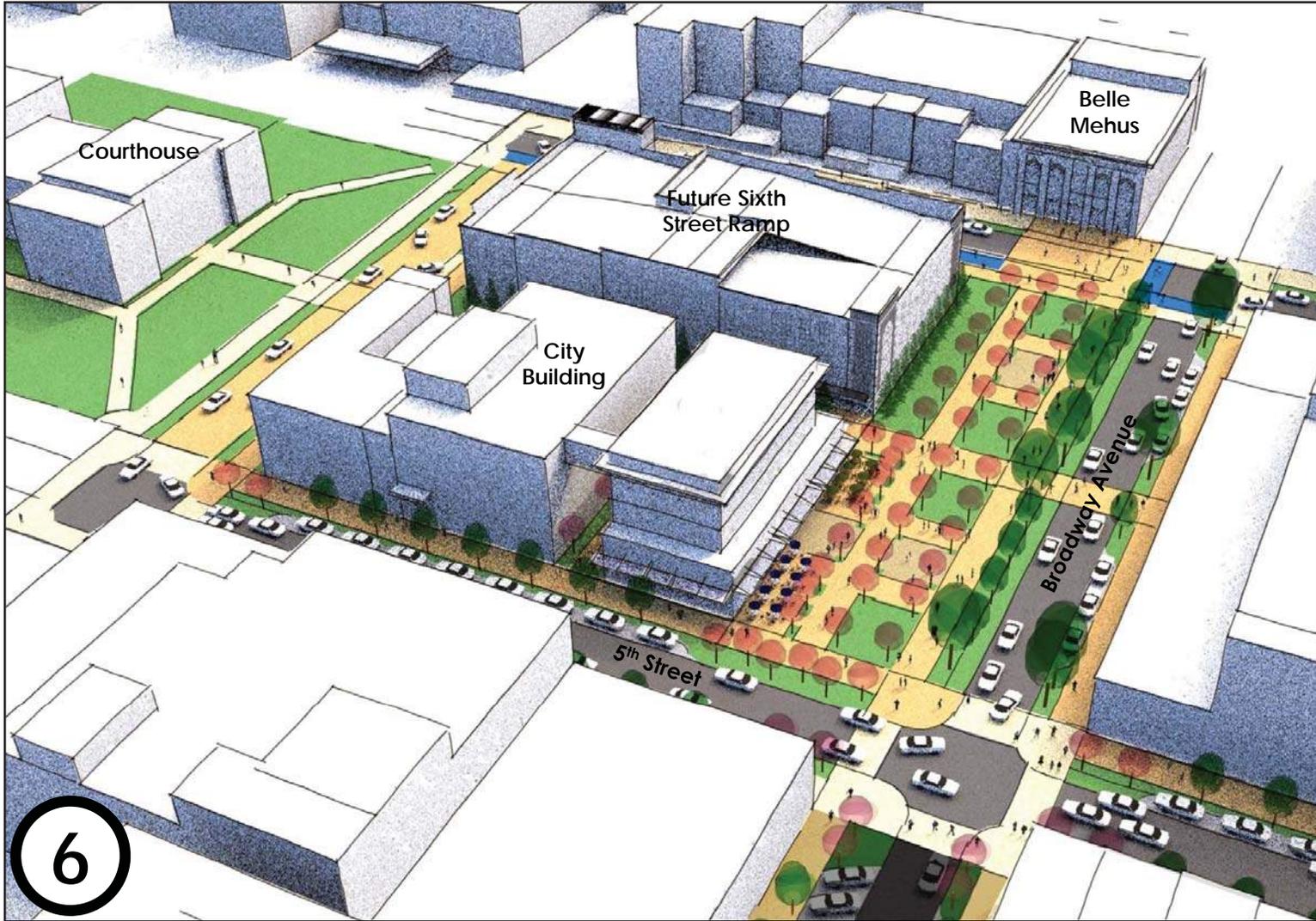


Convention Hotel



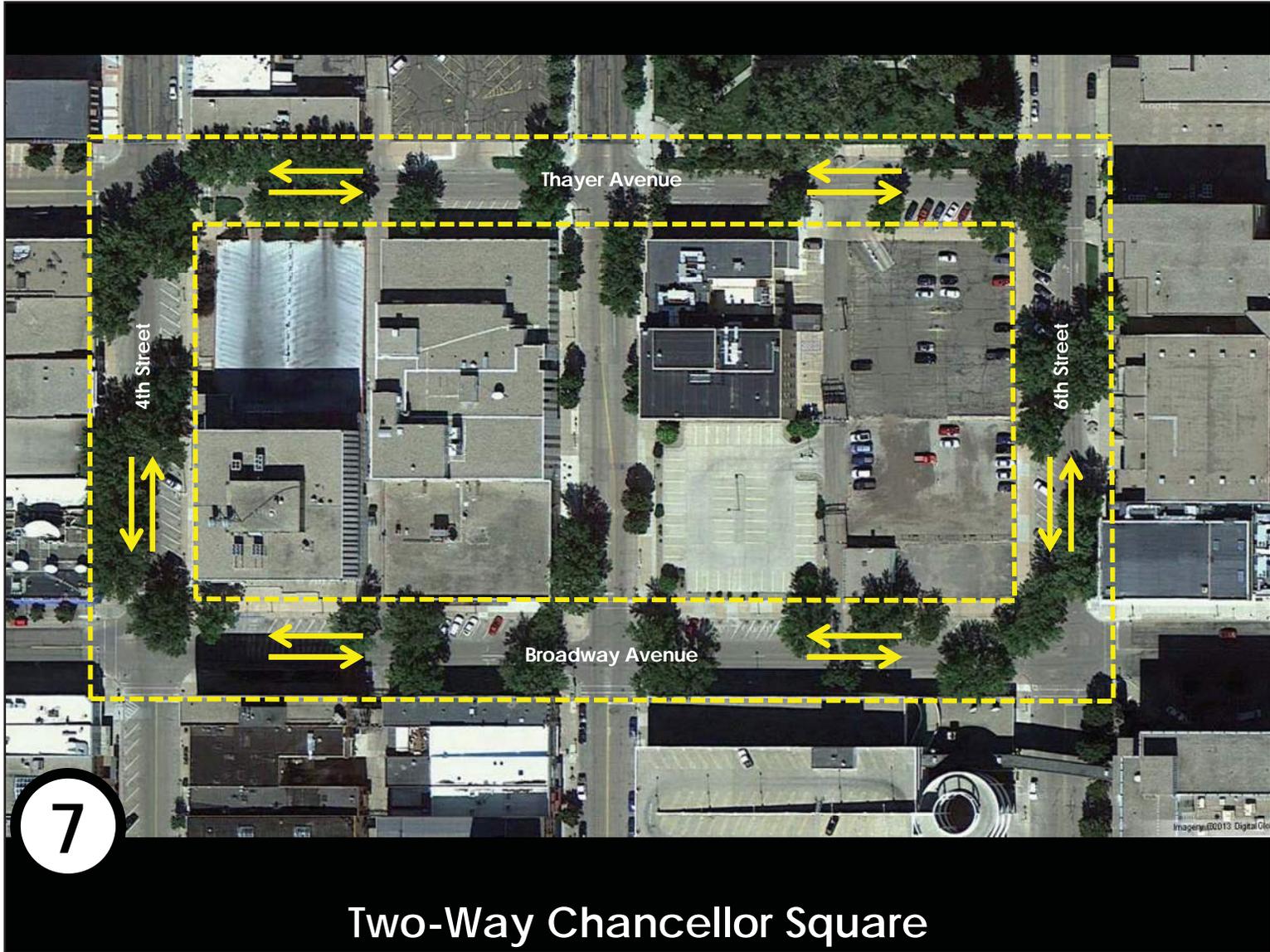


Sixth Street Cultural Trail



6

Mehus Commons

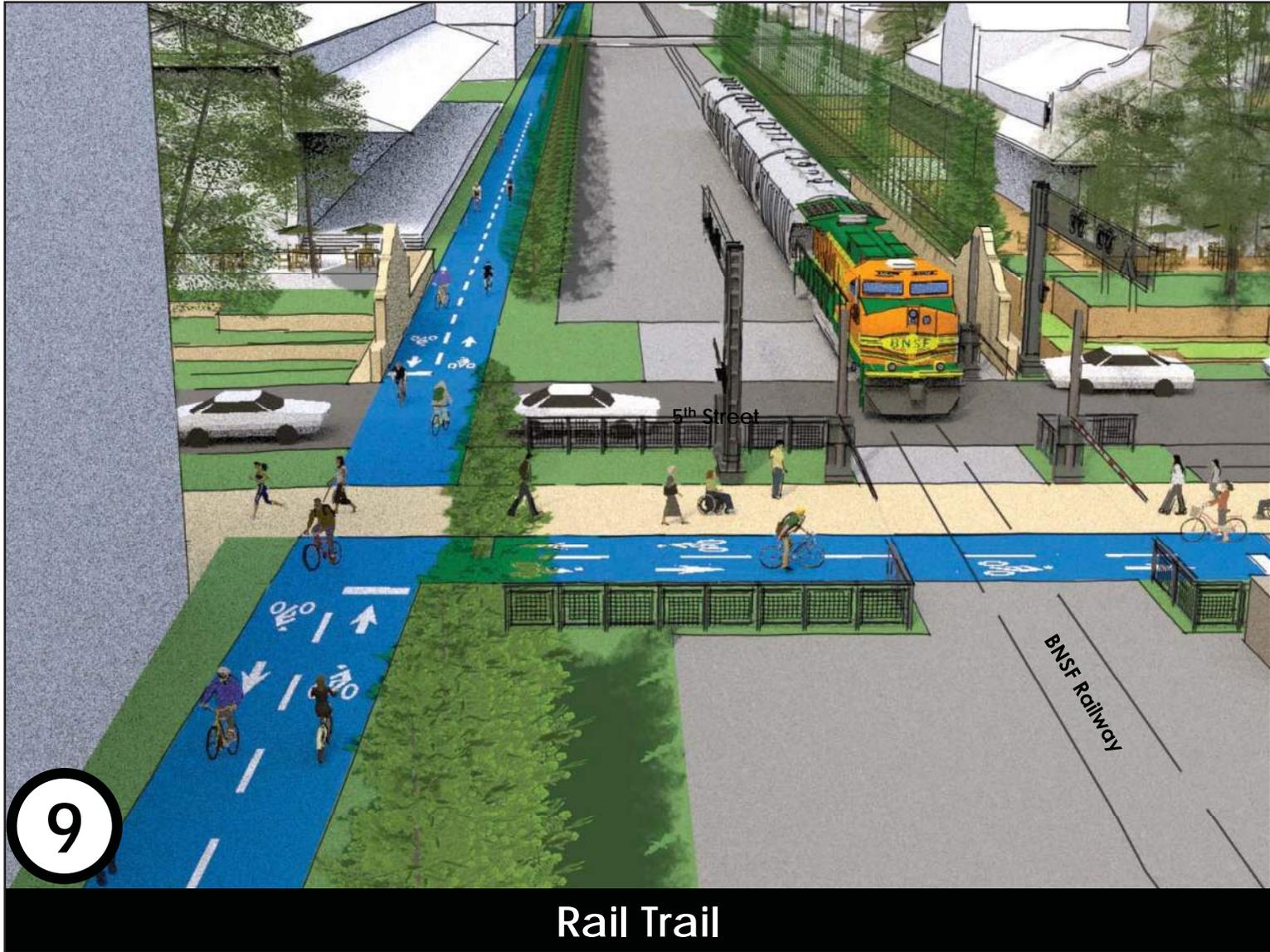


Two-Way Chancellor Square

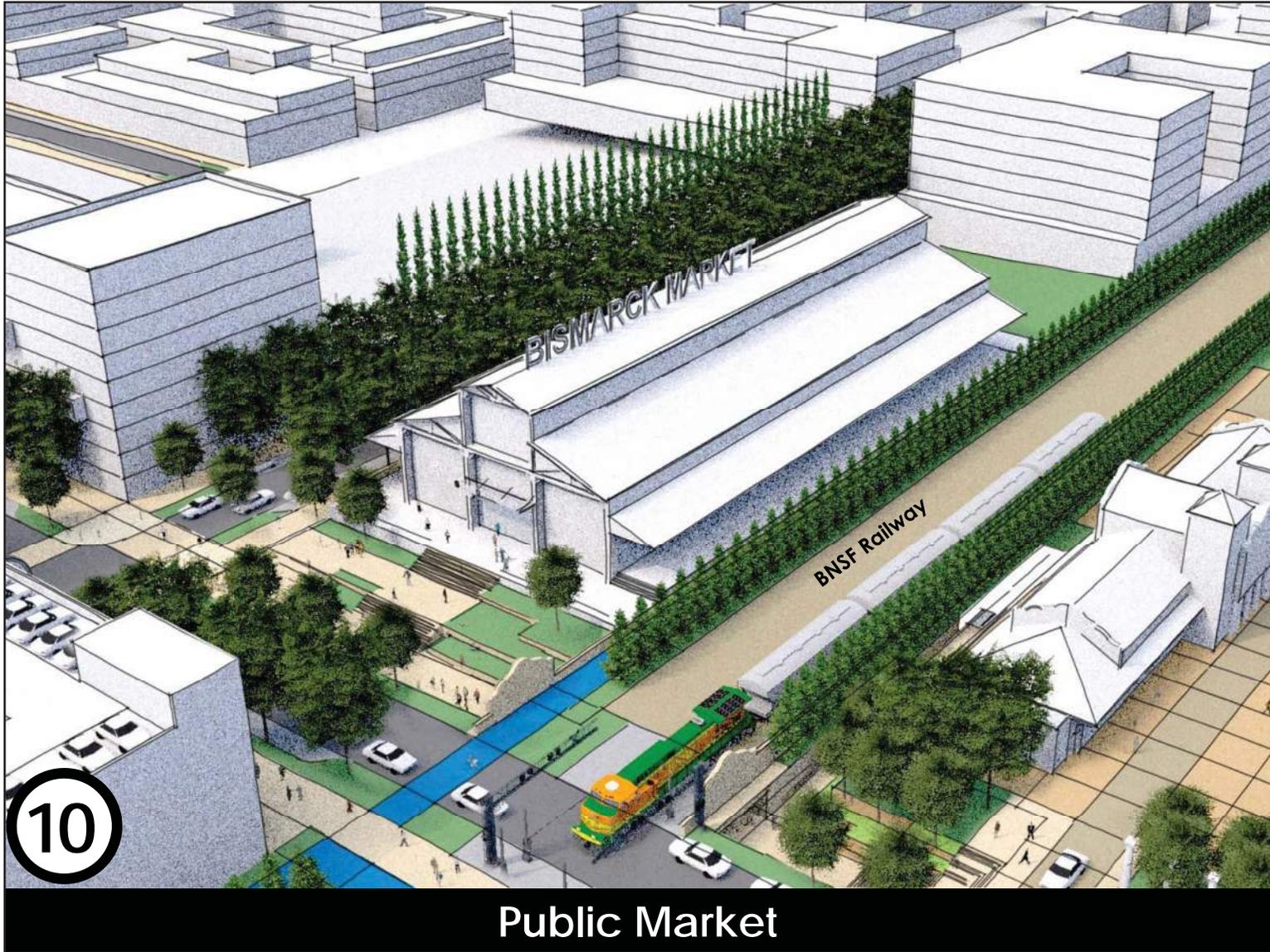


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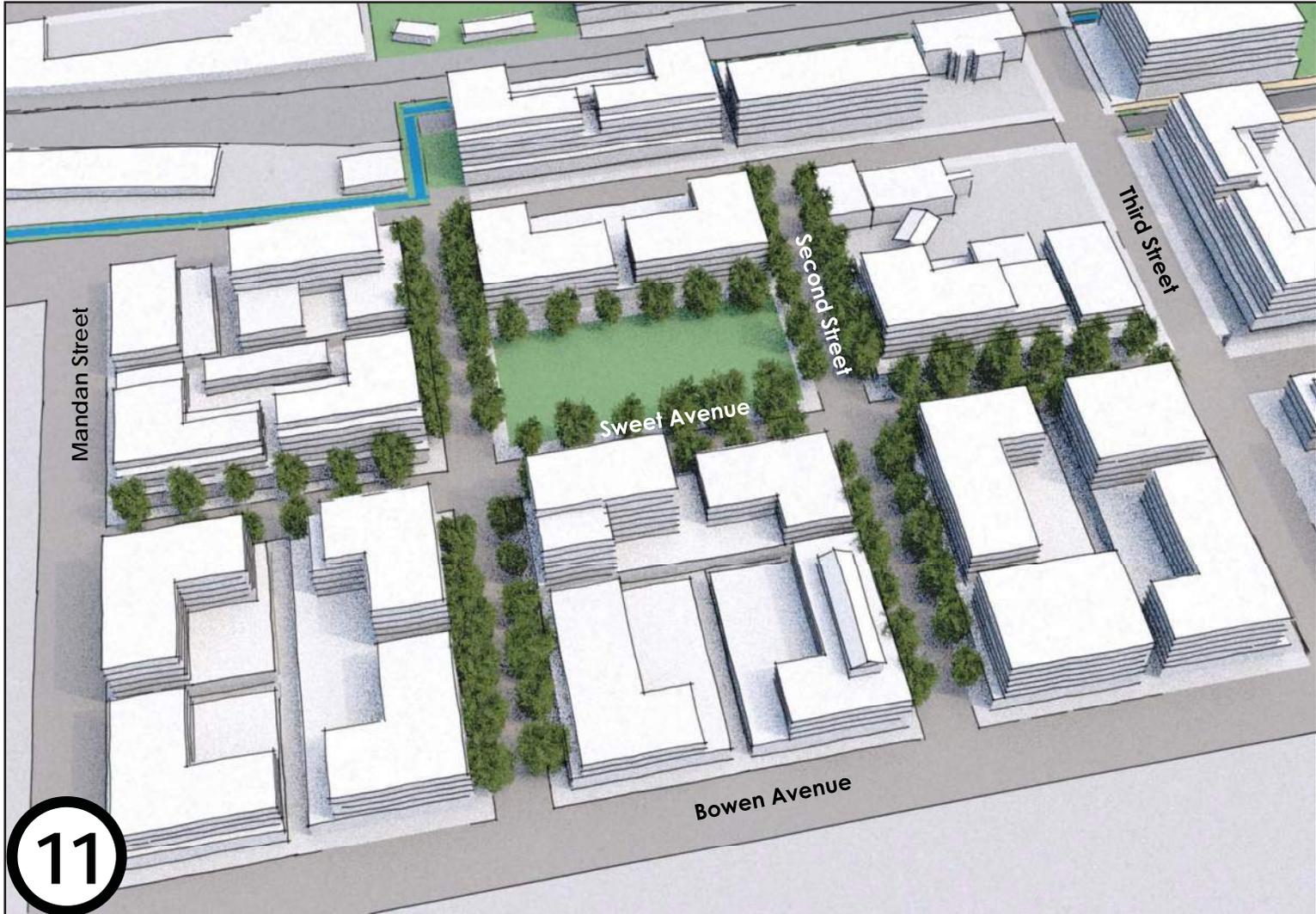
Fifth Street Pedestrian Underpass



Rail Trail

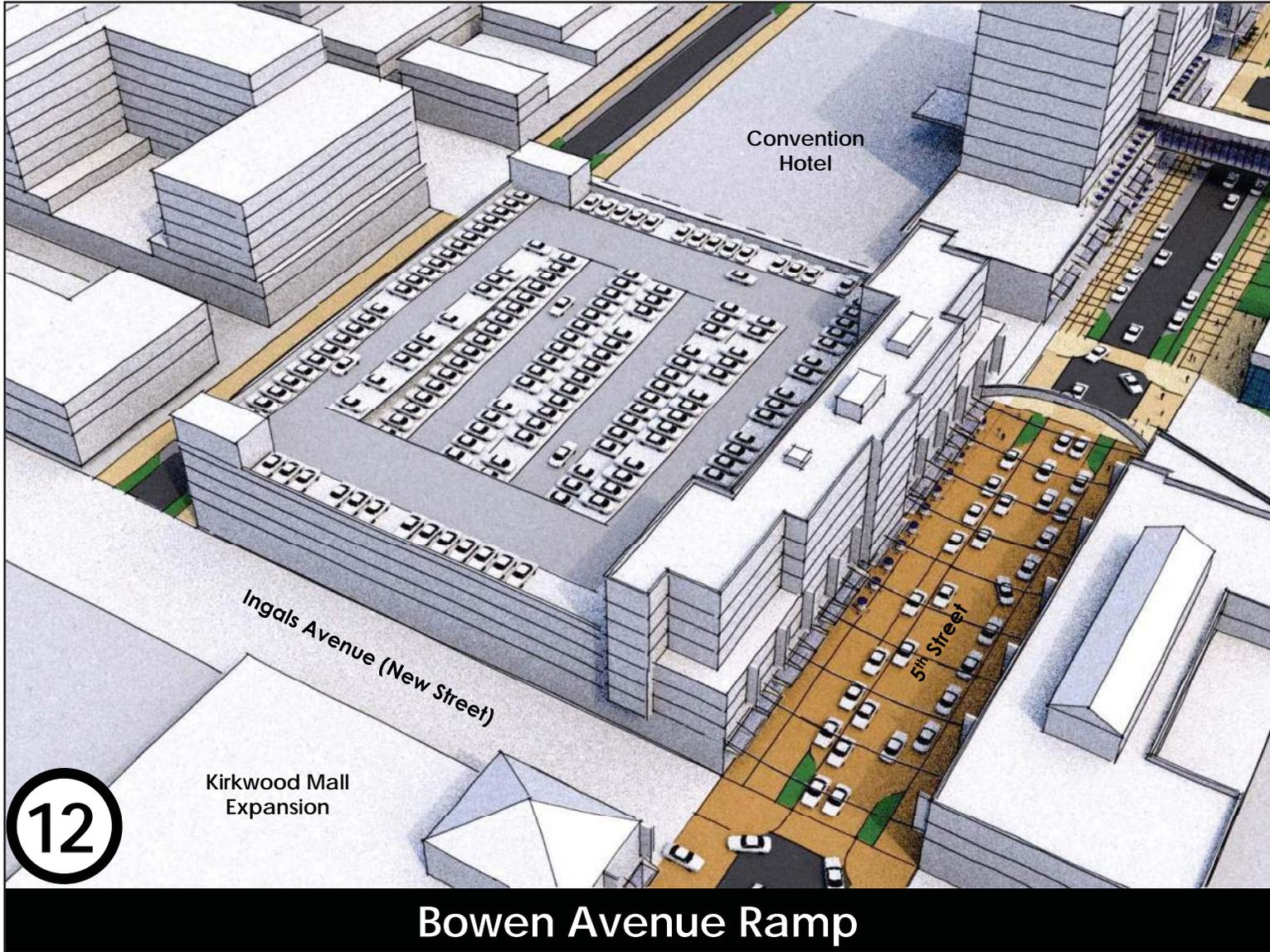


Public Market



11

Neighborhood Park



Bowen Avenue Ramp

Process & Schedule

2013

Feb | Mar | Apr | May | Jun | July | Aug | Sept | Oct | Nov

1 Starting

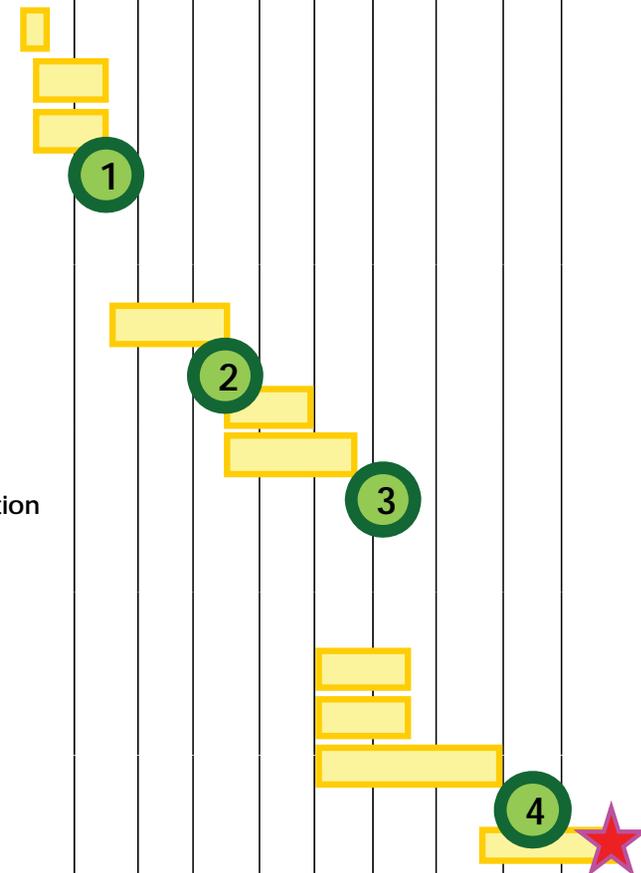
- Project Team Kick-off Meeting
- Collect and Review Background Information
- Prepare Opportunities and Constraints Analysis
- Meetings & Public Workshop 1—Identify Study Criteria

2 Designing

- Develop Alternatives
- Meetings & Public Workshop 2—Present Alternatives
- Refine Alternatives /Technical Reviews
- Draft Implementation Plan
- Meetings & Public Workshop 3—Refined Concept /Implementation

3 Implementing

- Finalize Concept and Implementation
- Recommend Regulatory Updates
- Draft Report and MPO Review
- Meetings & Public Hearings
- Prepare Final Report



MEETINGS/PUBLIC WORKSHOP #3 SUMMARY

Meeting Summary

Downtown Bismarck Subarea Study
Meetings & Public Workshop #3
August 13-15, 2013

3

Summary

The third of the Downtown Bismarck Subarea Study committee/stakeholders meetings and public workshop were held August 13 to August 15, 2013. During that period, the consultants presided over a total of eleven meetings that included the Technical Advisory Committee, Steering Committee, the City Commission and numerous stakeholder groups at the City/County Building located at 221 N. 5th Street.

The Public Workshop #3 was held on August 14 from 5:30-7:00pm at the Civic Center Prairie Rose Room 205 and was attended by approximately 75 community members.

Additional public input was collected via on-line response sheets up to September 2nd. The on-line response included an additional 26 response sheet submittals and comments. The tally on the following page is a summary of all public response sheet submittals.

Meetings and Workshop Purpose

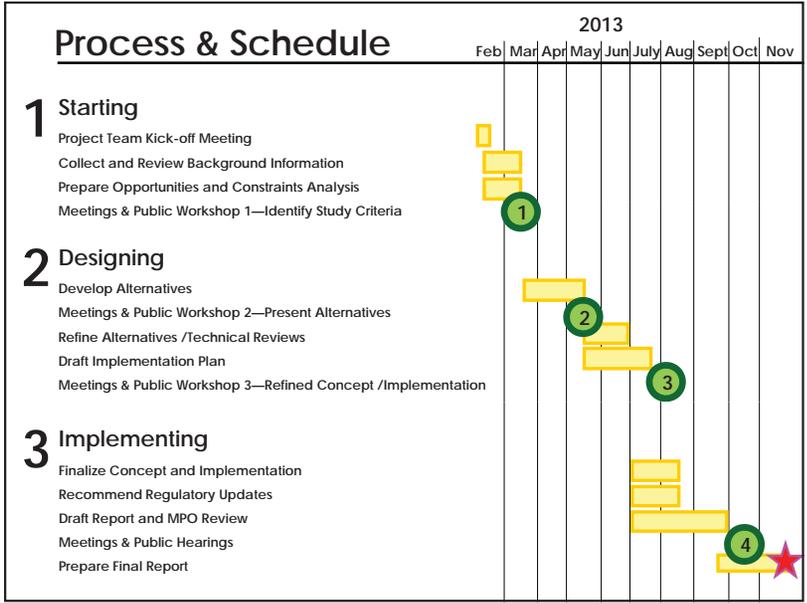
The purpose of the meetings and workshop was to:

- Review Results from Public Workshop #2
- Present Refined Concepts and a Draft Implementation Plan
- Answer Questions
- Identify Preferences for Game-Changer and Essential Projects

Each meeting and the Public Workshop began with the consultants presentation of the project process and schedule (shown above right), a summary of Public Workshop #1 and #2, and a review of the game-changer projects and essential projects identified in the implementation plan. Following the presentation, attendees discussed the projects over table maps. The workshop portion concluded with each table summarizing their discussion. Written response sheets (shown right) were provided to all with the purpose of documenting preferences for four game-changer projects and an additional eight essential projects.

The following pages include:

- Response Sheet Tally (Meetings, Workshop & On-line submissions)
- Response Sheet Comments
- Table Reports



Project Process & Schedule

Response Sheet

Downtown Bismarck SubArea Study
 Meetings & Public Workshop #3
 August 13-14, 2013

3

Implementation Strategy

- A. Do you agree with the Game Changers?**
- 1. Main Avenue Yes No
 - 2. Depot Plaza Yes No
 - 3. Fifth Street Yes No
 - 4. Convention Hotel Yes No
- Other Project: _____



Game Changers

- B. Do you agree with the Essential Projects?**
- 5. Sixth Street Cultural Trail Yes No
 - 6. Mehus Commons Yes No
 - 7. Chancellor Square Yes No
 - 8. Fifth Street Underpass Yes No
 - 9. Rail Trail Yes No
 - 10. Public Market Yes No
 - 11. Neighborhood Park Yes No
 - 12. Bowen Avenue Ramp Yes No
- Other Project: _____



Essential Projects

Comments

Please note additional comments below or use the back of this sheet:

Name (optional): _____

If you need additional time to respond, please return your comments through:
MAIL: 520 SW Yamhill, Roof Suite 4, Portland, OR 97204 **WEB:** www.DowntownBismarckStudy.com/public-meetings

Response Sheet

Response Sheet- Game-Changer and Essential Projects

A. Do you agree with the Game Changers?

- 1. Main Avenue Yes No
- 2. Depot Plaza Yes No
- 3. Fifth Street Yes No
- 4. Convention Hotel Yes No

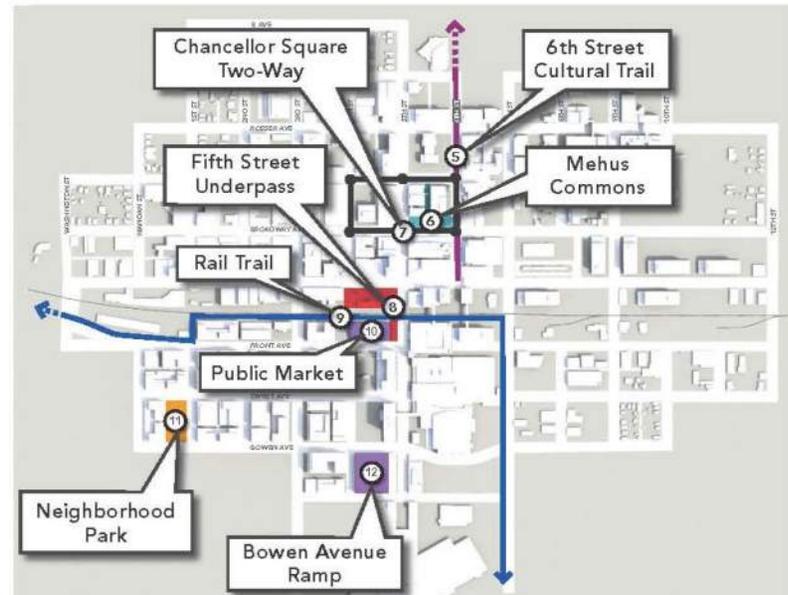
Other Project: _____



Game Changers

B. Do you agree with the Essential Projects?

- 5. Sixth Street Cultural Trail Yes No
- 6. Mehus Commons Yes No
- 7. Chancellor Square Yes No
- 8. Fifth Street Underpass Yes No
- 9. Rail Trail Yes No
- 10. Public Market Yes No
- 11. Neighborhood Park Yes No
- 12. Bowen Street Ramp Yes No



Essential Projects

Response Sheet Tally- Game-Changer Projects

1. Main Avenue

69 Yes **3** No **0**



2. Depot Plaza

64 Yes **8** No **0** Other



3. 5th Street

69 Yes **3** No **0** Other



4. Convention Hotel

68 Yes **4** No **0** Other



Response Sheet Tally- Essential Projects

5. Sixth Street Cultural Trail

57 Yes **12** No **0** Other



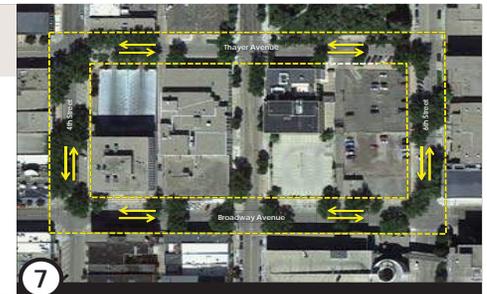
6. Mehus Commons

67 Yes **4** No **0** Other



7. Two-Way Chancellor Square

69 Yes **3** No **0** Other



8. Fifth Street Underpass

65 Yes **6** No **0** Other



Response Sheet Tally- Essential Projects

9. Rail Trail

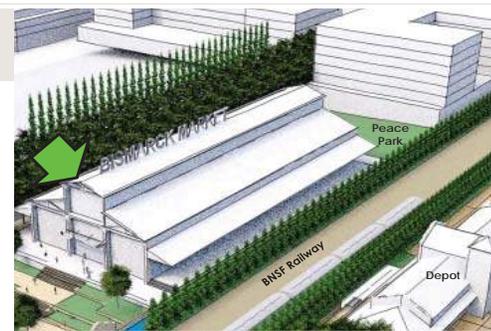
62 Yes **10** No **0** Other



10. Public Market

56 Yes **12** No **0** Other

Other _____



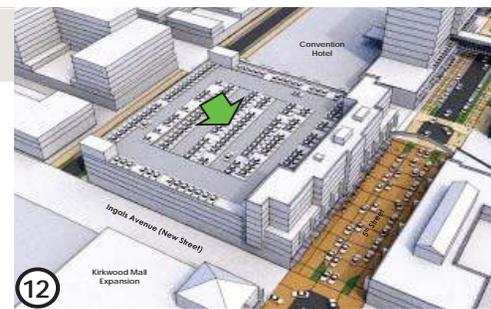
11. Neighborhood Park

59 Yes **11** No **0** Other



12. Bowen Ramp

65 Yes **7** No **0** Other



Response Sheet Comments

Below is a summary of the comments mentioned in the response sheets collected from the committees, stakeholder meetings and Public Workshop #3. and additional online submissions via the project web site.

Main Avenue

- I support Main Avenue with diagonal parking

Depot Plaza

- I do, however feel that the parking solution to the Depot Plaza poses an uninviting atmosphere of parking a few blocks away. North Dakotans are used to close parking- this may deter some from Fiesta's business

Fifth Street

- I would not be in favor of taking 5th Street parking away except for south of the railroad tracks
- 5th Street from Main to Broadway should be bricked or tiled- No driving- Just pedestrian only with playground equipment and waterfalls I like the flexible all season spaces
- I support 5th Street improvements with diagonal parking

Sixth Street Cultural Trail

- The 6th Street cultural Trail is too large of an area to span

Mehus Commons

- I believe the Mehus Commons should be a priority in Phase 1. Not Phase 2
- Mehus Square should be the number one project

5th Street Pedestrian Underpass

- 5th Street Underpass would be car and pedestrians?
- The Fifth Street Underpass would cost too much

Rail Trail

- The Rail Trail is not conducive to mixing BNSF operations, rail traffic, speed and noise with pedestrians

Public Market

- Make BAGA part of the grocer/public market

Bowen Street Ramp

- Back to Parking- I see the long term benefits of the Bowen ramp, but right now, I visualize a parking ramp closer to Main and between the Civic Center and the downtown bars being more able to sell to the public

Other

- Another essential project would be a demographic change to the Patterson Building housing make-up
- Maybe show a plan to 'bookend' the 5th street improvements by adding greenspace and a depot utilizing the existing parking and park-n-ride
- Implement this plan as fast as you can. We need walking space without being run over by bikes and places to sit and pause.
- I like what I hear. I hope private investment steps up to the plate
- Brilliant. Please continue!
- I hope Bismarck will take the current growth environment and capitalize to make it a planned and controlled effort
- Other game-changers should include more green space and public art works
- Another game-changer project should be AN ART MUSEUM- The heritage Center is great but a place for visual works by the Masters- Bismarck
- An additional essential project should be a 'splash pad' and a pedestrian mall
- An additional essential project should be no driving on 5th Street from Main to Broadway- to create a gathering space for families/ playground/kiosk
- An additional game-changer should be a pedestrian zone along 5th street between Main and Broadway

Response Sheet Comments

- I am in full support of this project. I have spent many years in Europe where the downtown/city center is made for the public and life is good for people and businesses thrive. We definitely need this in Bismarck, this city needs to move into the future, and this plan is wonderful. I appreciate the plan of green spaces, bike lanes, and new commercial areas. I'm not worried about parking- as that will get figured out. Most importantly Bismarck needs this to develop further. This renaissance of city centers has been happening all over the US, and its time for Bismarck to catch up
- I would like to see a playground structure for kids in a park downtown or in Mehus Commons or Depot Plaza. A splash pad will also attract a lot of families during the summer months
- Another game-changer should include a parking ramp at 5th and Front sooner rather than later. People will hate losing the parking at Fiesta (but they will get over it)
- I believe the Public Market is a better option if used within the current freight building rather than taking it down
- Great! Exciting plan!
- The top two projects should be the Convention Hotel and the Bowen Ramp

Response Sheet Comments (on-line)

Below is a summary of the comments mentioned in the additional online response sheets collected from the project web site.

Main Avenue

- Main street needs to live up to it's name - it needs to say this is what Bismarck is like
- I had not attended the prior meetings. My initial reaction was "will that work"? but thinking about it and hearing about the traffic impact, and the fact that this will slow things down through the downtown, I like it.
- Have serious doubts as to whether this can handle the traffic flow!
- Bulb-outs at important intersections and well-marked crossings.
- We are already seeing a more vibrant downtown with the addition of outdoor dining. I have seen how successful this has been in many of the major metro areas I have worked in: St. Louis, Denver, Indianapolis, Portland, Cincinnati to name a few. I love it
- Main Ave. (a.k.a. Highway 10) is a major east-west traffic corridor. Bismarckers are privileged to have have free flowing traffic and would be upset to see this change. One might make downtown an enjoyable gathering place much more easily by, for example, creating a pedestrian mall from the four block centered on 4th St and Broadway. At least we'd need to create a new east-west four lane nearby, like Front Ave.
- Will traffic really be able to divert to other streets? I thought an arterial road's function is to move people across town from east to west. There aren't the best routes to redistribute east west traffic in downtown.
- Just be sure that provisions are made for deliveries for the retail and food establishments without backing up traffic in the reduced # of lanes.

Depot Plaza

- Has coordination with the Depot owners occurred? seems like this could be a game changer that would require the relocation of those businesses if they don't have replacement parking adjacent to their property. Has that cost been factored into the plan?

Response Sheet Comments (on-line)

- This is an amazing idea!
- This is the game changer I am, by far, most excited about!
- I am unable to agree with destroying the Wachter Warehouse building for the parking ramp. Is there not a way to incorporate the historical building in to the parking ramp plan?
- This is a fabulous concept! The downtown truly is the heart of a community. I moved here from Fargo in 1980. I was there when West Acres was built, pulling businesses and customers from downtown. I also did some work for the Main Street angling project that finally did in many of the small businesses downtown. It has taken them 30 yrs to recover. We need to act now to prevent that from happening to Bismarck
- I'm all for this project - however I don't know that the privately owned business near this idea would be supportive, nor do I think that the owners of the actual location would sell for \$500K.
- Just picturing finishing up skating with the kids and stopping for a margarita and chips, ha!
- Theoretically sounds wonderful and would have made sense BEFORE things like the Fiesta Villa occupied the old depot building. Unless you think passenger rail is returning to Bismarck and thus the depot will be used as a terminal again, this will drive Fiesta out of the location (and eliminate the nicest outdoor dining locale in Bismarck)!
- I don't know about the practicality of a skating rink unless there is a refrigeration system and a roof/shade structure, but I like the idea of the depot plaza. Just need to keep it clean. I am concerned with the amount of trash and litter around our town. As a citizen, I have been picking up very often and try to pick up something every single day when I walk from parking to my office. We also need the city to step up and do more cleaning.
- Yes IF and ONLY IF the depot remains such and not a strip mall. This was more than a city's depot; it was the regions and needs to remain not a shell but a reality

Fifth Street

- Absolutely...tying the Downtown Distric to the Convention Center and major Retail at Kirkwood would make this Top Regional Destination!
- I like how it moves traffic up and down the area.
- This is extremely important. All the cool cities and towns have this.
- Second priority, after improvements to Main.
- The pedestrian underpass for the rail is never going to happen in Bismarck. Doing this without vehicle underpass for 5th and tracks is unacceptable.
- I like the outside dining today. But when you walk by you are right in the diners space and also have to walk single file
- Somehow there needs to be a winter amenity cover ...

Convention Hotel

- Very important to secure and retain conventions.
- I think this would be a good use of public funds. well leveraged for public/private partnership.
- The most important part in my opinion.
- This one I'm honestly not as sure of, but if it brings more business to downtown, then, that's good.
- I have worked at Alerus Center in Grand Forks connected to Canadd and Rushmore center with connected Holiday Inn, Sioux Falls with Sheraton all critical to successful conventions/conferences rated high by Attendees. I have worked over 30 years on planning group for large conferences in Dallas, Portland, Orlando, Las Vegas, snomass Co, Denver, Kansas City, the key ie to have the host hotel connected to the event center with food service, lounge and lodging for attendees. Close shopping for spouses very important. We have all the ingredients here to make downtown Bismarck a destination for our residents and out of town guests. The stars are aligned!
- Again - this is a much needed upgrade to this area, I am just concerned about how the current private property owners will view the idea of being "progressed over"

Response Sheet Comments (on-line)

- Still doesn't fix the Civic Center issues, in my book.
- The Civic Center is an ABSOLUTELY horrible venue. The seats are uncomfortable, there are trip hazards in the aisles, etc. etc.... My wife and I went to the Boston concert there, and swore that we would never set foot in that dive again! We'll drive to Fargo or beyond before going to something in that venue. If the oil companies need another hotel for their conventions, let them build it!
- Good addition.
- YES if it is solely built and maintained by an entity other than Bismarck City/Citizens

Are there other projects you consider to be Game Changers?

- Seems like you'd have to include the pedestrian underpass as part of depot plaza. It would be hard to accomplish the depot plaza without the ability to walk from the relocated parking.
- Mehus Park
- The walkway under the railroad and the parking ramps.
- I think that the railroad underpass would be a huge game changer, as well. Honestly, I think that most people don't even consider anything South of the railroad to really be part of downtown.
- If the hotel were connected with enclosed walkway to the Kirkwood shopping is imperative to getting year round venues not effected by our harsh winters. I have done many meetings in Minot where hotel/waterpark and shopping are connected and our attendee feedback is always high ratings. The hotel connected to the civic center will bring the conferences and people who spend money, creating consistent customer base for new business ventures
- Commercial development along the southern end of 5th Street approaching Kirkwood Mall.
- The pedestrian trails and bike path connection to the capital, mall and river.
- Public art space!!!! What will we do to make this project aesthetically pleasing and memorable?
- Some yes.

- (1) a combination of skyways and tunnels to connect all of the businesses/blocks/parking ramps. Think Rochester MN! (2) build parking ramps for the downtown employees (including the medical centers) outside of downtown (and let the utilizing businesses pay for them) and use the existing downtown ramps for visitors/customers.
- I like the 6th street cultural trail a lot. I think that could be a huge deal to make downtown an even bigger hub than it is today, especially once the Heritage Center is complete. Would like to see something down with World War Memorial building which would fit great with Belle Mehus Commons.
- Conventions are winter activities in Bismarck. Some of us only go downtown in winter ... you need to entice us ... give us walking clean space AND where does all the snow go that needs to be pushed off the new streets and plazas???

Sixth Street Cultural Trail

- Tying the Capitol Complex & Heritage Center to this core area is a great idea. Nice walk, bike Ride or maybe a trolley that would allow State worker to buzz downtown for a quick lunch without the parking hassle.
- I agree that there should be more bike connectivity in the city however.
- I honestly don't know, and I think it's because I don't know if I understand what it is what it accomplishes exactly, but I don't want to say no, because it is not that I don't like the idea. I just don't fully understand it.
- I like the idea, but unless we turn some of the parking lots into retail or private businesses, it is a waste. Who wants to take a lovely walk from the capital to the McDonald's parking lot? Nothing to do!!
- Not sure...
- Most sensible of all

Mehus Commons

- Lower priority but would be nice to clean that up and connect more with the activity on 5th St

Response Sheet Comments (on-line)

- I love this idea.
- This would be wonderful. People are always gathering before and after shows that the Belle and inevitably, it ends up very crowded and I think this would encourage people to start the night earlier and/or end it later, spending more time (and money) downtown.
- Downtown really needs green space.
- The scale on this rendering is off by a long shot and therefore makes this project look more impactful than it actually would be. You're actually looking at - what? Maybe a 20' -40' depth? I just don't see this location as being ideal for a public square/park.
- Outdoor art!!
- Again, besides the Belle gaining from this I do not see the funds that would be generated by this change?
- Makes much more sense than the fiesta villa location.
- It needs to 'place" and purpose - it would be such a great "off-broadway" Play/music center...especially since it is off Broadway

Chancellor Square

- I think this should start as soon as possible. Why not right now?
- Traffic engineers need to really study this and be sure we do the right thing.
- Either eliminate the one way access or expand it to more than two blocks. was expansion of the one way considered?
- Sooner rather than later.
- One-way traffic REALLY doesn't seem to serve any purpose whatsoever, other than confuse people and make them not want to go downtown.
- Eliminating the the one-ways would generally be good, except that I advocate making the 4-block cross centered on the corner of Broadway and 4th St. a pedestrian mall.
- Traffic pattern change in this area is needed regardless.
- Maybe depends on cost and how they are going to handle traffic volumes
- Not sure it is essential but it is logical

5th Street Pedestrian Underpass

- I'm not sure I understand why this is only a pedestrian underpass. Why not a full underpass including car traffic?
- This would be great but sounds like the cost may be prohibitive ..i hope not as very important to connect to the Convention Center and Kirwood area without the train hassel..especially in the winter.
- Seems like this one is tied to the other game changers. the game changers won't work if they have to cross the railroad tracks at grade
- Should be a game changer
- Absolutely. The railroad is not only a physical, but also a mental barrier between the core of downtown and the South part of downtown. They are very disconnected without this Underpass.
- It should probably be bigger and must have handicap access for wheelchairs.
- Connecting North South side of tracks essential to bring things together with safe walkway for pedestrians
- This is a fantastic idea.
- The train tracks are a major barrier to connecting development south of Main St., but improved on-grade connections at 5th and 3rd could help to resolve this.
- Would hope this would bring the Civic Center traffic to Downtown area after events, more safely without a lot of traffic moving around.
- Also need a fifth street VEHICLE underpass, and a 3rd street one, and a 26th street one and possibly a 12th street one. The vehicle underpasses are more important than the pedestrian. However, hopefully the city can figure out flooding of the underpass before building it (unlike 7th and 9th)
- Yes IF it has place on sides for LOCAL artists AND even behind window area/s for music/chorus during holidays and whenever

Rail Trail

- If not..at least a motorized Trolley car or two to connect to the river, zoo and park areas.
- Good idea and relatively cheep to incorporate

Response Sheet Comments (on-line)

- Again, I'm not sure what this accomplishes, but I'm not entirely opposed to it.
- Better connecting downtown to the river would be an outstanding improvement.
- If it hooks up with the existing trail along the river, this would be an amazing community asset.
- Would like to see more work on this. Needs to be a money maker to be worth it to me.
- Need to hear more about this one. Not against it, but did not fully understand where it would start/stop.
- Yes! Keep Bismarck Bismarck

Public Market

- Great Idea...as soon as possible!
- Great destination and traffic builder and opportunity for small businesses to sell products.
- This would be an empty area for so much of the year. these funds could be better served elsewhere.
- Love!
- I think this is actually a game changer project. I would give much bigger priority to this project than any of the other essential projects. This would fill the void for in the colder months, by providing a cool retail experience that could be appreciated year-round without having to be out in the cold during the winter.
- The railroad warehouse is an important part of Bismarck's character and should not be removed. Bismarck was literally founded by the railroad, and having the building there helps tell that story. I would at least like to have seen evidence that there is enough demand for a year-round farmer's market, especially given our short growing season. If we do need one, then it could surely fit within the standing building.
- This really excites me, a key ingredient to a center location for people to gather and socialize, something that is needed in this disconnected society, human connections are important

- I'm torn on this project because I know that a food co-op is looking for space and would like to locate in the downtown area, but we need to get real. There is a relatively new building on this spot right now that houses everything your presentation suggested be housed in it. Bismarckers are a pragmatic lot and I don't see many people seeing the utility in tearing down something that currently exists in order to rebuild something that is nearly the exact same thing.
- LOVE this!! a facility such as this could be used in various ways year round
- Love this SO much.
- I've been to them in many cities they start good and then devolve into flea market/garage sale/swap meet
- Would depend on what goes in there....
- Public Markets needs to be interspersed throughout the city. AT minimum if one here than one very near Capital Bld area. Not all Bismarckians will or will want to go into dense traffic areas for the simples. I think this would be a money loser

Neighborhood Park

- I said yes, but not sure this is very high on my list.
- OK, but the parkd is not that far away.
- This would be up to the person purchasing the land and redeveloping. i doubt they would buy up land and convert to a park when there's a park across the street.
- I'm really not sure it this is important or not. I certainly hope it would be more than a plot of grass with trees around it. If it actually had something of interest, it would be great. If not, my vote would be no, because it would just be a green empty lot.
- Green space attracts and connects us to living things
- I'm only saying no because I don't see your idea of housing in this area to be realistic.
- I would like to see more amenities in the housing area, such as a continuance of the community garden near the ballpark; a bike path from the housing area to Sertoma Park and to Kirkwood and

Response Sheet Comments (on-line)

Dan's Super Valu. This way, renters can safely bike and walk to buy groceries and shop at Kirkwood. Might even be nice to connect the proposed housing area to Elks Pool/Custer Park with a designated walking/biking trail. What do you think?

- PLEASE!! don't forget the parks
- Lets see the neighborhood start in the right direction first or simultaneously.
- Not sure
- City needs to do a better job of keeping our parks and park parking lots clean. Sad that so many folks can't use the many garbage cans the city has provided. But that's the fact. Need more sanitation engineers.
- Yes BUT rather than I huge --- many small -- it is essential IF want families living downtown BUT in reality they have parks down there -- why not just 'trail' over to "our Eagle"???

Bowen Street Ramp

- Very important part of the equation to provide convenient parking for all the surrounding entities to.
- It would be tied to convention center's needs
- Maybe even a game changer.
- People in North Dakota love to have ample, ample, ample parking.
- Here I'm torn. I'd much rather see the Wachter building restored to its historic appearance than torn down. Are there other parking options?
- Parking will Always be biggest gripe so needed to be addressed
- Yes only if the hotel is a realistic project. No if the hotel doesn't materialize.
- As soon as the mall expansion and hotel projects start
- Only if a fifth street vehicular underpass is built
- YES but top parking isn't smart here ... going to push the snow off or drive it off plus 'roadway' in elements and people in wind ... re-think this remembering other failed top parking areas

Are there other projects you consider to be Essential Projects?

- Downtown housing
- Entertainment district by the mall.
- The Kirkwood Mall and Civic Center Expansion sure would be nice.
- I would like to see something done to the building on the SE corner of Main and 5th. That building is an eye sore and the parking lot is ugly and dirty.
- I am unable to agree with destroying the Wachter Warehouse building for the parking ramp. Is there not a way to incorporate the historical building in to the parking ramp plan? Enhance/improve the performing arts spaces. Get more performing spaces/prep areas/rehearsal space for our vital performing arts groups. The Belle is too small to hold all. Dakota Stage's rented building is falling apart. Northern Plains Dance has a rented space outside the Downtown Area. The Symphony and the Civic Choir have no one consistent space. Invest in culture and the arts.
- Bismarck needs more opportunities to tell it's own history. It was built on the remains of a remarkable earlier civilization (the Mandan nation). It was the scene of remarkable and historically influential political movements like the Non-Partisan League. Preserving places relating to such stories and interpreting them would further strengthen the heart of downtown and enhance tourism.
- This is great start, excellent work!
- Broadway needs to be addressed in this plan. Most of the retail in this area is located along Broadway, and the pedestrian environment is currently below-average.
- Camp Hancock has never been discussed as an important element in the downtown. This is the original town site of Edwinton (Bismarck) and is historically significant for public functions in downtown.
- See my ideas to upgrade the attractiveness of the proposed housing area (#11).
- Love what you have all done. thank you for all the hard work

Response Sheet Comments (on-line)

- We need more parking ramps, period. If the object is to draw people downtown, there needs to be less sprawling parking lots and trashy buildings and new development in these areas. For some reason the interest seems low on the priority list to make that happen?
- You covered it!
- This needs paths - bike and pedestrian to Sertoma Park ... that is as essential as the Capital ...that is family friendly, that has our ZOO!! It is not that far from the "garden/playground and it is huge and already there.

General Comments

- Keep up the good work!
- lots of good ideas. tough to have an action plan if there are not monies allocated to the projects. agree that the action plan needs to get moving right of the bat or else the study, although well put together, if not implemented would be just another document sitting on a shelf somewhere.
- Great ideas, lets keep the momentum going and start putting the Game Changers into action.
- This plan makes me so excited, I don't even have words.
- Love almost all of these ideas. It would be insanely exciting to have a downtown like this in the next five years. I really hope that much, if not, all of this becomes a reality.
- Not looking forward to the bottleneck that going to happen on Main with 3 lanes. 5th street and others like it need more room for delivery trucks.
- I am unable to agree with destroying the Wachter Warehouse building for the parking ramp. Is there not a way to incorporate the historical building in to the parking ramp plan? Enhance/improve the performing arts spaces. Get more performing spaces/prep areas/rehearsal space for our vital performing arts groups. The Belle is too small to hold all. Dakota Stage's rented building is falling apart. Northern Plains Dance has a rented space outside the Downtown Area. The Symphony and the Civic Choir have no one consistent space. Invest in culture and the arts.
- Thanks so much for all your hard work. Most of the ideas you've developed look excellent!
- I really believe the Bismarck as one of the larger cities in ND has the most potential to be the best "community" in the region before it spreads itself out to the North which is fine -- but I almost always go to the "heart" of a city - the downtown that "differentiate" it from the box store, franchises that are everywhere - the same.
- This is a great study and I appreciate all your hard work. I look forward to seeing any of the projects come to fruition.
- I am so pleased with this plan, and am excited to see it implemented. As a downtown resident, I am especially interested in seeing the proposed housing area (SW of downtown in the plan) done well. I'll be keeping my eyes on the housing aspect of the plan, particularly looking for ways to make this south-of-the-tracks area attractive to potential housing developers.
- So grateful that people are open to seeing this as an important investment to the cultural life of our city
- Great to see the thoughts going into this plan. Would like to really see development take off in this area.
- Love the whole plan, perfect time to get started too.
- Good meeting. I think a little more background on each project and scope would have been nice for those of us who did not attend the first two. Or, having now been to the web site, promoting that people watch those videos on the first two meetings before attending the third. But I understand you have to keep the meetings reasonable in length. Well run.
- Frankly, the city needs to pay a few cents per home and mail a postcard saying this study is one, where, online ability. Once more it 'appears' the people are considered too dumb to have an opinion and it 'appears' the commissioners did not learn their lesson. Their arrogance will be the stopper.

Table Reports

Below is a summary of the comments presented by members of each table during Public Workshop #3.

Table 1

- The Public Market is a concern, due to the historic structure there today

Table 2

- The main theme for our group was that there is so much potential for something, we are glad this is a thoughtful plan
- We support the pedestrian friendly elements and like the public square at the Depot

Table 3

- We liked the Game-Changers
- The sixth street cultural trail is important, but maybe underdeveloped
- Is the rail trail pedestrian and bike, as well as, passenger rail?
- If I won the lottery I would pay for all of this!

Table 4

- Rapid City and Market Square is a great example of a public space that includes art
- Our plan should include details that make us unique. You have brought some of this to the table
- Incorporate art on light boxes as a part of the plan
- The Public Market should include art therapy

Table 5

- We need spaces for Mom's to go with kids. This plan is wonderful
- Some of us spent time in Europe, people there are together and out in public spaces
- We should incorporate art into downtown
- We need to rally people behind this plan. People fear change... change can be better for Bismarck

Table 6

- We had a contentious group. Change is difficult and fearful. We are fiscally conservative in Bismarck
- One on one people want to see this place more beautiful and change is inevitable. The west side of the state is not ahead of the curve
- This is a vision for our future growth and long overdue. We argue the details not the intent
- With art you have to be careful and attract to more than specific groups. I like downtown today
- We support two-way traffic
- We could use more greenspace and reuse of existing places like Custer Park for an ice skating rink

General Comments

- I am the newest business relocating here to downtown- Montana Furniture on Main Avenue. North Dakota will be rebuilt from the ground up. Today you are starting from what is working in downtown today and I appreciate that.
- One thing I recognized is that downtown is so compact-- but there is no where to sit or assemble.
- Stay ahead of the game here!
- Are we losing the character of Bismarck?
- I do like Chancellor Square two-way. The 1960's needs to go!
- In your plan for Missoula you have diagonal parking...you have parallel in this plan. I don't like it.
- The big question...How do we pay for this? TIF and Ren. Zone are already taken. Folks don't want to raise taxes.
- In the Neighborhood Park you have limited space for snow collection or rain.
- Is it unique to have a plan where change is too drastically different?
- Is there an area in this pan for a playground or splash pad?

REFINED TRAFFIC OPERATIONS REVIEW SUMMARY



Agenda/3-Lane Discussion

Downtown Bismarck Traffic Operations Review Project Status

- Previous Discussion (June 7th - Traffic Data/Quiet Rail)
- City/NDDOT Comments to Traffic Operations Report
 - Need to review future conditions
 - Truck data/routes
 - Emergency vehicle access

Data Collection

- SRF developed plan (collected by SRF and City)
- All data collected between May 21 and June 6
 - Historical turning movement counts (2 intersections)
 - A.M. and p.m. peak hour intersection turning movement counts (6 intersections)
 - 15-minute peak period pulse counts (5 intersections)
 - 24-hour road tube data to collect vehicle classification (3 locations)

Existing Traffic Volumes

- A.M. versus p.m. peak hour (p.m. peak hour is busiest (i.e. worst-case))
- Truck volumes along Front Avenue - range from 2 percent to 4 percent
- Daily traffic volumes (estimated from turning movement counts/review of 2012 ADT data)
 - **Main Avenue (4-lane undivided): 8,000 vpd (west) to 12,500 vpd (east)**
 - **Front Avenue (3-lane undivided): 6,200 vpd (west) to 3,500 vpd (east)**
- Planning level corridor capacity thresholds by facility type

Facility Type	Daily Capacity Ranges (ADT)	Approaching Capacity (85% of ADT) ⁽¹⁾
Two-lane undivided urban	8,000 - 10,000	8,500
Three-lane undivided urban ⁽²⁾ (two-lane divided with turn lanes)	15,000 - 17,000	14,450
Four-lane undivided urban	18,000 - 22,000	18,700
Five-lane undivided urban ⁽²⁾ (four-lane divided with turn lanes)	28,000 - 32,000	27,200

⁽¹⁾ Values based on the upper limit of daily capacity range

⁽²⁾ The lower value of the daily capacity range is applied to the undivided section; the upper value of the daily capacity range is applied to the divided section.

3-Lane Concept Assumptions

- Assumed 3-lane section would begin/end between 6th Street and 7th Street
- No right-turn lanes were assumed, unless they already exist (3rd Street)
- 3-lane facility on 5th Street
- Optimized signal timing

Existing Operations (4-lane versus 3-lane)

- Analyzed using Synchro/SimTraffic software
- Reviewed intersections operations (i.e. level of service, queuing) and corridor travel times
- Focused on p.m. peak hour operations (i.e. worst-case condition)
- Travel time results summarized in the following table:

Roadway	Eastbound Corridor Travel Time			Westbound Corridor Travel Time		
	4-Lane	3-Lane	Change	4-Lane	3-Lane	Change
Main Avenue	160 sec.	155 sec.	- 5 sec.	145 sec.	165 sec.	+ 20 sec.
Front Avenue	195 sec.	200 sec.	+ 5 sec.	200 sec.	205 sec.	+ 5 sec.

Note: Travel times based on SimTraffic micro-simulation.

- Travel times along Main Avenue and Front Avenue are not significantly impacted due to conversion.
- All intersections operate at an acceptable (LOS C or better) under both the 4-lane and 3-lane scenarios.

Year 2035 Traffic Forecasts

- Reviewed year 2035 daily traffic forecasts
 - Developed as part of the Bismarck-Mandan LRTP
 - Growth equates to approximately one (1) percent annually
- Applied growth rate to the existing peak hour volumes to develop year 2035 peak hour turning movement counts
- Approximate year 2035 daily traffic volumes
 - **Main Avenue: 10,500 vpd (west) to 17,900 vpd (east)**
 - **Front Avenue: 7,750 vpd (west) to 3,900 vpd (east)**
- The Main Avenue daily volumes are over the planning-level thresholds for a three-lane facility (i.e. two-lane facility with turn lanes)

Year 2035 Operations (4-lane versus 3-lane)

- Focused on p.m. peak hour operations (i.e. worst-case condition)
- Travel time results summarized in the following table:

Roadway	Eastbound Corridor Travel Time			Westbound Corridor Travel Time		
	4-Lane	3-Lane	Change	4-Lane	3-Lane	Change
Main Avenue	175sec.	185 sec.	+ 10 sec.	175 sec.	> 5 min.	+ 2 min.
Front Avenue	250 sec.	275 sec.	+ 25 sec.	200 sec.	180 sec.	- 20 sec.

Note: Travel times based on SimTraffic micro-simulation.

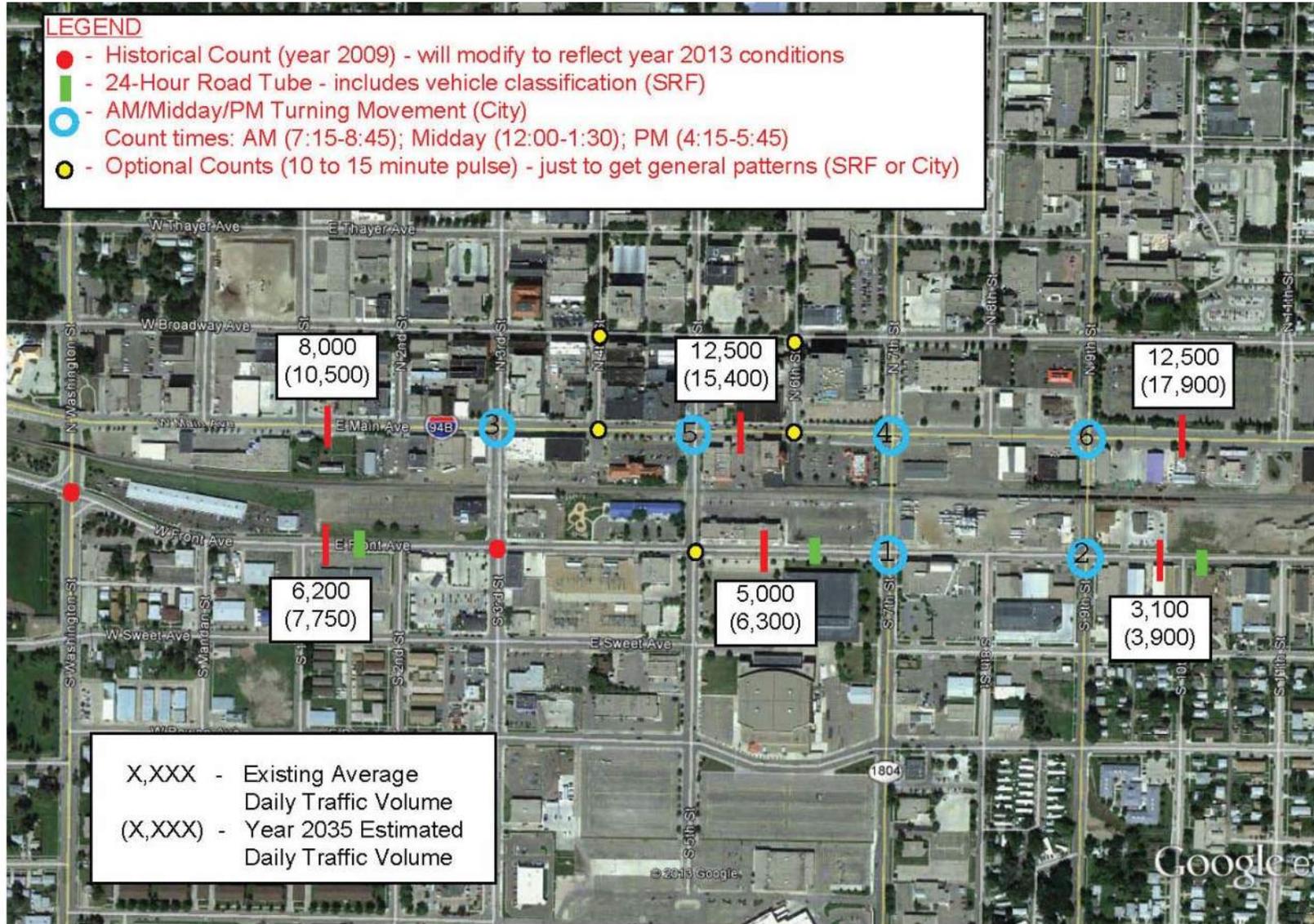
- Eastbound travel times (along Main Avenue and Front Avenue) and westbound travel times (along Front Avenue) are not significantly impacted due to conversion.
- Westbound travel times along Main Avenue significantly increase due to conversion and are considered unacceptable.
- All intersections operate at an acceptable (LOS C or better) under the 4-lane scenario.
- Under the 3-lane scenario, the westbound through volume is too high and can't be accommodated through a single lane (e.g. LOS E operations at 7th Street).

Considerations to make a 3-lane work

- Achieve volume diversion of approximately 1,500 to 2,000 vpd (a reduction in westbound volume is key)
 - Must leave Front Avenue open
 - Allow two-way traffic on Broadway Avenue
 - Extend Broadway Avenue to Main Avenue
- Alternative improvements to consider:
 - Access modification at 4th Street (right-in/right-out or three-quarter) and 6th Street (three quarter), including removing the traffic signals
 - Addition of right-turn lanes along Main Avenue
 - Modification of 3rd Street to have two through lanes in the northbound/southbound direction (i.e. shared left/through and shared through/right lanes)

LEGEND

- - Historical Count (year 2009) - will modify to reflect year 2013 conditions
- - 24-Hour Road Tube - includes vehicle classification (SRF)
- - AM/Midday/PM Turning Movement (City)
Count times: AM (7:15-8:45); Midday (12:00-1:30); PM (4:15-5:45)
- - Optional Counts (10 to 15 minute pulse) - just to get general patterns (SRF or City)



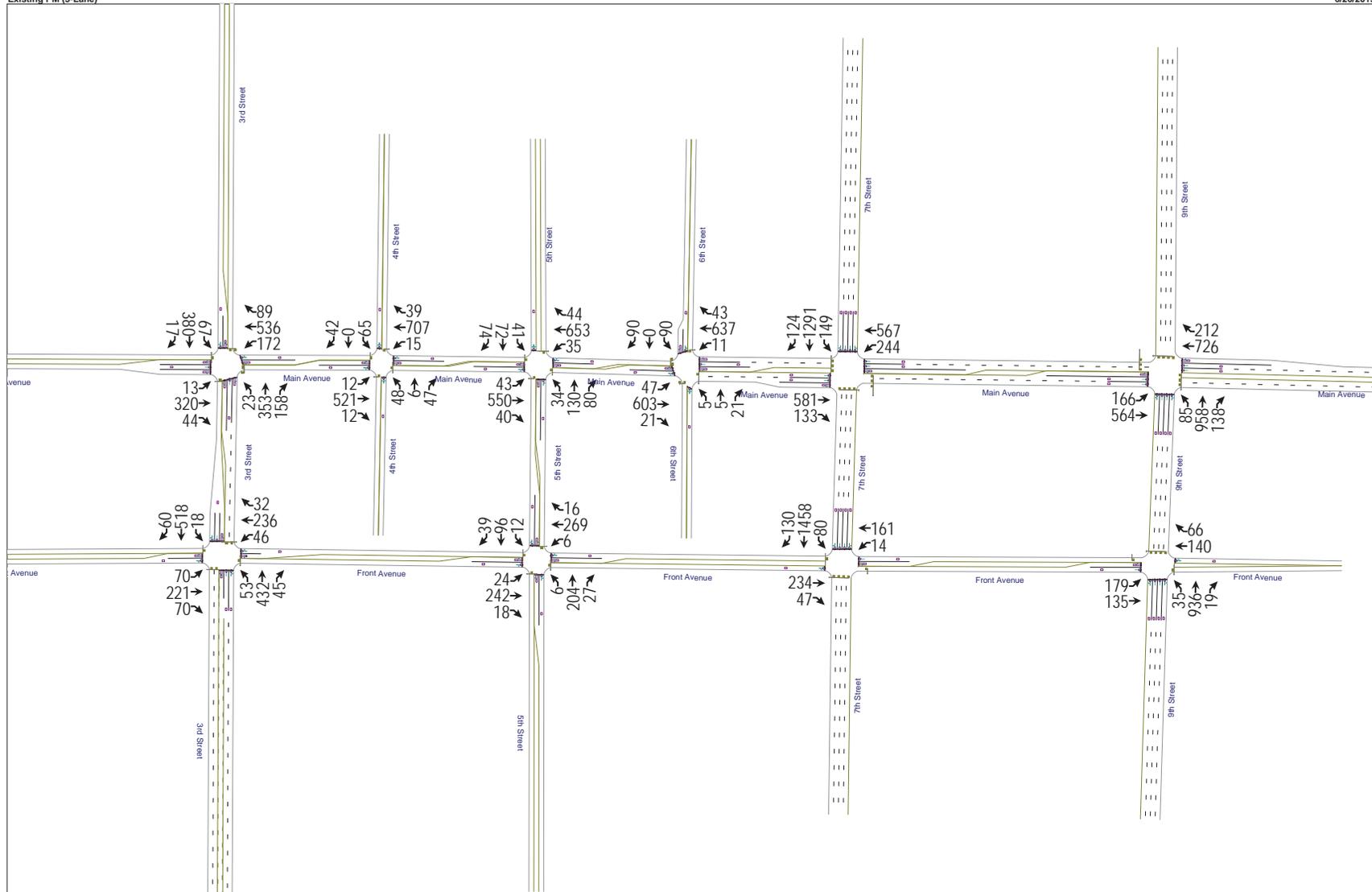
X,XXX - Existing Average Daily Traffic Volume
(X,XXX) - Year 2035 Estimated Daily Traffic Volume

Google earth

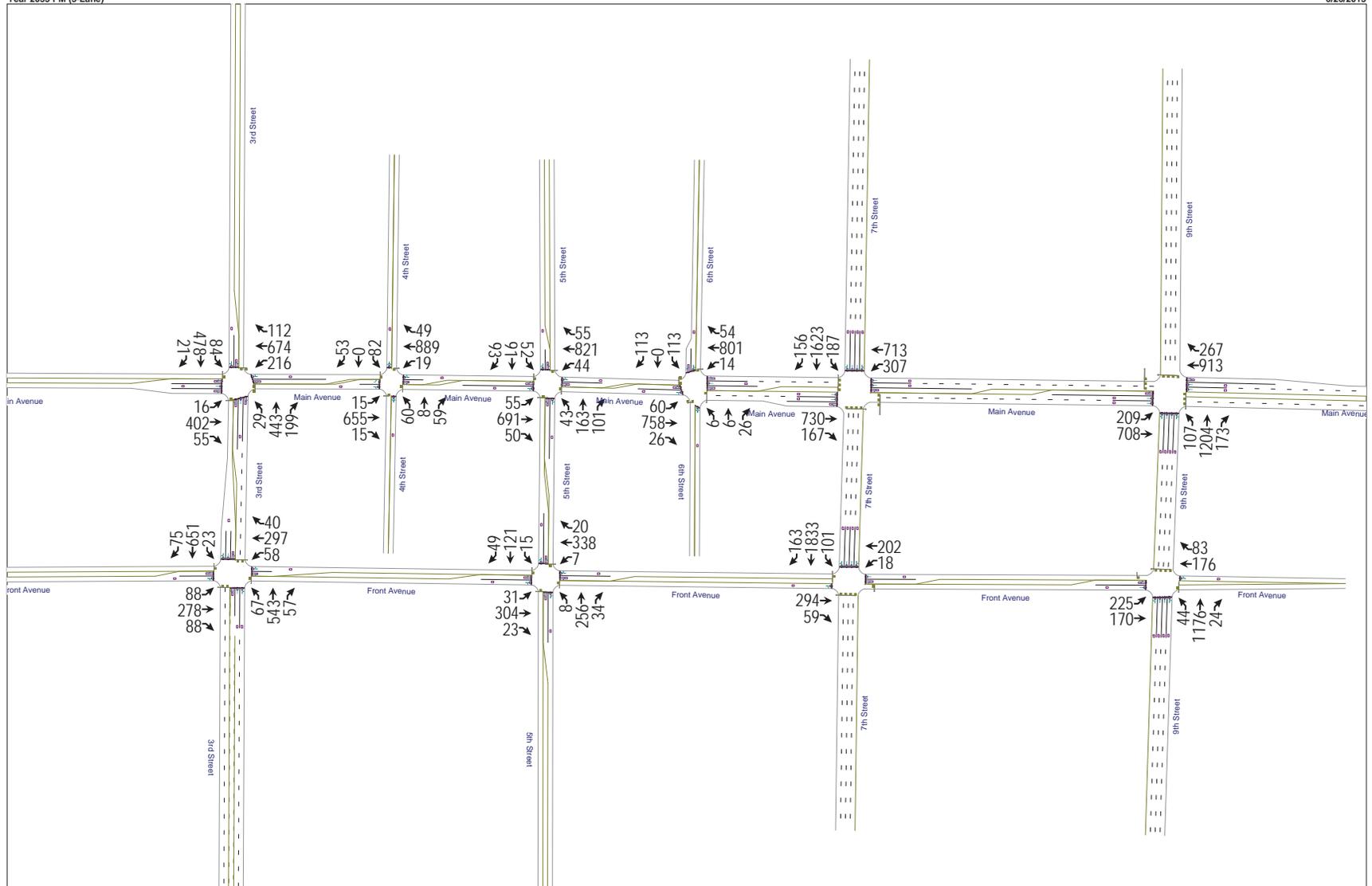
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REFINED TRAFFIC OPERATIONS MEMO

Memorandum

SRF No. 8161

To: Jason Graf, Associate Principal
Crandall Arambula PC

From: Matthew Pacyna, PE, Associate

Date: July 29, 2013

Subject: Downtown Bismarck Subarea Study - Traffic Operations Review Addendum

Introduction

As requested, SRF completed an addendum in response to comments received from the City and North Dakota Department of Transportation (NDDOT) regarding the Downtown Bismarck Subarea Study - Traffic Operations Review dated May, 1, 2013 (see Appendix). The initial traffic operations review provided a planning-level analysis of various Downtown Bismarck redevelopment concepts and their associated impacts. The main concern raised as part of the review was how the Main Avenue Road Diet (i.e. four-lane to three-lane conversion) would operate, particularly from both a short- and long-term level of service and queuing perspective. Therefore, this addendum focuses on the detailed operations analysis, but also addresses potential heavy commercial/emergency vehicle access and future transit center locations. The following information summarizes the assumptions and analysis completed.

Main Avenue Road Diet

The Main Avenue Road Diet concept was assumed to reconfigure Main Avenue from west of 3rd Street to a location between 6th Street and 7th Street. This reconfiguration would consist of converting the existing four-lane undivided roadway to a three-lane (i.e. two-lane with a center left turn lane) roadway. It should be noted that for purposes of this additional analysis, no right-turn lanes were assumed unless they already exist. Furthermore, 5th Street was also assumed to be reconfigured as a three-lane facility as part of this analysis.

Existing Conditions

Data Collection

To determine how the potential Main Avenue Road Diet would impact existing operations (i.e. short-term conditions), traffic data was collected within the study area to establish a baseline level of operation. This data included vehicular intersection turning movement and pedestrian counts, which were collected by a combination of SRF and City staff during the a.m. and p.m. peak periods between May 21, 2013 and June 6, 2013 at the following intersections:

- Main Avenue/3rd Street
- Main Avenue/9th Street
- Main Avenue/5th Street
- Front Avenue/7th Street
- Main Avenue/7th Street
- Front Avenue/9th Street

These turning movement counts were supplemented with manual 15-minute pulse counts and historical (i.e. year 2009) data as noted in the following locations. This data was modified to reflect year 2013 a.m. and p.m. peak hour conditions, accordingly. This data collection approach was selected to minimize data collection efforts while acquiring sufficient data to accurately determine operational impacts associated with the potential conversion.

Average daily traffic (ADT) volumes, including vehicle classification data, was collected at the following locations:

- Front Avenue between 1st Street and 2nd Street
- Front Avenue between 5th Street and 7th Street
- Front Avenue between 10th Street and 11th Street

Furthermore, historical and year 2012 annual average daily traffic (AADT) volumes, provided by the NDDOT, were also collected. It should be noted that the existing heavy commercial vehicle percentages along Front Avenue ranged from approximately two to four percent. The geometrics, traffic controls, and volumes within the study area for the existing four-lane and proposed three-lane configurations are shown in Figures 1 and 2, respectively.

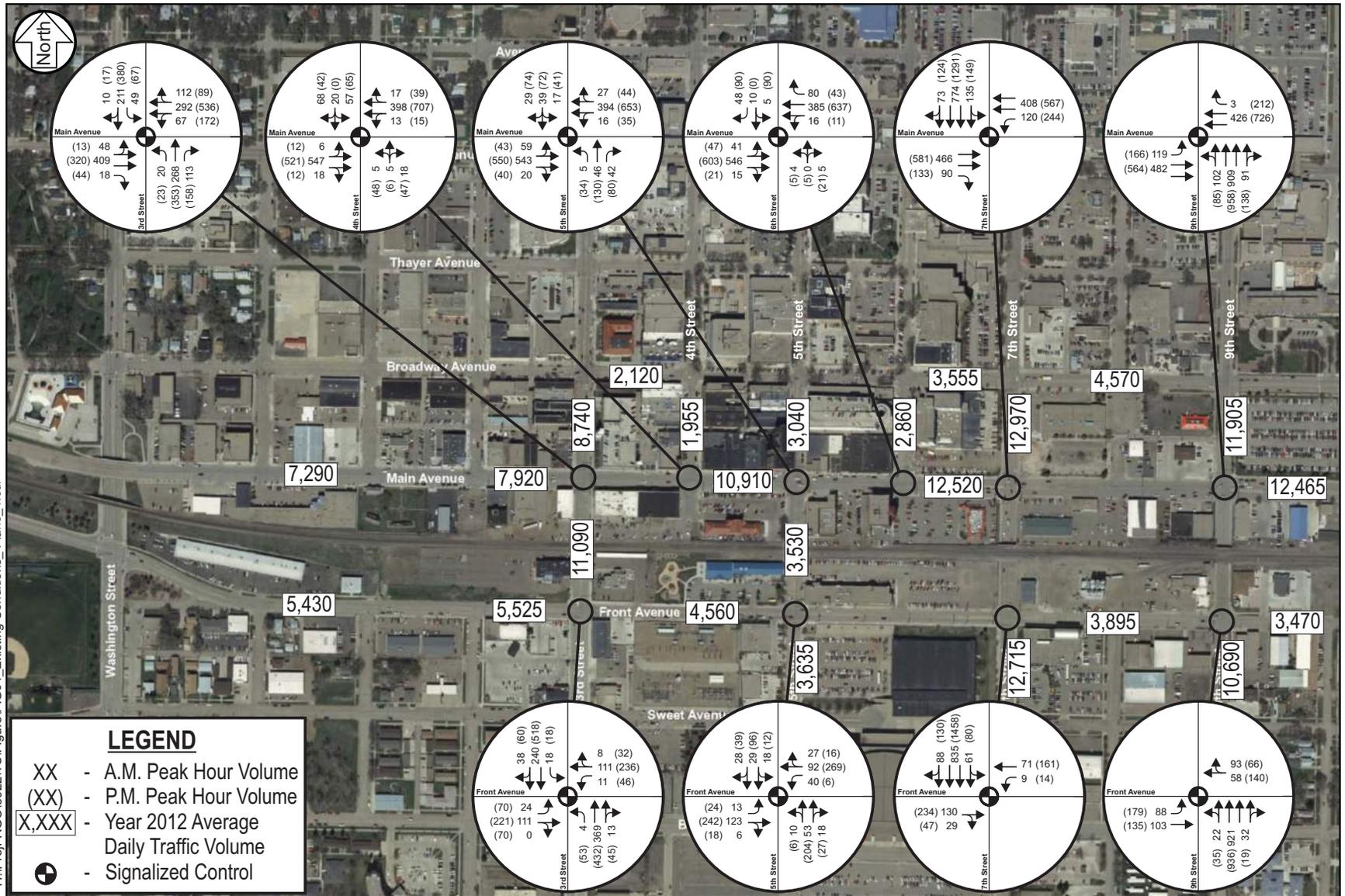
Intersection Capacity Analysis

To determine the short-term operational difference between the existing four-lane and proposed three-lane configurations, a detailed operations analysis was conducted. All intersections were analyzed using the Synchro/SimTraffic software. Capacity analysis results identify a Level of Service (LOS) which indicates how well an intersection is operating. Intersections are given a ranking from LOS A through LOS F. The LOS results are based on average delay per vehicle, which corresponds to the delay threshold values shown in Table 1. LOS A indicates the best traffic operation and LOS F indicates an intersection where demand exceeds capacity. LOS A through C is generally considered acceptable by drivers in the bismarck area. It should be noted that optimized timing was assumed for each scenario.

Table 1
Level of Service Criteria for Signalized and Unsignalized Intersections

LOS Designation	Signalized Intersection Average Delay/Vehicle (seconds)	Unsignalized Intersection Average Delay/Vehicle (seconds)
A	< 10	< 10
B	10-20	10-15
C	20-35	15-25
D	35-55	25-35
E	55-80	35-50
F	80 <	50 <

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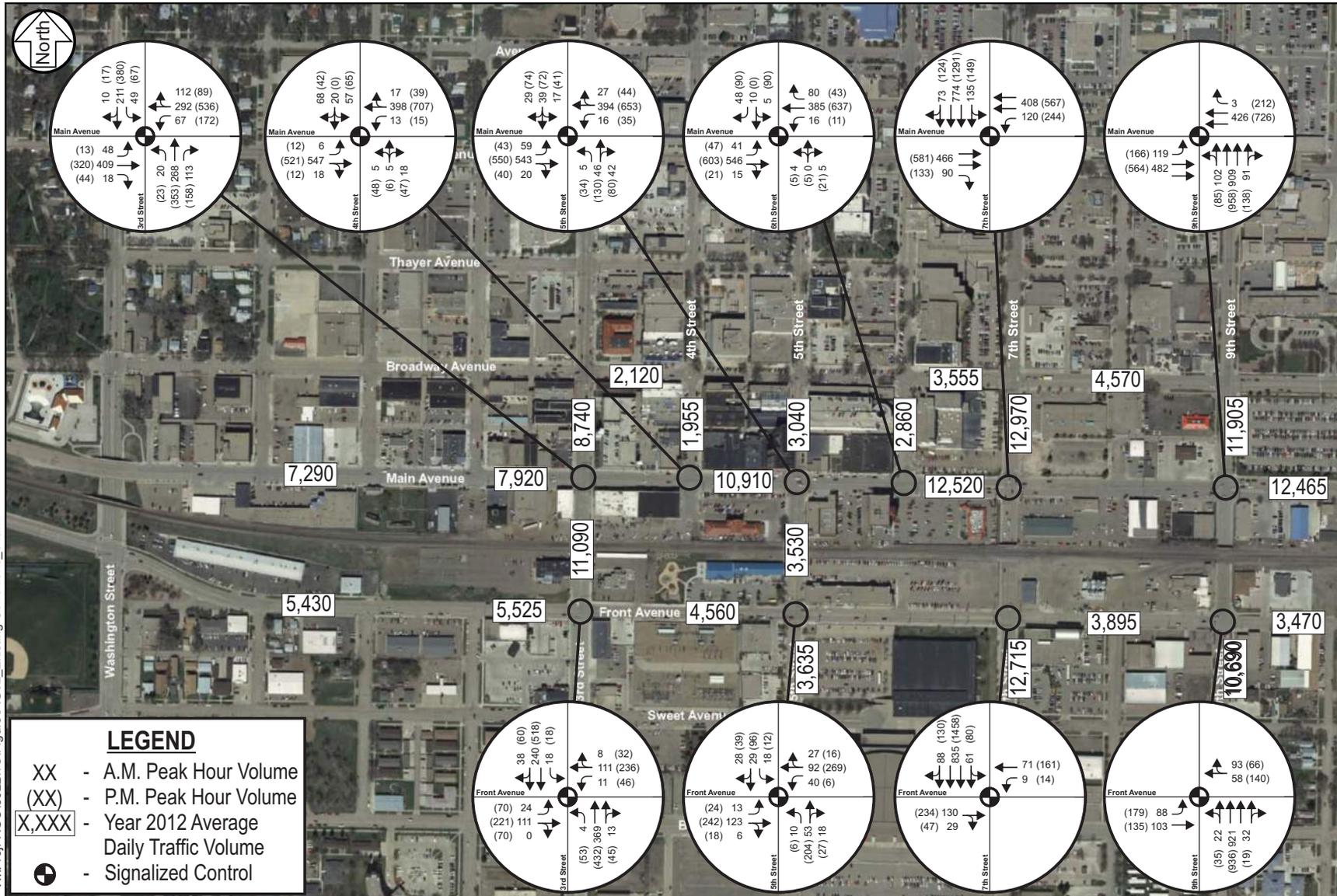


Existing Conditions (Four-Lane Configuration)

Downtown Bismarck Subarea Study
Bismarck, North Dakota

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Figure 1



Existing Conditions (Three-Lane Configuration)

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Bismarck, North Dakota

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Figure 2

Results of the existing intersection capacity analysis comparison between the current four-lane and proposed three-lane configurations shown in Table 2 indicate that each roadway layout would provide acceptable intersection operations from a level of service perspective. In general, the operational differences between the two configurations are relatively minor during the existing a.m. and p.m. peak hours. However, it should be noted that minor westbound queues are expected to occasionally (i.e. approximately five percent of the p.m. peak hour) impact adjacent intersections under the three-lane configuration during the p.m. peak hour. These queues are primarily the result of the close intersection spacing and could potentially be mitigated through a more detailed signal coordination analysis. Furthermore, the three-lane conversion and other area roadway improvements (i.e. Chancellor Square modification)

Table 2
Existing Peak Hour Capacity Analysis Comparison

--	--	--

t	B (10 sec.)	B (11 sec.)	B (18 sec.)	B (15 sec.)
Front Avenue/5th Street	B (18 sec.)	B (19 sec.)	C (22 sec.)	C (20 sec.)
Front Avenue/7th Street	A (6 sec.)	A (7 sec.)	A (9 sec.)	B (10 sec.)
Front Avenue/9th Street	A (9 sec.)	B (10 sec.)	B (15 sec.)	B (18 sec.)

* Represents the overall intersection delay.

Travel Time Analysis

In addition to the peak hour operations analysis, corridor travel times along Main Avenue and Front Avenue were also reviewed. SRF evaluated the travel time using Synchro/SimTraffic under both the existing four-lane and proposed three-lane configurations along Main Avenue. Only the p.m. peak hour was reviewed since it represents the worst-case condition. The travel time impacts during the a.m. peak hour are expected to be negligible. The comparison shown in Table 3 indicates that Main Avenue and Front Avenue are not expected to be significantly impacted by the proposed three-lane configuration.

Table 3
Existing Travel Time Comparison

Roadway	Eastbound Corridor Travel Time			Westbound Corridor Travel Time		
	Four-Lane	Three-Lane	Change	Four-Lane	Three-Lane	Change
Main Avenue	160 sec.	155sec.	- 5 sec.	145 sec.	165 sec.	+ 20 sec.
Front Avenue	195 sec.	200 sec.	+ 5 sec.	200 sec.	205 sec.	+ 5 sec.

Note: Travel times based on SimTraffic micro-simulation.

It should be noted that that emergency vehicle access/response times were mentioned as a particular concern regarding the potential conversion of Main Avenue. Based on the existing travel time analysis, the response times for vehicles currently using Main Avenue are not expected to be significantly impacted. Although the travel times are shown to increase slightly during the p.m. peak hour, this analysis does not account for Emergency Vehicle Preemption (EVP) systems currently used, which can significantly improve travel time.

Traffic Forecasts

To determine the long-term impacts of the proposed three-lane configuration, traffic forecasts were developed for year 2035 conditions. Year 2035 ADT volumes were developed as part of the Bismarck-Mandan Long Range Transportation Plan (LRTP), which indicates an average annual growth rate of approximate one-percent. It should be noted that the City discussed the potential for an annual growth rate of approximately four percent within the Bismarck-Mandan area. However, given the current density within the study area and expected growth towards the outer City limits, a four percent growth rate appears high for the downtown area. Therefore, the one-percent annual growth rate was applied to the existing volumes to develop year 2035 peak hour forecasts. The year 2035 geometrics, traffic controls, and volumes within the study area for the existing four-lane and proposed three-lane configurations are shown in Figures 3 and 4, respectively.

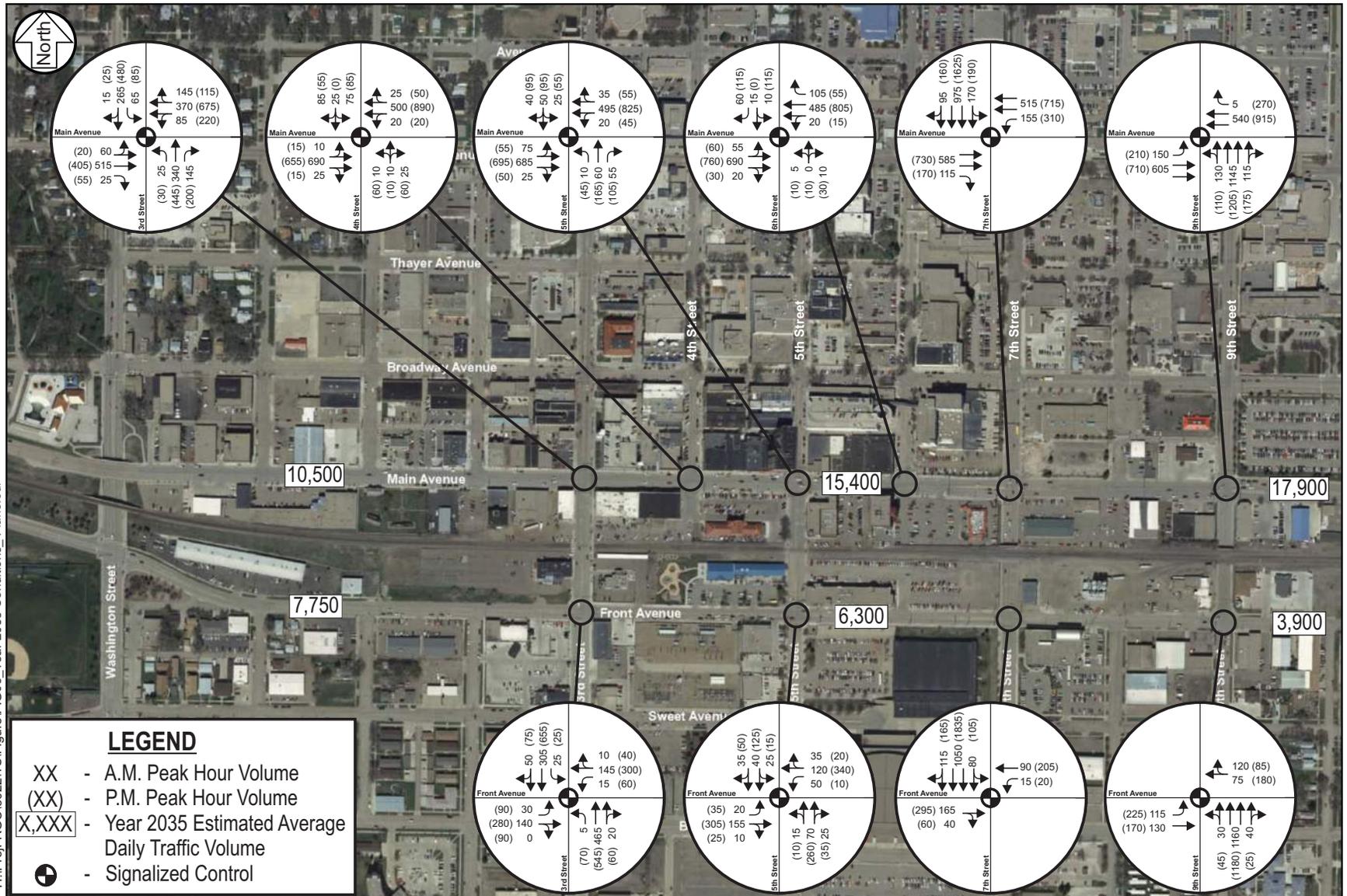
Review of the year 2035 daily traffic forecasts indicate that volumes along Main Avenue are expected to range from 10,500 vpd to 17,900 vpd. Along Front Avenue volume are expected to range from 3,900 vpd to 7,750 vpd. Based on planning level capacity thresholds discussed as part of the previous analysis, Main Avenue is expected to be over capacity if reconfigured as a three-lane facility. However, a more detailed analysis was completed to confirm the capacity results and identify potential mitigation. It should be noted that this additional analysis does not account for any diversion, which would be likely to occur.

Year 2035 Conditions

Therefore, the existing four-lane and the proposed three-lane configurations along Main Avenue were analyzed under year 2035 conditions. Once again, a detailed traffic operations analysis was conducted using the Synchro/SimTraffic software.

Results of the year 2035 operations analysis shown in Table 4 indicate that all study intersections are expected to operate at an acceptable overall LOS C or better during the a.m. and p.m. peak hours, except along Main Avenue at 7th Street and 9th Street. These intersections are expected to operate at LOS E during the p.m. peak hour under the three-lane configuration. The poor operations are a result of westbound vehicles queuing as they approach the three-lane segment, which impacts the adjacent intersections at 7th Street and 9th Street. It should be noted that the long-term operations indicate that diversion away from Main Avenue would likely occur, but is not considered as part of this analysis. Preliminary review of adjacent streets (i.e. Front Avenue) indicates there is sufficient capacity to account for any diversion. However, to achieve additional diversion from Main Avenue, other improvements which are discussed later within this document, should be considered.

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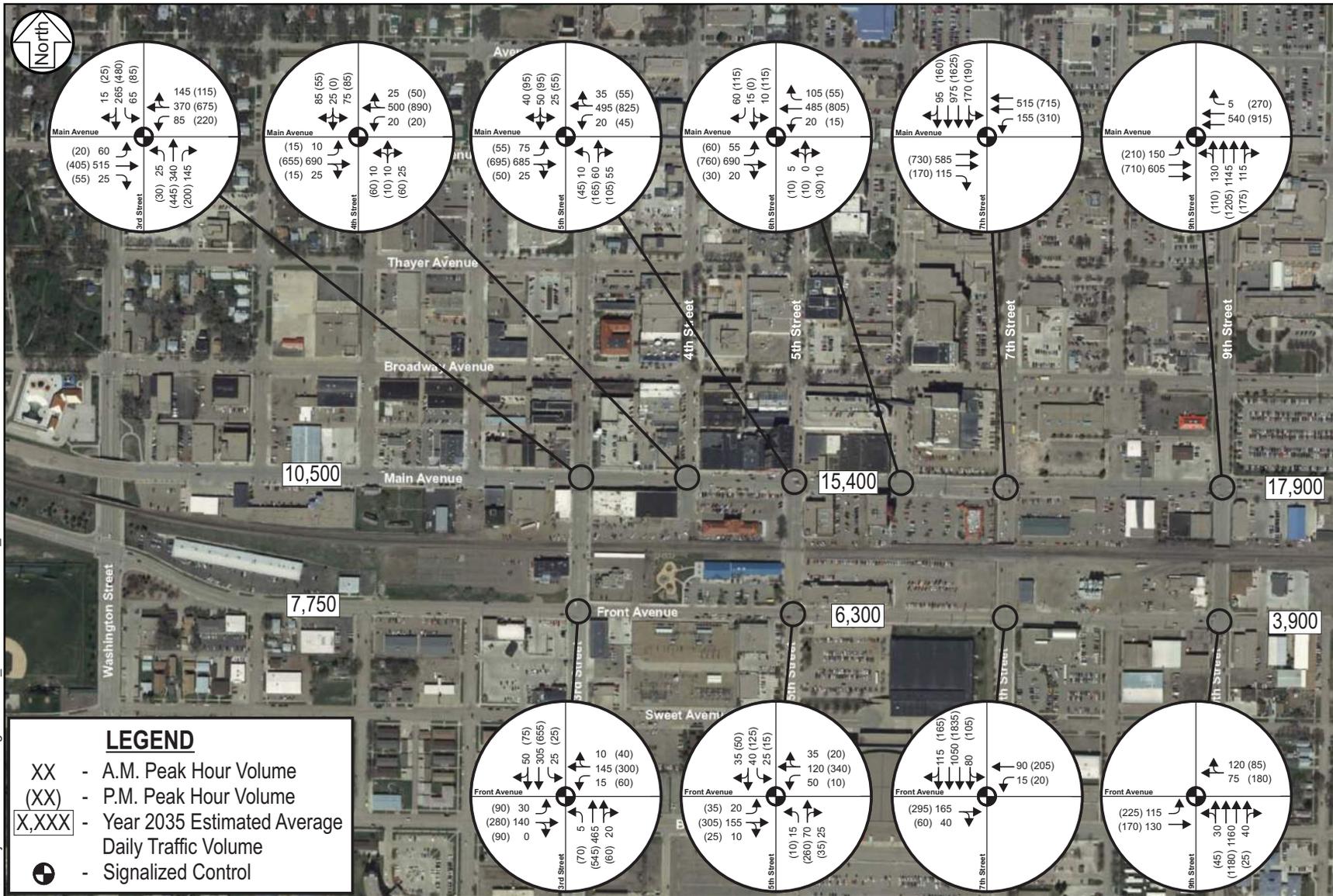


Year 2035 Conditions (Four-Lane Configuration)

Downtown Bismarck Subarea Study
Bismarck, North Dakota

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Figure 3



Year 2035 Conditions (Three-Lane Configuration)

Downtown Bismarck Subarea Study
Bismarck, North Dakota

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Figure 4

Table 4
Year 2035 Peak Hour Capacity Analysis Comparison

Intersection	LOS (Delay*)			
	A.M. Peak		A.M. Peak	
	Four-Lane	Four-Lane	Four-Lane	Four-Lane
Main Avenue/3rd Street	B (18 sec.)	C (20 sec.)	C (32 sec.)	C (30 sec.)
Main Avenue/4th Street	A (8 sec.)	B (11 sec.)	B (10 sec.)	C (22 sec.)
Main Avenue/5th Street	A (9 sec.)	B (10 sec.)	B (17 sec.)	C (27 sec.)
Main Avenue/6th Street	A (7 sec.)	A (8 sec.)	B (11 sec.)	C (27 sec.)
Main Avenue/7th Street	C (25 sec.)	C (22 sec.)	C (30 sec.)	E (56 sec.)
Main Avenue/9th Street	C (21 sec.)	C (21 sec.)	C (23 sec.)	E (64 sec.)
Front Avenue/3rd Street	B (12 sec.)	B (11 sec.)	C (20 sec.)	B (18 sec.)
Front Avenue/5th Street	B (13 sec.)	C (22 sec.)	B (16 sec.)	C (21 sec.)
Front Avenue/7th Street	A (7 sec.)	A (9 sec.)	B (16 sec.)	B (17 sec.)
Front Avenue/9th Street	B (11 sec.)	B (11 sec.)	C (29 sec.)	C (31 sec.)

* Represents the overall intersection delay.

Travel Time Analysis

In addition to the peak hour operations analysis, corridor travel times along Main Avenue and Front Avenue were also reviewed. SRF once again evaluated the travel time using Synchro/ SimTraffic under both the existing four-lane and proposed three-lane configurations along Main Avenue. Only the p.m. peak hour was reviewed since it represents the worst-case condition. Travel time impacts during the a.m. peak hour are expected to be negligible. The travel time comparison shown in Table 5 indicates that westbound travel time along Main Avenue is expected to be significantly impacted during the p.m. peak hour due to the proposed three-lane configuration. This corresponds with the intersection capacity analysis previously identified. It should be noted that the travel time along eastbound Main Avenue and both directions along Front Avenue are not expected to be significantly impacted by the proposed conversion.

Table 5
Year 2035 Travel Time Comparison

Roadway	Eastbound Corridor Travel Time			Westbound Corridor Travel Time		
	Four-Lane	Three-Lane	Change	Four-Lane	Three-Lane	Change
Main Avenue	175sec.	185 sec.	+ 10 sec.	175 sec.	> 5 min.	+ 2 min.
Front Avenue	250 sec.	275 sec.	+ 25 sec.	200 sec.	180 sec.	- 20 sec.

Note: Travel times based on SimTraffic micro-simulation.

Three Lane Mitigation Options

Based on the poor operation and significant impacts to the westbound travel time identified during the year 2035 p.m. peak hour conditions with the proposed three-lane configuration, motorists are expected to divert from Main Avenue to alternative routes. To achieve acceptable operations along Main Avenue, approximately 1,500 vpd to 2,000 vpd would need to divert to alternative routes, particularly in the westbound direction. To ensure that the adjacent corridors can adequately support the potential diversion from Main Avenue, the following should be considered:

- Front Avenue must remain open to two-way traffic
- Allow two-way traffic on Broadway Avenue (i.e. Chancellor Square conversion)
- Extend/connect Broadway Avenue to Main Avenue

Furthermore, the following additional improvements should also be considered:

- Access modification at 4th Street (right-in/right-out or three-quarter) and 6th Street (threequarter), including removing the traffic signals
 - This would likely improve vehicular operations, but would be less pedestrian friendly
- Addition of right-turn lanes along Main Avenue, where appropriate
- Modification of 3rd Street to have two through lanes in the northbound/southbound direction (i.e. shared left/through and shared through/right lanes)

Transit

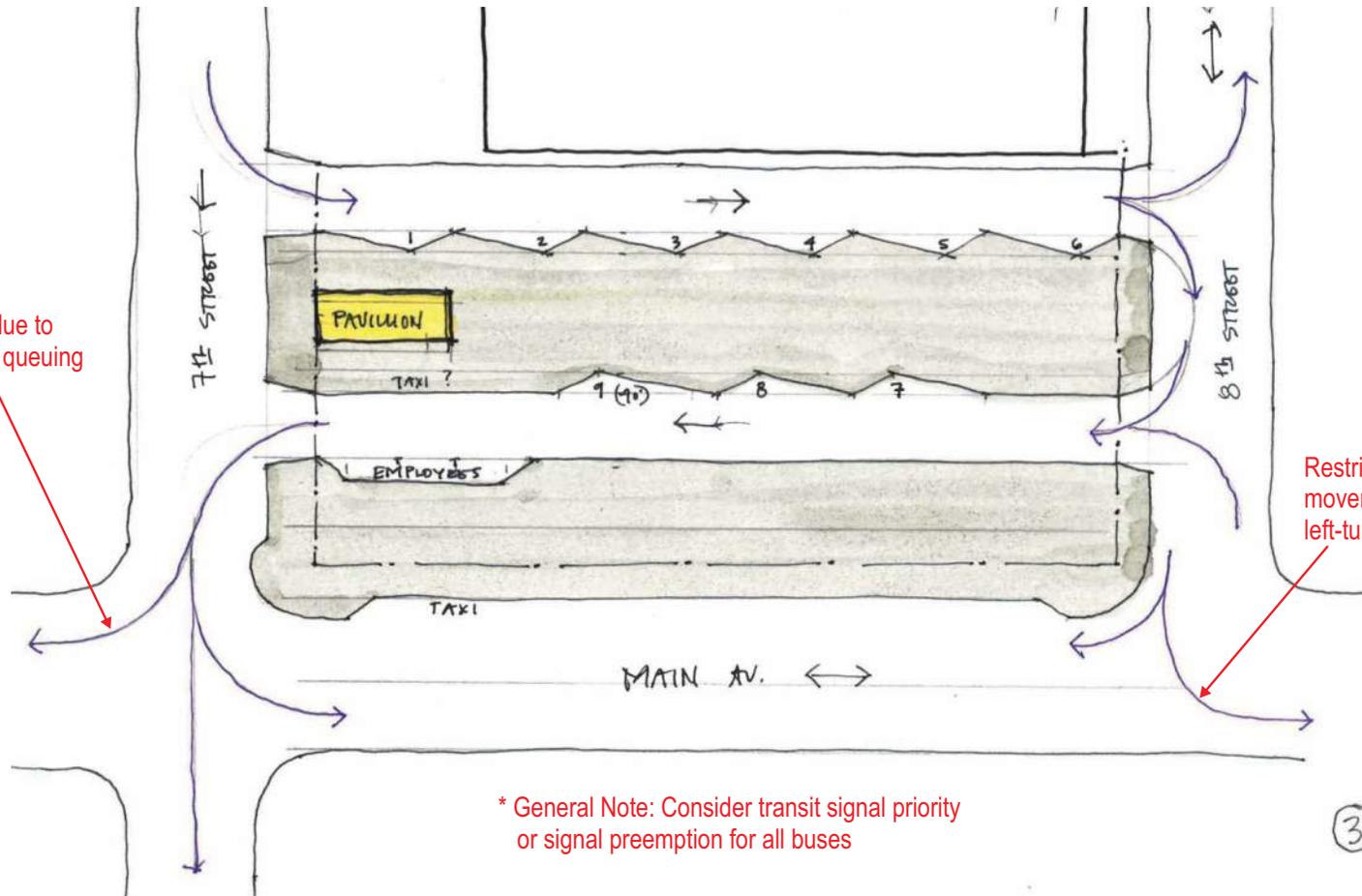
Regardless of the four-lane/three-lane configuration discussion along Main Avenue, there is the potential of a new transit center to be located downtown. Based on discussions with planning staff, the preferred location of the future transit center is Option 3 as identified in the Bismarck Downtown Transit Center: Review Program Concept, Location and Site Alternatives – Draft completed by Nelson\Nygaard Consulting Associates Inc (see Figure 5). Therefore, as part of this addendum, SRF has completed a planning-level review of the preferred transit center location to identify any potential traffic operation/circulation issues. The following conclusions and recommendations are offered for consideration:

- The transit center is located along Main Avenue between 7th Street and 8th Street and assumes that 8th Street would be reopened to the north, allowing access to/from Broadway Avenue.
- Buses will be allowed to enter/exit the transit center along both 7th Street and 8th Street.
- Based on the traffic operations analysis, southbound queues from the Main Avenue/7th Street intersection are expected to frequently queue beyond the transit center access.
 - These queues will make it difficult for buses exiting onto 7th Street, particularly those destined westbound along Main Avenue, which would have to cross multiple lanes of traffic.
 - To mitigate this situation, buses destined west along Main Avenue should be located in stall one through six and be required to access Main Avenue via 8th Street.
- Based on the traffic operations analysis, making a southbound left-turn movement from the Main Avenue/8th Street intersection is expected to be difficult assuming the current sidestreet stop control.
 - To prevent buses from having to make this difficult maneuver, buses destined east along Main Avenue should exit via 7th Street and complete a southbound left-turn movement at the signalized Main Avenue/7th Street intersection.
- To minimize impacts to exiting buses, transit signal priority (TSP) or signal pre-emption could be implemented to dissipate any queuing prior to exiting.



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Restrict this movement due to southbound queuing



Restrict this movement due to left-turn difficulty

* General Note: Consider transit signal priority or signal preemption for all buses



Preferred Downtown Transit Location (Option 3)

Downtown Bismarck Subarea Study
Bismarck, North Dakota

0138022
July 2013

Figure 5

GAME CHANGER INVESTMENT SUMMARY



Game Changers and New Development

Game-Changers

Public Investment Summary

Project	Total
① Main Avenue (500K/Blk)	\$ 3,500,000*
② Depot Plaza (40,000 SF @ \$170/SF)	\$ 8,500,000*
③ 5 th Street Improvements (A to Mall @\$1 M/Blk)	\$ 10,500,000*
④ Civic Center Convention Hotel	\$ TBD

Public Investment: \$ 22.5 MIL

* Estimates are meant to be strictly order of magnitude construction cost estimates plus a 20% Contingency.
Depot Plaza costs include property acquisition of \$.5 MIL

Draft 08-08-13
Crandall Arambula PC

New Development

Potential Private Investment Summary

Use	Units/SF/SP	Total
Housing (Unit)*	430 DU	\$108,450,000
Hotel (Rooms)*	250 DU	\$ 42,500,000
Office (SF)*	363,000 SF	\$ 94,650,000
Retail (SF)*	289,000 SF	\$ 72,300,000

Potential New Private Investment: \$ 317.9 MIL

* Estimates include costs for on-site parking

Draft 08-08-13
Crandall Arambula PC

New Development

Potential Private Investment Summary

Block	Use	Cost	Units/SF/SP	Investment
a.	Office- 6 Floors	\$200/SF	120,000 SF	\$ 24,000,000
	Parking- 2 levels	\$28,000/SP	250 SP	\$ 7,000,000
b.	Office- 6 Floors	\$200/SF	66,000 SF	\$ 13,200,000
	Parking- 1 level	\$38,000/SP	75 SP	\$ 2,000,000
c.	Office- 5 Floors	\$200-\$250/SF	26,000 SF	\$ 5,600,000
	Retail- Gr Flr	\$250/SF	7,000 SF	\$ 1,800,000
	Parking- 1 Level	\$38,000/SP	96 SP	\$ 3,800,000
d.	Retail - Gr Flr	\$250/SF	6,000 SF	\$ 1,500,000
	Housing- 4 Floors	\$200,000 DU	40 DU	\$ 8,000,000
	Parking- 1 level	\$38,000/SP	44 SP	\$ 1,800,000
e.	Retail - Gr Flr	\$250/SF	8,500 SF	\$ 2,000,000
	Housing- 4 Floors	\$200,000 DU	48 DU	\$ 9,600,000
	Parking- 1 level	\$38,000/SP	51 SP	\$ 2,000,000
f.	Retail - Gr Flr	\$250/SF	16,400 SF	\$ 4,000,000
	Housing- 4 Floors	\$200,000 DU	20 DU	\$ 4,000,000
	Parking- 1 level	\$38,000/SP	20 SP	\$ 750,000
g.	Retail - Gr Flr	\$250/SF	13,500 SF	\$ 3,400,000
	Office- 4 Floors	\$200/SF	5,000 SF	\$ 1,000,000
	Parking- 1 level	\$8,000/SP	15 SP	\$ 125,000
h.	Retail - Gr Flr	\$250/SF	3,000 SF	\$ 750,000
i.	Retail - Gr Flr	\$250/SF	10,000 SF	\$ 2,500,000

Draft 08-08-13
Crandall Arambula PC

New Development

Potential Private Investment Summary

Block	Use		Cost	Units/SF/SP	Investment
j.	Retail -	Gr Flr	\$250/SF	15,000 SF	\$ 3,750,000
	Office-	4 Floors	\$200/SF	48,000 SF	\$ 9,500,000
	Parking-	1 level	\$38,000/SP	85 SP	\$ 3,250,000
k.	Retail -	Gr Flr	\$250/SF	5,400 SF	\$ 1,350,000
l.	Retail -	Gr Flr	\$250/SF	5,000 SF	\$ 1,375,000
	Housing-	3 Floors	\$200,000 DU	46 DU	\$ 9,200,000
	Parking-	1 level	\$28,000/SP	50 SP	\$ 1,400,000
m.	Retail -	Gr Flr	\$250/SF	4,600 SF	\$ 1,050,000
	Housing-	3 Floors	\$200,000/DU	18 DU	\$ 3,600,000
	Parking-	1 level	\$28,000/SP	20 SP	\$ 500,000
n.	Retail -	Gr Flr	\$250/SF	7,800 SF	\$ 2,075,000
o.	Retail -	Gr Flr	\$250/SF	12,000 SF	\$ 3,000,000
	Housing-	4 Floors	\$200,000/DU	28 DU	\$ 5,600,000
	Parking-	2 levels	\$28,000/SP	250 SP	\$ 7,000,000
p.	Retail -	Gr Flr	\$250/SF	10,000 SF	\$ 2,500,000
	Office-	3 Floors	\$200/SF	18,000 SF	\$ 3,500,000
	Parking-	1 level	\$8,000/SP	40 SP	\$ 375,000
q.	Retail -	Gr Flr	\$250/SF	4,800 SF	\$ 1,250,000
	Office-	3 Floors	\$200/SF	80,000 SF	\$ 16,000,000
	Parking-	1 level	\$38,000/SP	140 SP	\$ 5,300,000

Draft 08-08-13
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New Development

Potential Private Investment Summary

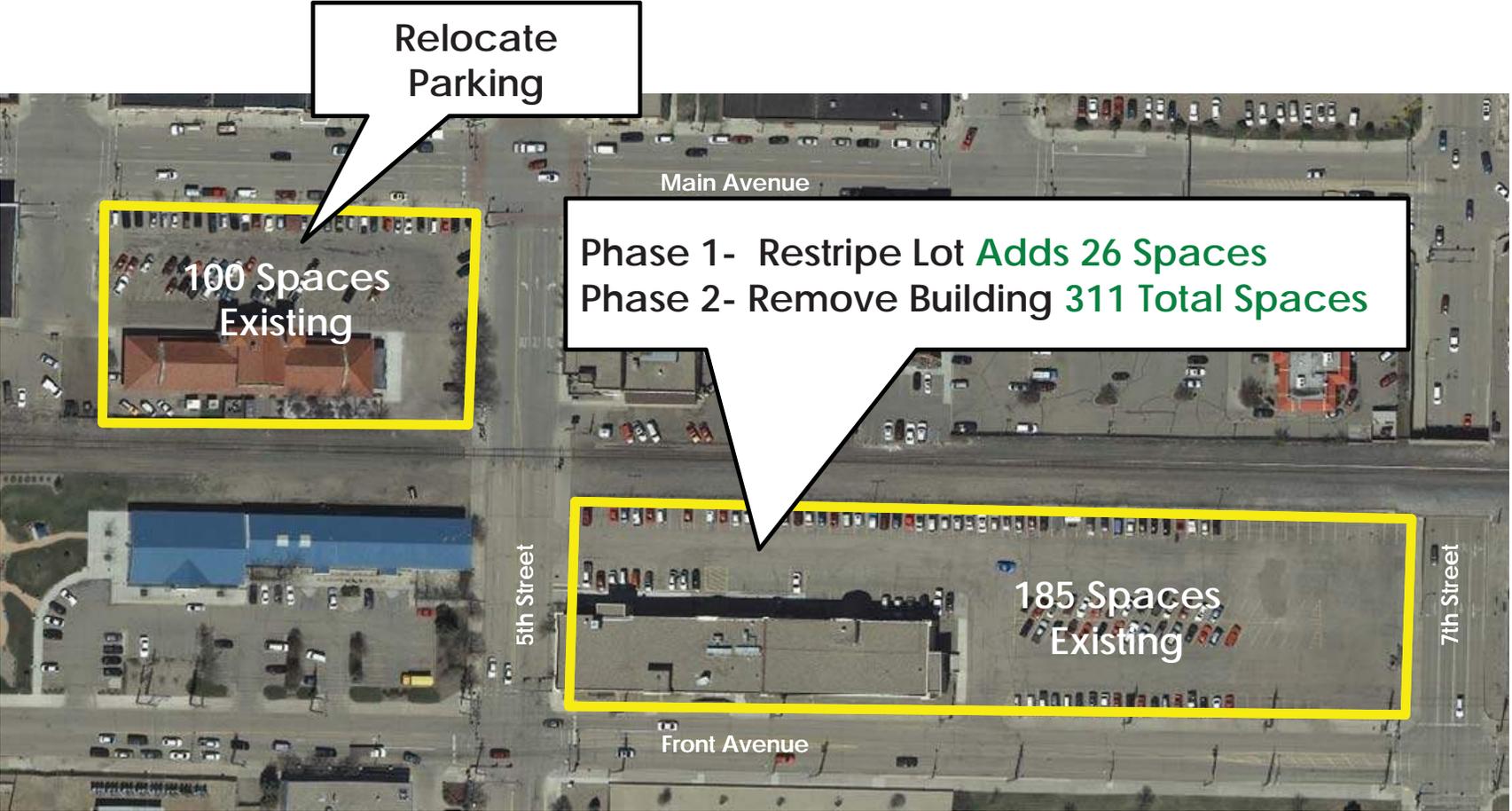
Block	Use		Cost	Units/SF/SP	Investment
r.	Retail -	Gr Flr	\$250/SF	15,000 SF	\$ 3,750,000
	Housing-	8 Floors	\$200,000 DU	130 DU	\$ 26,000,000
	Parking-	1 level	\$28,000/SP	155 SP	\$ 4,500,000
s.	Retail -	Gr Flr	\$250/SF	4,000 SF	\$ 1,000,000
t.	Retail -	Gr Flr	\$250/SF	14,000 SF	\$ 3,500,000
	Hotel-	10 Flrs	\$146,000 RM	250 RM	\$ 41,500,000*
	Parking-	1 level	\$8,000/SP	100 SP	\$ 1,000,000
u.	Retail -	Gr Flr	\$250/SF	15,000 SF	\$ 3,750,000
	Housing-	7 Flrs	\$200,000 DU	65 DU	\$ 13,000,000
	Parking-	1 level	\$8,000/SP	100 SP	\$ 1,000,000
v.	Retail -	Gr Flr	\$250/SF	12,000 SF	\$ 3,000,000
	Housing-	2 Flrs	\$200,000 DU	35 DU	\$ 7,000,000
	Parking-	1 level	\$38,000/SP	85 SP	\$ 3,500,000
w.	Retail -	Gr Flr	\$250/SF	100,000 SF	\$ 25,000,000
TOTAL:					\$317,900,000

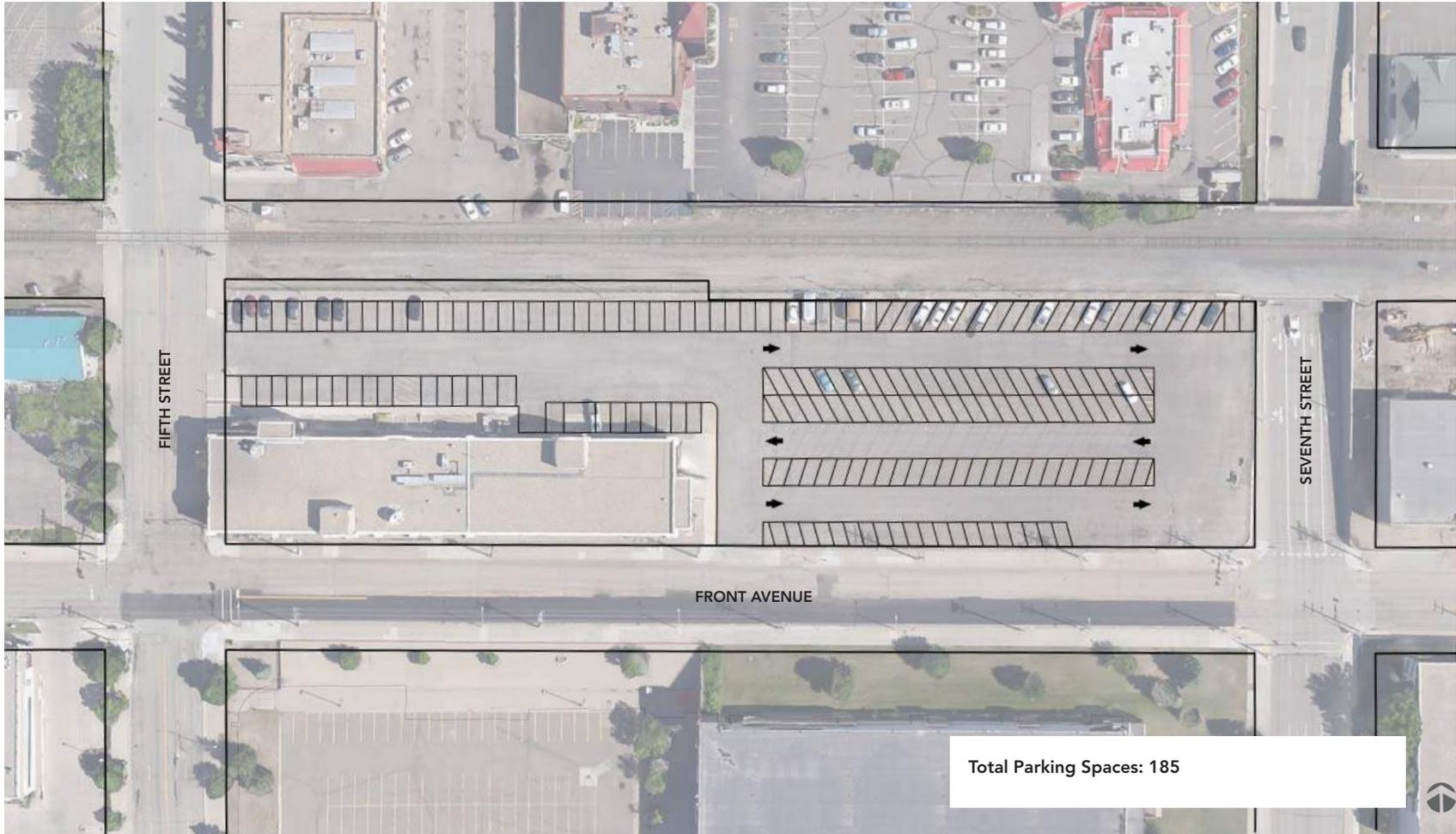
* Includes land acquisition cost of \$ 5 MIL

Draft 08-08-13
Crandall Arambula PC

DEPOT PLAZA REPLACEMENT PARKING

Construction of the Depot Plaza will eliminate the existing 100 off-street parking spaces. A strategy for replacing this needed parking in the downtown is to relocate the parking to the city-owned Front Avenue parking lot located between Fifth Street and Seventh Street. Through restriping of the existing lot and future demolition and relocation of the Bismarck/Burleigh Public Health Building a total of 311 public parking spaces could be created.

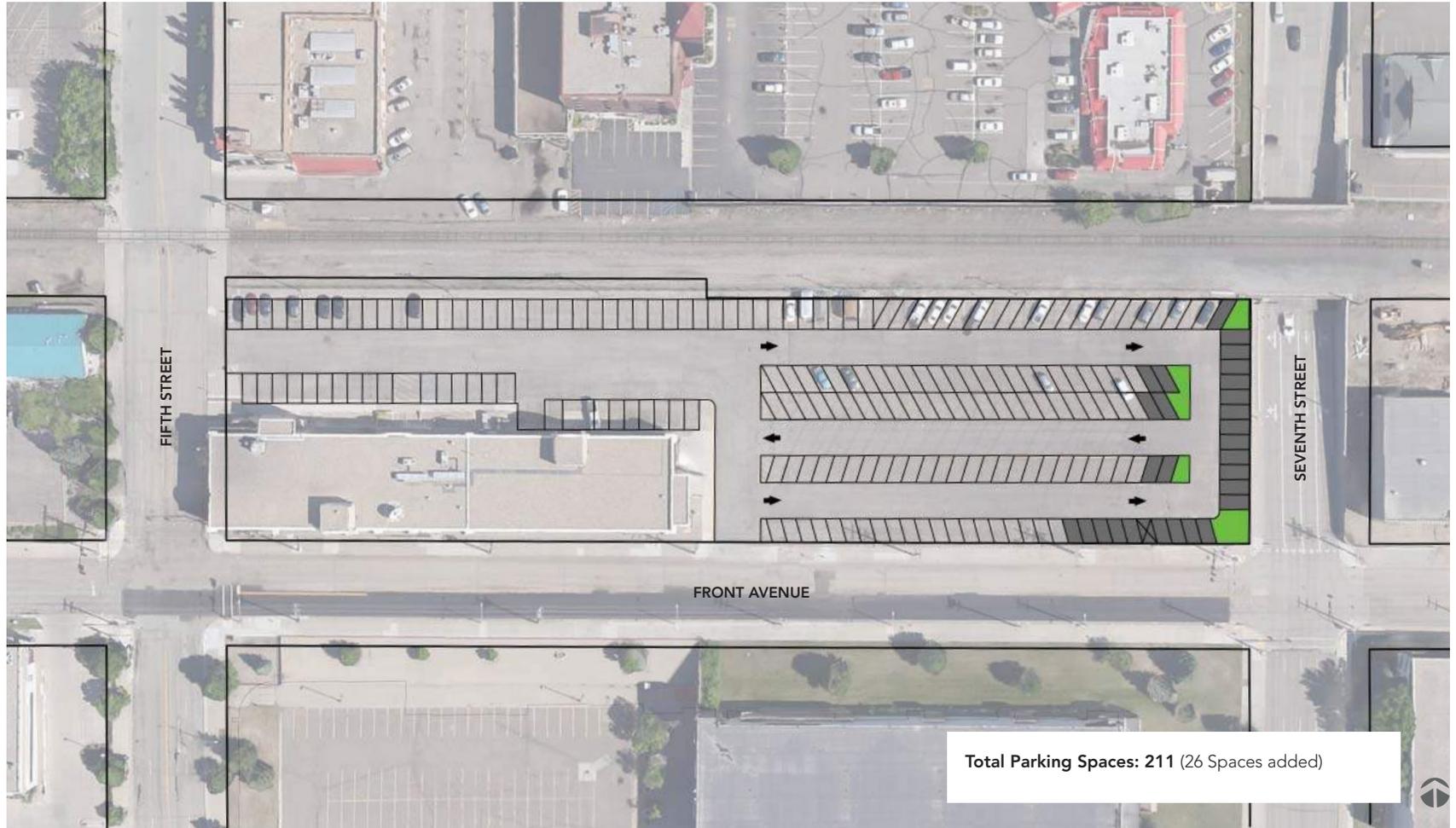




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Bismarck SubArea Study— Front Avenue Parking Lot Site
Existing Conditions

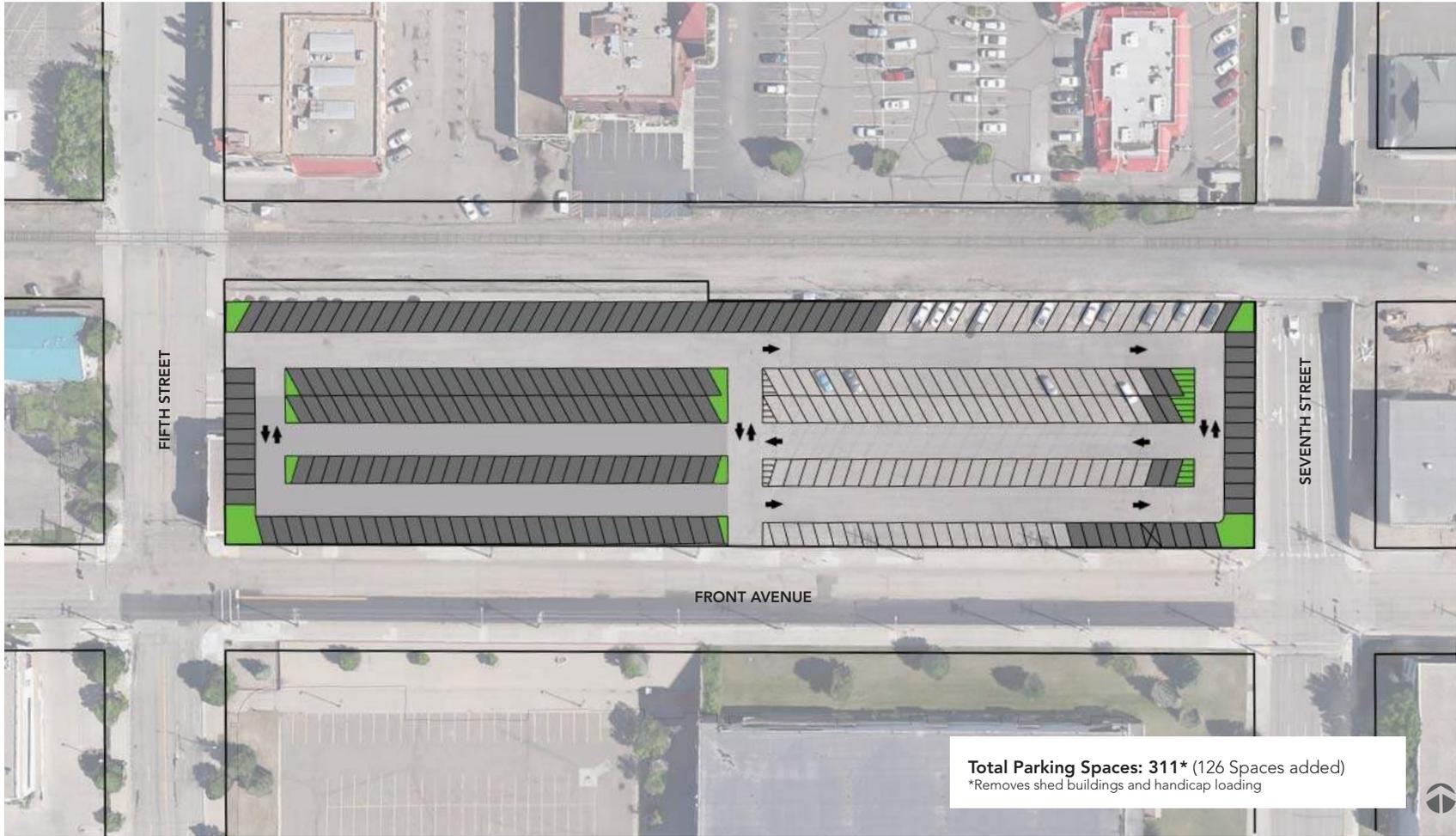




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Bismarck SubArea Study— Front Avenue Parking Lot Site
Phase I (Re-Stripe)

0' 60' 120'



Draft 09-09-13
Crandall Arambula PC

Bismarck SubArea Study— Front Avenue Parking Lot Site
Phase II (Building Demolition)





IMPLEMENTING

UPDATES TO POLICIES AND REGULATIONS

Date: September 20, 2013

To: Steve Saunders, Transportation Planner
Bismarck-Mandan MPO

From: Jason Graf, Associate Principal
Crandall Arambula

Project: Task 3.2, Recommended Updates to Existing Policies, Regulations and Guidelines

To ensure that the Subarea Plan is supported by policies, regulations and guidelines, the following memo identifies necessary amendments to or possible updates of existing City of Bismarck and/or Bismarck-Mandan MPO documents to be executed by the City and/or MPO planning and engineering staff following adoption of the Subarea Plan.

Recommended updates to existing policies, regulations and guidelines include:

- I. Main Avenue Road Diet and Diversion Study-** Additional detailed traffic analysis will be required to determine the necessary improvements for alternative east/west corridors in the Downtown that will allow for traffic calming of Main Avenue.
- II. Policy Updates-** The Downtown Subarea Plan's urban design and complete streets frameworks should be utilized as a basis for updating the Long Range Transportation Plan and Growth Management Plan to ensure policies support downtown revitalization.
- III. Zoning Ordinance Updates-** The intent of the zoning ordinance updates is to clarify and strengthen essential permitted land use types, and site design and improvement standards within the Downtown Bismarck Subarea Study boundary. Wherever possible, unnecessary permitted uses and inappropriate site design and improvement standards should be eliminated.
- IV. Downtown Design Guidelines-** The City should fund the creation of Downtown Design Guidelines to ensure high quality projects and development certainty for private investors and developers. Downtown Bismarck Design Guidelines would aid designers and developers in understanding the City's urban design expectations by providing a framework for an orderly discretionary review process that would supplement and strengthen downtown regulatory codes.
- V. Downtown Street Design Standards-** Downtown Street Standards will need to be developed for streets within the Downtown Subarea and would replace the Streetscape Design Guidelines for Downtown Bismarck (1995). Implementation of Downtown Street Design Standards will ensure that a complete-streets approach to circulation design is built as envisioned in the Downtown Bismarck Subarea Plan.
- VI. Managing Implementation-** Plan implementation will require a coordinated effort that includes establishing an updated regulatory framework, city departments and agencies cooperation, public and private partnerships, integrated funding strategies and a central managing entity and oversight committee to direct implementation efforts
- VII. Financing-** The City of Bismarck has a variety of fiscal resources to assist in the implementation of downtown projects.

I. MAIN AVENUE ROAD DIET AND DIVERSION STUDY

Intent

The single defining element of the Bismarck Subarea Plan is the creation of a retail and entertainment destination along Main Avenue and 5th Street. This will require converting, at a minimum, Main Avenue (1st St to 6th Street) and 5th Street (Main Avenue to Bowen Avenue) from a 4-lane section to a 3-lane section. The benefit of this conversion is:

- Improved traffic circulation by way of signalization improvements and a dedicated turn lane
- Pedestrian enhancements that include widened sidewalks to support street level retail and on-street curbside parking
- Curb extensions at intersections to ensure safe and comfortable pedestrian crossings

The Downtown Bismarck Subarea Study - Traffic Operations Review by SRF provided a planning-level analysis of the conversion of Main Avenue and 5th Street, as well as, two-way operations on Chancellor Square. The results of that analysis concluded the following:

- Travel times on Main Avenue are not significantly impacted
- Main Avenue intersections would operate at an acceptable Level of Service
- There is sufficient capacity to convert 5th Street to a three-lane facility
- 2035 traffic operations would require a two-way Broadway Avenue
- The Chancellor Square conversion to two-way operations has the potential to provide some diversion from Main Avenue.

Additional detailed traffic analysis will be required to determine the necessary improvements needed to improve access through alternative east/west corridors in the Downtown and help divert some “through” traffic away from Main Avenue. To ensure that the adjacent corridors can adequately support the potential diversion from Main Avenue, the traffic analysis should consider the following:

- Front Avenue must remain open to two-way traffic
- Allow two-way traffic on Broadway Avenue (i.e. Chancellor Square conversion)
- Extend/connect Broadway Avenue to Main Avenue
- Access modification at 4th Street (right-in/right-out or three-quarter) and 6th Street (three-quarter), including removing the traffic signals. This would likely improve vehicular operations, but would be less pedestrian friendly- consider maintaining the signal at 4th Street
- Addition of right-turn lanes along Main Avenue, where appropriate
- Modification of 3rd Street to have two through lanes in the northbound/southbound direction (i.e. shared left/through and shared through/right lanes)

II. POLICY UPDATES

Intent

This Downtown Subarea Plan document will need to be adopted by the City Commission and Metropolitan Planning Organization (MPO) as a supporting element to the Long Range Transportation Plan (LRTP), and the Growth Management Plan. The Plan will replace the Central Business District Plan (1993) as the guiding policy framework and implementation plan for Downtown Bismarck. The Downtown Subarea Plan's urban design and complete streets frameworks should be utilized as a basis for the current in-process update to the LRTP and Growth Management Plan to ensure coordinated policies that support downtown revitalization.

Growth Management Plan

Ongoing update of the Growth Management Plan primarily includes modification to the Future Land Use Plan (FLUP) outside of the Downtown Bismarck Subarea Study boundary. The draft FLUP and urban design framework are generally aligned, as the FLUP designated much of the Subarea Study boundary as mixed-use development and the overarching land use concept for the Growth Management Plan update is a 'Development Block Concept' that supports and encourages a mix of uses within specifically identified land use areas. The Bismarck Subarea Plan should be used as a guide for implementing future land uses within the downtown core area.

Long Range Transportation Plan 2015-2040 (MPO)

The Long Range Transportation Plan 2015-2040 currently being updated should include consideration of the Downtown Subarea Study. It is recommended that an alternate scenario for land use assumptions for transportation model purposes of the Long Range Transportation Plan account for the additional homes and jobs identified in the Downtown Bismarck Subarea Study. Furthermore, The City Commission should approve funding for the Main Avenue Road Diet and Diversion Study to more definitively determine necessary traffic diversion improvements and priorities for necessary capital projects and costs for roadway diversion improvements, streetscape enhancements, and bicycle and pedestrian facilities improvements.

Bismarck Central Business District Plan (1993)

Much of the concepts and frameworks identified in the Central Business District Plan (CBDP) have carried over into the Downtown Bismarck Subarea Plan- most directly in the general location of key public gathering spaces (Depot Plaza and the Mehus Commons) and the prominence and importance of the Main Avenue and Fifth Street corridors as areas for concentrated retail and entertainment infill and redevelopment. Fifth Street should provide a seamless link between the downtown and the Civic Center/Kirkwood Mall area south of the rail line. The Subarea Plan expands upon the Districts concept of the CBDP and provides greater detail and direction for growth in employment, strengthening Downtown retail in conjunction with Mall expansion and more importantly encouraging increased housing opportunities. Circulation concepts also deviate from the original plan with a greater emphasis on calming traffic along Main Avenue and Fifth Street that is essential for creating a Downtown pedestrian oriented shopping and entertainment environment. More specifically the Subarea Plan provides additional detail and the priority actions required for Plan implementation. The adopted Downtown Bismarck Subarea Plan will replace the Central Business District Plan (1993) as the guiding policy framework and implementation plan for Downtown Bismarck.

Renaissance Zone

The Renaissance Zone encourages reinvestment in downtown and requires design review by staff and the Renaissance Zone Authority for development projects within the Renaissance Zone boundary. In an effort to provide sufficient design review and financial incentives for development that implements the Downtown Bismarck Subarea Study, the Renaissance Zone District boundary should be modified. The location of the modified boundary should include:

- All parcels west of Fifth Street from the BNSF Rail Right of Way south to Bowen Avenue and west to Mandan Street
- All parcels south of Bowen Avenue to Ingals Avenue from Third Street to Seventh Street

Modification of the Renaissance Zone boundary that adds additional land area will require removal of areas currently included in the Renaissance Zone Boundary.



Renaissance Zone

III. ZONING ORDINANCE UPDATES

Establishing the regulatory framework in the form of codes, standards and discretionary guidelines for design review are time sensitive measures to be instituted to ensure future development meets the intent of the Subarea Plan, and encourages quality development while providing certainty to private developers and the community. The necessary regulatory and design review process updates include:

- Creating a Downtown Subarea Overlay District and mapping of overlay standards to include required ground-floor uses, build-to-lines, active edges, building heights and areas of no minimum residential lot size requirement
- Consolidating the Subarea into three zoning districts by expanding the Downtown Core, Downtown Fringe and Health Medical Zone Districts
- Amending the permitted uses for each zoning district to allow for the predominate mix of uses identified in the Urban Design Framework of the Subarea Plan with an emphasis on accommodating and encouraging mixed-use and residential development
- Creating Design Guidelines, a Design Review Committee, and a Design Review Process as an essential tool for review of Subarea buildings and public areas
- Codifying Street Standards and details for the Subarea to ensure unified and consistent design and construction of streets, sidewalks, intersections and public spaces

When considering a zoning approach for the Downtown Subarea it is important to ensure that:

- A vertical or horizontal mix of uses can be accommodated
- Encourage appropriate residential density to support the retail and commercial uses
- Ensure retail and commercial uses are not precluded by incompatible uses, at street level within the Subarea

- The scale, and form, of buildings is in context with community values and transitions in scale to maintain compatibility between lower and higher intensity uses
- The orientation of buildings supports an active street edge
- Public parks or spaces are centrally located and function as amenities for higher intensity uses
- Encourage a street network and block form that supports development and multi-modal access

Of the three types of zoning approaches a Subarea Overlay offers the best possible tool for establishing the regulatory framework within the context of this planning effort. The types of zoning approaches are as follows:

Traditional Zoning

Traditional zoning defines and designates land use zones and stipulates for each zone and zoning category maximum densities and building heights, maximum lot coverage and minimum setback, yard and lot dimensions. Conventional zoning has produced patchwork quilts of single-use districts and private enclaves, often with minimal vehicular, pedestrian or visual connections between neighboring zones.

Concerns for the use of traditional zoning to regulate neighborhood centers are as follows:

- Separates, rather than mixes land uses
- Sets minimum (or maximum) standards/requirements, rather than promoting desired outcomes
- Doesn't address important urban design and "quality" issues
- Doesn't address the "public realm" (streets, street/building interface, open spaces)

Form Based Codes

Form based codes address the relationship between building facades and the public realm, the form and mass of buildings in relation to one another, and the scale and types of streets and blocks. The regulations and standards in form-based codes are keyed to a regulating plan that designates the appropriate form and scale of development rather than simply distinctions by the use of land use zones.

Concerns for the use of form-based codes to regulate downtown are as follows:

- Requires a full code rewrite for the city and regulating plan increasing the time and cost to create the implementing regulatory framework
- Form-based codes can be prescriptive and very rigid which impact development time lines and limit flexibility
- Allows for competing uses that dilute the certainty for specific outcomes for types of uses to build out over time, such as a retail and commercial hub

Subarea Overlays

Subarea Overlay zoning identifies the type and assembly of land uses that encourage mixed use development and required standards for private development (such as build-to-lines, required ground-floor uses and active edges) and the design of public spaces and streets within a specific defined area.

Subarea Overlay zoning provides the greatest opportunity to tailor make the district you wish to create that can exist within the existing or modified zoning code. The Subarea Overlay zone supports a land use framework with appropriate land use configurations that encourage the right kind of development in a specific area that is not precluded by simply using a form-based code approach.

In the case of the Downtown Subarea Plan, the Subarea Overlay offers a more condensed version of the form-based code for building form, by limiting prescriptive elements to only those that are essential.

Intent

The intent of the zoning ordinance updates is to clarify and strengthen essential permitted land use types, site design and improvement standards within the Downtown Bismarck Subarea Study boundary. Wherever possible, unnecessary permitted uses and inappropriate site design and improvement standards should be eliminated. While, additional regulation will need to be developed to ensure that specific uses are required and current uses are prohibited within the Downtown to meet the Downtown Bismarck Subarea Plan vision.

Throughout Downtown, existing permitted conforming uses should be 'grandfathered' to remain, be improved upon, or sold and operated 'as-is' until renovated as property owners deem necessary. Furthermore, existing private property shown for public uses (such as a park) would not be rezoned until the parcels are acquired by a public entity or transferred to public use by easement, dedication, or other means.

The Downtown Subarea Plan framework elements generally comply with existing policies and regulations. However, where inconsistencies exist, updates should be made to existing City documents. These updates should address:

- Existing zoning, including permitted uses and development standards. Regulatory changes should apply to all new or redevelopment projects. Where current uses are inconsistent with the future vision, 'Grandfather' existing uses to allow current uses to remain until renovated
- Building and site development standards that emphasize compatibility with existing historic design characteristics and a pedestrian orientation
- Creation of new discretionary review design guidelines to supplement regulations

Title 14 Ordinance Update

The Subarea boundary is regulated under eight District Zone designations. The Downtown Fringe and Downtown Core District Zones provide the model for mixed-use and pedestrian oriented development along with design standards and a design review process that encourage quality development consistent with the Subarea Plan. However, the design review process within the Title 14 code should be amended and the design review elements of the code should be integrated in a new Design Guidelines document. Development projects within the Subarea should be required to go through a separate design review process and reviewed by an appointed Design Review Committee. Additional information on design guidelines and a recommended design review process is identified in section IV. Downtown Design Guidelines.

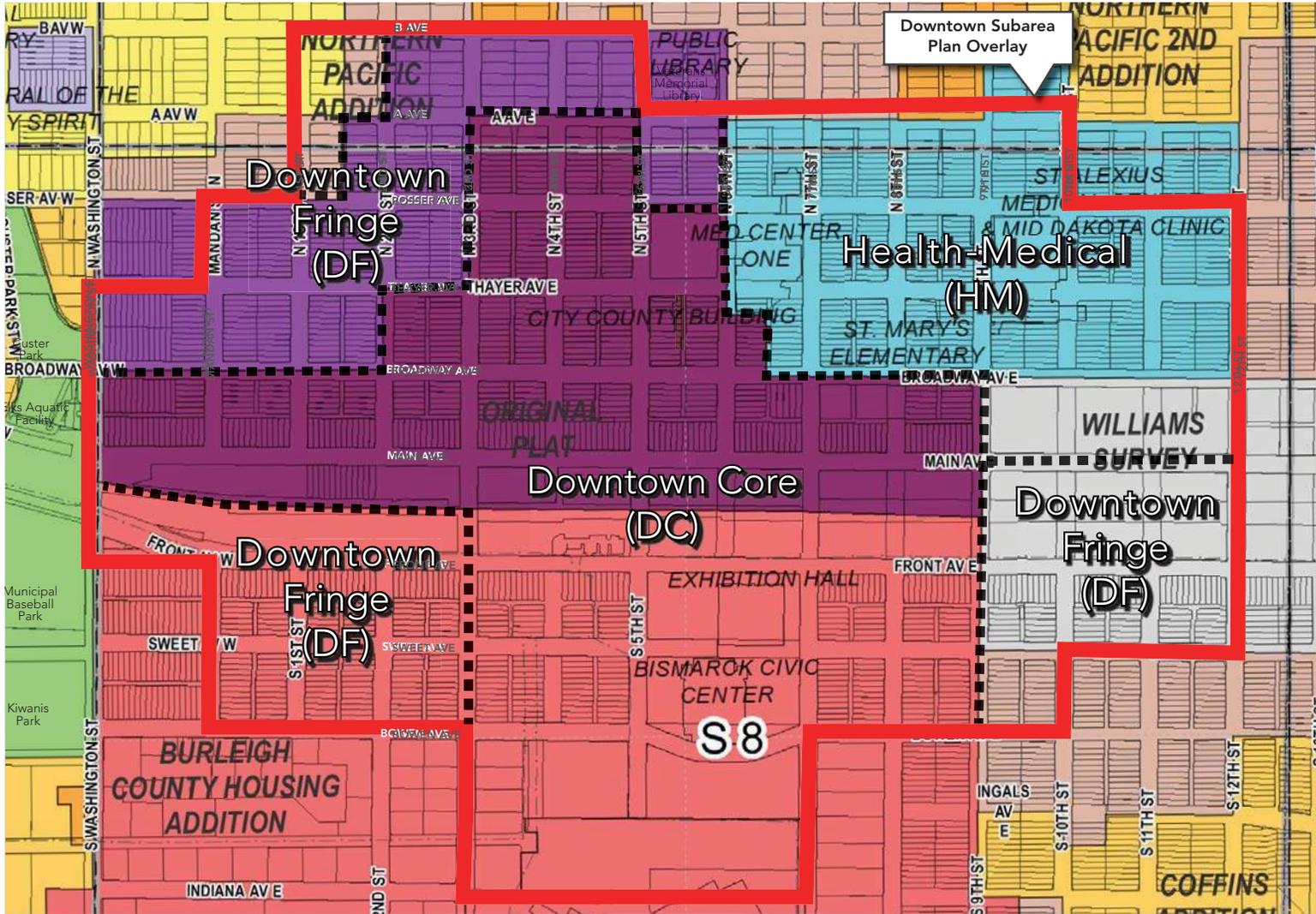
To align current code requirements with the Subarea Plan, two approaches to address inconsistencies might be to:

- 1) Maintain current zoning and provide additional or revised permitted use tables. Additionally, provide new or revised site design and development standards.
- 2) Consolidate the Downtown Subarea Overlay into three Zoning Districts by expanding the Downtown Core, Downtown Fringe and Health-Medical Zoning Districts. For each zone, rewrite the permitted use tables, and create new or revised dimensional and design standards consistent with the urban design framework of the Downtown Bismarck Subarea Plan.

Suggested ordinance updates include:

- 1. Foster mixed-use development.** Adjustments to foster mixed-use development and increase Downtown vitality, existing regulations that limit or prohibit higher density residential development should be revised or amended to promote increased residential density (dwelling units per acre) and/or intensity (floor area ratio). No maximum densities or intensities should be applied. Instead, maximum heights or other means of controlling compatibility, massing, and building form should be enacted as part of updates to the Dimensional and Design Standards.

Title 14- District Zones Consolidation



2. Reassess downtown parking requirements. Currently, Downtown businesses are largely within the parking exempt- Downtown Parking District (See graphic). Outside of the district, parking requirements within the Commercial General and Commercial Regional districts are onerous for housing, retail, and commercial development. In some cases requiring parking ratios at suburban development standards that will only continue the development of large surface lots. As an option to the current parking requirement, the City should consider the feasibility of requiring either on-site parking at lower minimum thresholds such as 2.5 spaces per 1000 SF for retail/commercial and 1 space per unit for residential or require in-lieu-of parking fees to be assessed on new development for providing off-site parking within designated parking ramps.

Downtown Parking District



3. Expand downtown parking enforcement. Parking enforcement should be expanded to include all block frontages of the Subarea Plan boundary south of the BNSF rail line. A parking enforcement strategy should be developed to:

- Maximize high turnover of on-street parking along Third Street and Fifth Street (1 hour- no reparking) between Front Avenue and Ingals Avenue.
- Consider establishing a restricted parking zone (RPZ), a half block west of Third Street to Mandan Street to help ease parking congestion for residential development through the use of signed time limits from which vehicles displaying a valid RPZ permit are exempt.

Downtown Parking Time Zones



Specific Permitted Use Updates

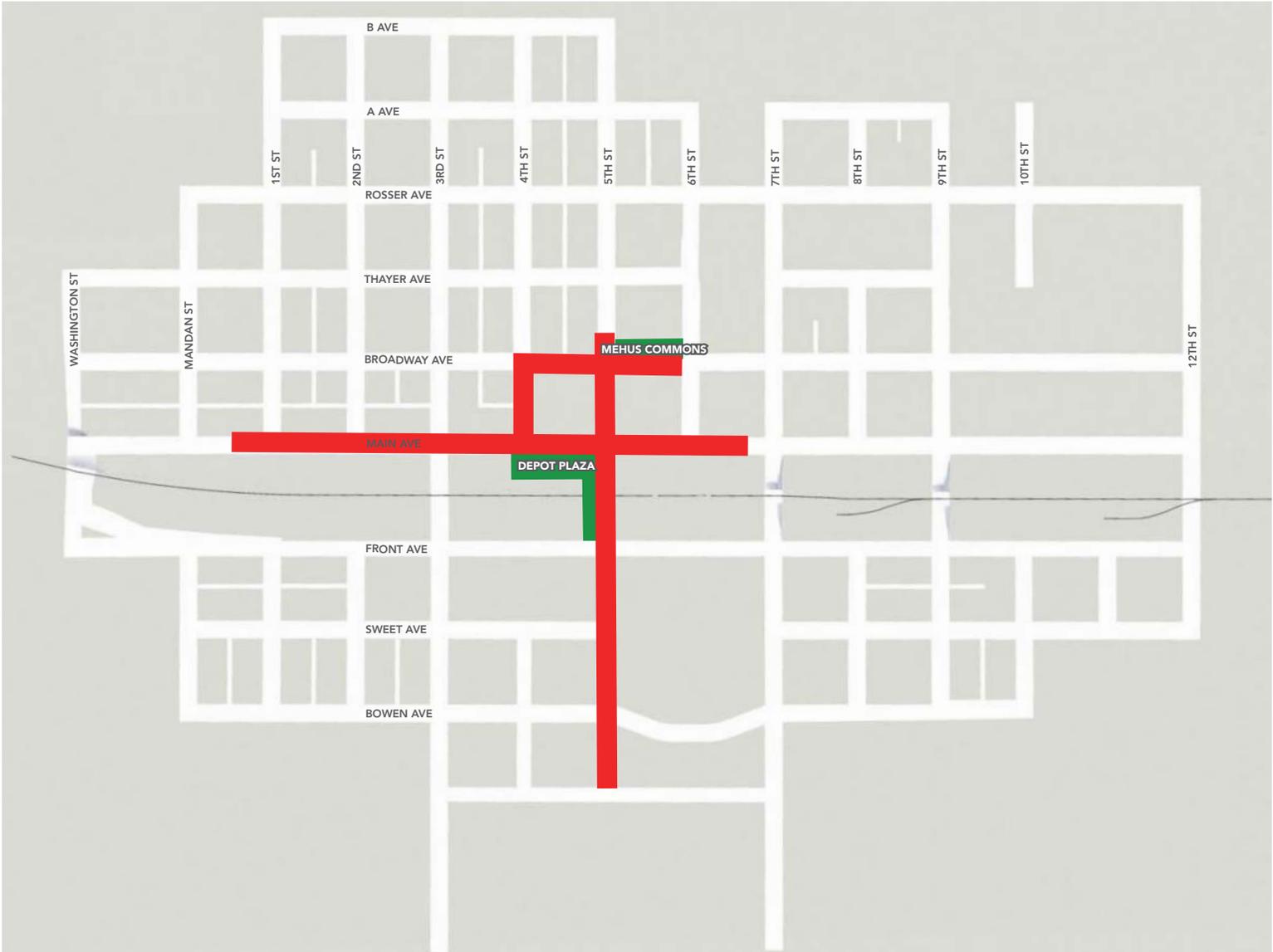
Intent

Permitted uses outside of the Downtown Core and Downtown Fringe zone districts within the Subarea Boundary should be modified to the predominate and mix of uses identified in the Downtown Bismarck Subarea Plan.

The following are essential zoning ordinance permitted-use updates that must be addressed:

- 1. Modify existing permitted-use tables—** for the District Zones within the Downtown Bismarck Subarea boundary- prohibit, provide restrictions, or set forth conditions-of-use upon auto-oriented uses (drive-through windows of banks, restaurants, and other similar businesses) and allow for and encourage retail development along Main Avenue, Fifth Street, and Broadway Avenue and higher density residential development (no maximum dwelling unit density requirements) within existing and new development.
- 2. Retail Opportunity Areas—**defined exclusively as the sale of goods, restaurants, and entertainment uses, encourage the street frontages along Main Avenue, Broadway Avenue and Fifth Street to include ground-floor retail and limit or exclude non-retail uses that are not complimentary to shopping, dining and entertainment. Exceptions for residential or office lobbies should be allowed/reviewed as a conditional use. Prohibition of service bays, entrances to parking lots or service and loading bays should also be enacted.

Retail Opportunity Area



Dimensional and Design Standards Updates

Intent

To ensure that the Main Avenue, Broadway Avenue and Fifth Street storefronts are active and animated, requiring additional 'form-based' building site design and façade design standards should be enacted. The following standards are recommended to be applied throughout the Subarea Overlay boundary and should be placed in a map format.

In general, it suggested that where applicable standards such as, building heights, and setbacks be located on maps rather than the text form that exists today. Since the mapping standards would apply throughout the Subarea Overlay these maps could be located at the beginning of the Chapter and provide a quick reference for Title 14 users.

1. Requirement of active edge—block frontages within the retail opportunity area should also include a provision for active edges on the ground floor of buildings. Active edges are building frontages with direct sidewalk entries and a high degree of transparency. Active edges increase visual and physical interaction between people inside and people outside of the buildings and create a safe and vibrant pedestrian environment. Active edges must provide a minimum of 70 percent transparent glass along ground-floor facades; frosted, tinted, reflective or other types of glass that diminish transparency should be prohibited and primary ground-floor uses must be oriented to the public right-of-way.



Active Edges

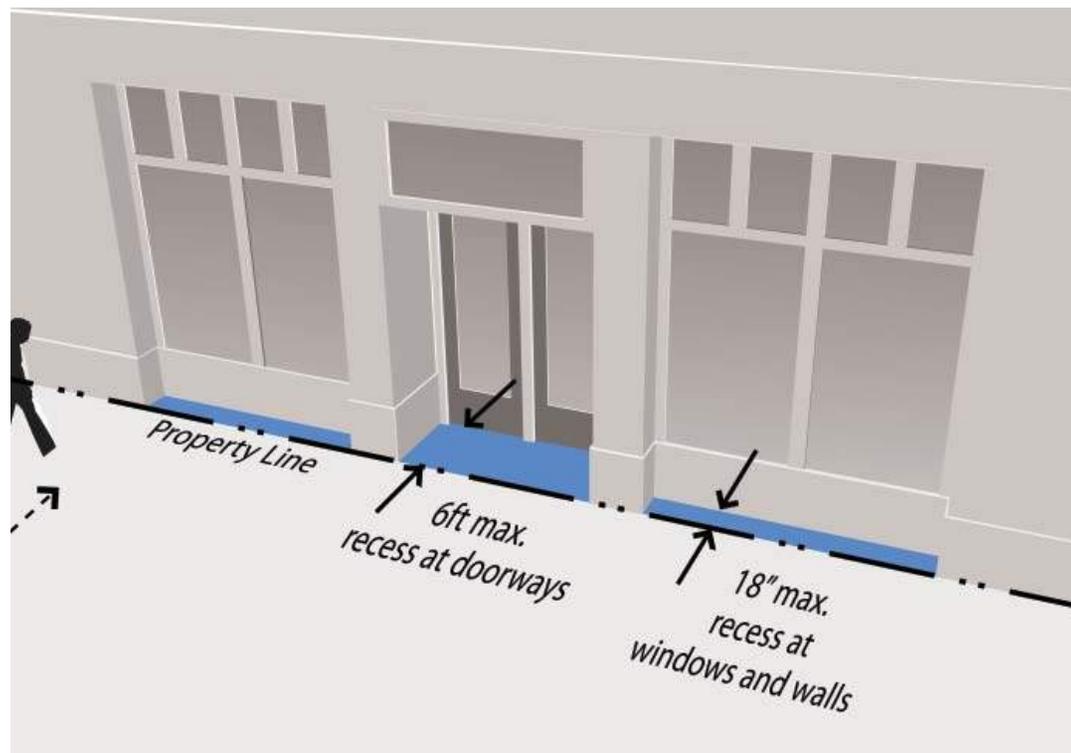
The following active-edge criteria should be met for all other land uses throughout the Downtown Subarea:

- Primary entrances must be oriented toward the street
- Windows should be provided along facades with a minimum of 50 percent transparent glass along ground-floor facades; frosted, tinted, reflective or other types of glass that diminish transparency should be prohibited
- Art walls, flower booths, newsstands or other activating uses are appropriate throughout

2. Requirement of Build-to-Lines—block frontages within the retail opportunity area should also include a provision for Build-to-Lines that require a continuous ‘street wall’ framing identified retail streets and the plaza. A diagram that illustrates where ground floors of buildings must engage the property line directly should be provided as indicated on the right. Exceptions to these ‘zero-setback’ requirements might include—windows and walls recessed up to 18” from the property line to accommodate columns or other architectural elements and those frontages along the west side of Fifth Street where a ten foot building setback is encouraged to expand the sidewalk area to accommodate larger outdoor seating areas specifically within the Fifth Street entertainment zone.

For those areas outside of the retail opportunity area build-to-lines should include a maximum ten-foot setback to provide:

- Privacy of street level residential uses, dining and entertainment uses that intend to use the setback area for outdoor seating
- Interruptions for accommodation of passageways, building lobbies, or private courtyard entrances

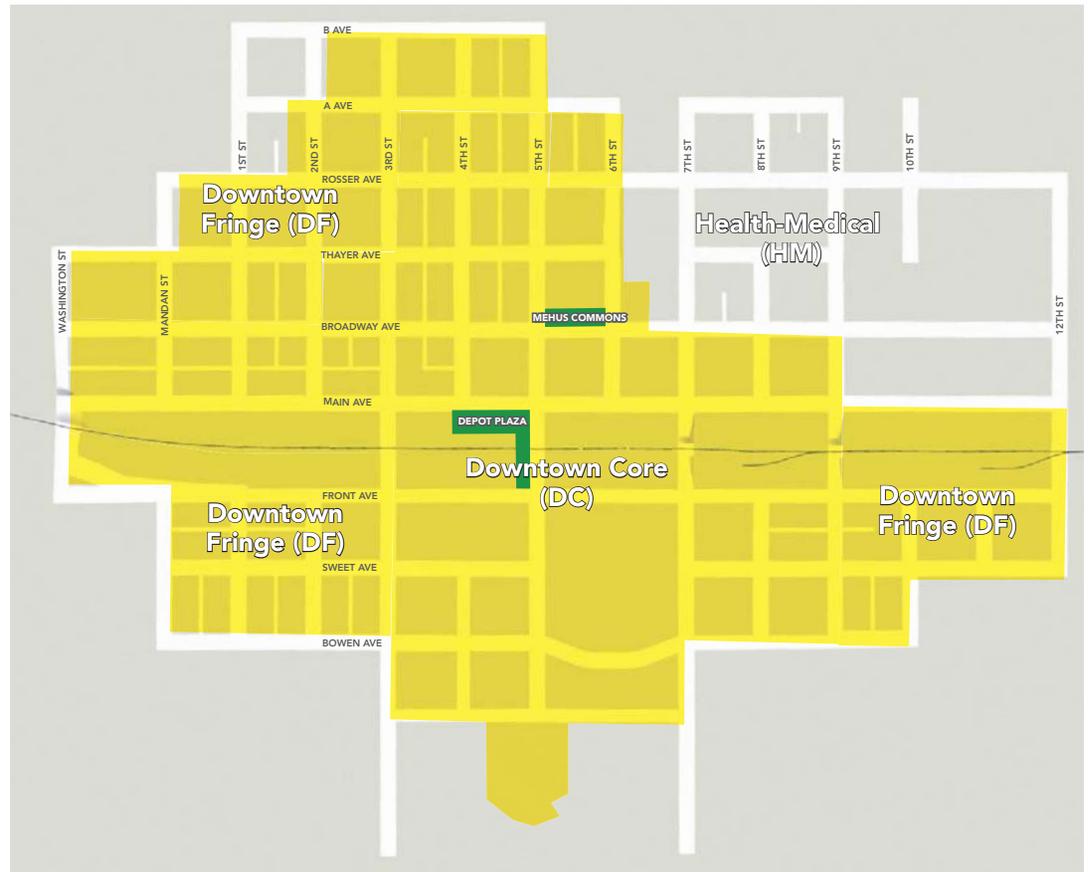


Build-To-Lines

3. No Minimum Residential Single-Use Lot Size Requirements

New high-density residential development is an essential component of the Downtown Bismarck Subarea Plan. This proposed overlay is suggested for only those areas where current zoning requirements may limit or discourage high density residential development. The graphic illustrates where higher density development should be encouraged.

The Downtown Core and Downtown Fringe Zoning Districts limit density of permitted residential uses by requiring minimum lot area. No minimum residential lot size should be required for single purpose residential buildings in the areas illustrated in the diagram.



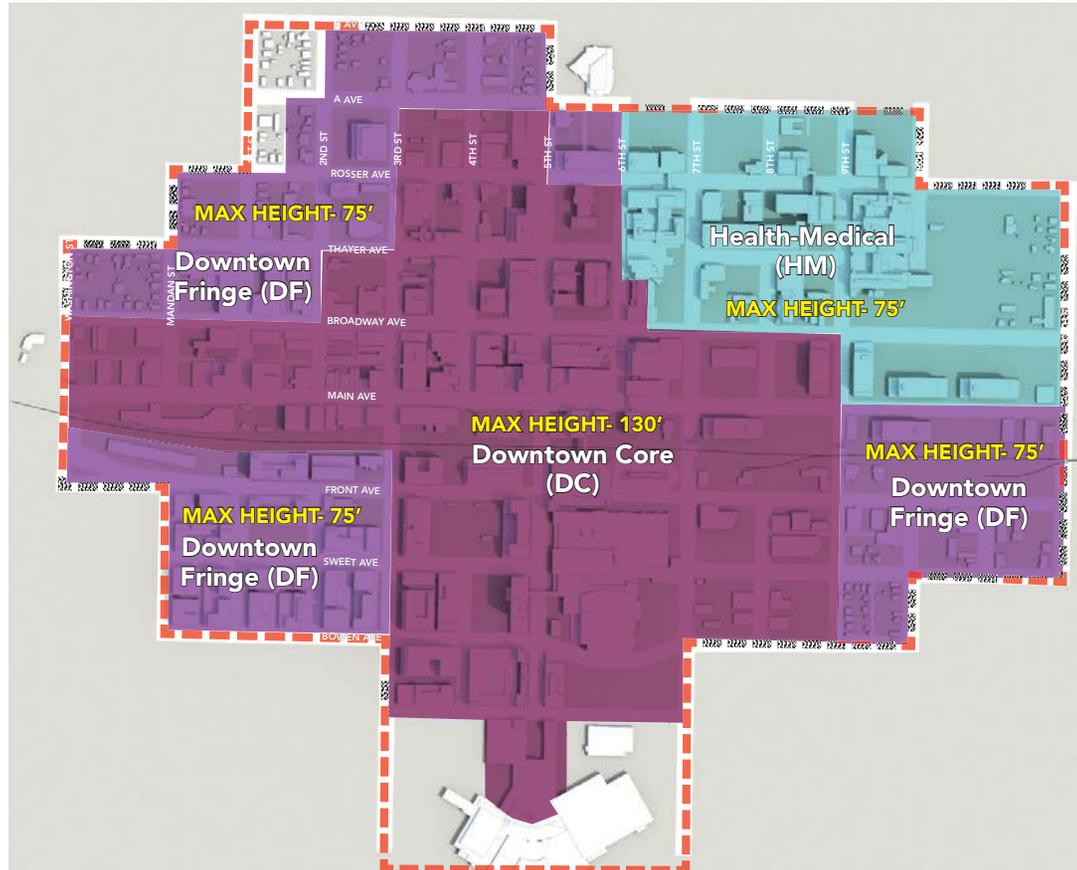
Residential Single Use Lot Size Exemption

4. Building Heights

The diagram shown to the right identifies proposed maximum building heights. Adjustments are suggested only for key development areas where additional density is envisioned.

Within the Downtown Fringe and Health Medical District Zones consider increasing the allowable building heights to 75'. A 75' dimension allows for higher density residential construction that consists of wood frame construction over a groundfloor constructed of concrete. Typically called five-over-one construction, this type is an affordable model for mixed use development that supports increased housing density.

In addition, consider 'stair-stepping' requirements for buildings to establish transitions with development located adjacent to significant historic buildings or consider reducing the height limit to 50' when adjacent to well established single-family neighborhoods.



Building Heights

IV. DOWNTOWN DESIGN GUIDELINES

Intent

Currently there is limited design review required within the Downtown Bismarck Subarea and what is required is imbedded in the design standards for only the Downtown Core and Downtown Fringe zoning districts. Many of the so-called standards are technically guidelines and as such should be removed and located within a set of Subarea Overlay-wide Design Guidelines. The recommended design standards to be removed and repurposed as design guidelines include:

- **d. New Construction**
- **e. Building Materials**
- **d. New Construction**, *except for the following which should remain as a standard Non-transparent mirrored or one-way glass with a reflectance greater than forty percent (40%) shall be prohibited from covering more than forty percent (40%) of the exterior walls. All subsequent renovations, additions and related structures undertaken after the construction of an original building shall be finished with materials comparable to those used in the original construction and shall be designed in a manner conforming to the original architectural design and general appearance.*
- **f. Building Colors**
- **i. Width**
- **j. Horizontal Rhythms**
- **k. Entrances**

Furthermore, the review process falls into the hands of the Renaissance Zone Authority rather than the Planning and Zoning Commission which should be the standard procedure.

In an effort to establish greater clarity in the design review process and to expand the reach of design review throughout the Subarea Overlay boundary the City should fund the creation of Downtown Design Guidelines to ensure high quality projects and development certainty for private investors and developers.

A Design Review Commission should be created to review all projects within the Downtown Subarea boundary. Minor projects should be reviewed by City staff. The Design Review Commission should be appointed by the City Commission and comprised of architects, landscape architects, engineers, real estate developers and citizens at large. The Design Review Commission would provide recommendations to the Planning and Zoning Commission and City Commission for all projects, using the Design Guidelines as an exclusive evaluation tool.

Downtown Bismarck Design Guidelines would aid designers and developers in understanding the City's urban design expectations by providing a framework for an orderly discretionary review process that would supplement and strengthen downtown regulatory codes. The guidelines would ensure a degree of order, harmony and quality within the built environment; they would foster the development of buildings and projects that are attractive individually yet contribute to a downtown that is unified and distinctive as a whole.

Role of Design Guidelines

There should always be many ways of meeting a particular guideline. The guidelines should not prescribe specific design solutions, nor should they be rigid requirements without flexibility.

The design guidelines should provide a descriptive template for maintaining and improving the urban character of Downtown, without dictating or prescribing a specific style or theme. The guidelines should however foster the evolution of Downtown Bismarck from an auto-dominated to a pedestrian-friendly downtown where buildings should be built edge-to-edge and engage public sidewalks. Moreover, the Design Guidelines should ensure that new development does not treat existing older buildings as urban leftovers; lost and unattached fragments of the past. Rather, new construction and rehabilitation should respect and build upon the unique qualities of Bismarck's existing historic downtown.

Design Guideline Content

The guideline document should be the primary tool for the design review process for any new project that occurs in the Downtown Bismarck Subarea Study boundary. It should be divided into four sections or 'Guideline Elements,' each of which addresses a particular set of design concerns that affect the downtown environment. These guideline elements would include the following:

- Pedestrian Emphasis
- Architecture
- Lighting
- Signs

For each of these guideline elements, there should be an introductory page describing the intent of that section of guidelines, followed by specific guidelines. Each specific guideline should contain a descriptive statement of the guideline itself, as well as, examples of recommended and not recommended applications, both listed in text form and illustrated in photo examples. The visual examples should be included in each guideline element, as models for design and review purposes. They would provide designers, developers, and the Design Review Commission a means to effectively judge a building relative to appropriate and inappropriate design criteria.

Bismarck Character

The guidelines should address Bismarck’s unique ‘sense of place,’ its special quality and personality. People’s image of Bismarck is that of the capitol city which is hospitable and family-oriented. They should address what gives Bismarck this feeling, this ‘character’ as a unique collection of spaces and buildings, not simply a group of individual projects that could be anywhere.



PEDESTRIAN EMPHASIS

In Downtown Bismarck, the pedestrian should be the ‘priority.’ These guidelines should address the ways in which buildings and spaces may be designed to create a convenient, comfortable, human-scaled environment where people will want to be.



ARCHITECTURE

The Architecture Guidelines should promote quality development while reinforcing the individuality and spirit of Bismarck. The guidelines should promote architectural types indigenous to Bismarck and/or the Great Plains. Buildings in Bismarck should seem to be ‘at home’, reflecting its character and heritage, suiting its climate, landscape, and downtown urban setting.



LIGHTING

Lighting should not only provide nighttime security, but also encourage nighttime patronage of businesses. Lighting should create an atmosphere of festivity and activity, especially where special elements or places are concerned. Utilitarian application of glaring, offensively-colored lights should not be appropriate for Downtown.



SIGN

Signs should make it easy to locate and identify businesses as well as providing other information relevant to getting around and doing business in Downtown. However, signs should never overwhelm either building or landscape. Moreover, signs should provide information in a highly graphic format that is complementary to downtown architecture. Tasteful logos, symbols and graphics are encouraged. A strong pedestrian orientation should be encouraged for all signs.

Design Review Process

Projects should be evaluated for consistency with the Downtown Bismarck Subarea Plan, the Title 14 Zoning Ordinance and the proposed Design Guidelines. The Design Guidelines should be applied during City review of development applications. City staff should review development proposals in the Downtown and determine the appropriate review procedures. Decisions to approve, approve with conditions, or denying a proposal should be made by staff or by an appointed 'Design Review Commission'. Development projects should be reviewed to determine consistency with permitted uses and site design standards of an updated Title 14 Zoning Ordinance, and substantial consistency with the Downtown Design Guidelines. Where a project is not found consistent with the Downtown Design Guidelines, staff or the Design Review Commission may impose conditions of approval requiring the project to be modified, or it can be established that design details or other site factors warrant finding for approval of the project without meeting the particular design guidelines. For those applications that are substantially inconsistent with the guidelines, staff or the Design Review Commission should also have the option to deny the development request.

All new development, additions, remodels, and renovations within the Downtown should be subject to design review for determination of consistency with the Design Guidelines. The process for design review might be based on the extent of work proposed and whether it falls under one of three categories as follows:

1. Exterior Maintenance and Repair (Administrative staff review only for all work requiring a building permit)
2. Minor Exterior Modifications (Administrative staff review)
3. Major Exterior Modifications (Review before a newly appointed Design Review Commission)

To aid the design review process the guidelines should be summarized into a single, one page 'checklist' (see example).

DEVELOPMENT GUIDELINES CHECKLIST

Project and Applicant Name: _____
 Zoning: _____
 Building Use: _____
 Other : _____
 Submission Date: _____

DEVELOPMENT GUIDELINES

	Applies		Complies	
	Yes	No	Yes	No
1). Oak Park Character				
• Reinforce Oak Park's Sense of Place	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Integrate the Environment	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Promote Linkages to Oak Park's Heritage	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Establish or Strengthen Gateways	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Promote Architectural Compatibility	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Preserve Historic Buildings	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Integrate Art	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2). Pedestrian Emphasis				
• Reinforce and Enhance the Pedestrian System	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Define the Pedestrian Environment	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Protect the Pedestrian from the Elements	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Provide Places for Stopping and Viewing	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Create Successful Outdoor Spaces	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Integrate Universal Design	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3). Architecture				
• Residential Doors	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Residential Windows	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Residential Bay Windows	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Balconies	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Retail Doors	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Retail Corner Doors	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Retail Windows	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Building Massing	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Security/Privacy	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Roofs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Rooftops	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Wall Materials	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4). Lighting				
• Building Lighting	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Pedestrian Lighting	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5). Signs				
• Wall Signs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
• Hanging Signs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
6). Sustainability				
• Green Architecture	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Sample Checklist

V. DOWNTOWN STREET DESIGN STANDARDS

Fundamentally, the Central Business District Streetscape Design Plan (Streetscape Guidelines for Downtown Bismarck) was designed to support the Central Business District Plan (1993) and the elements of the document provide a limited foundation for directing future improvements to streets and public areas that meet the intent of the Downtown Bismarck Subarea Study. Other than a few standards for lighting, benches and fencing- the majority of the document describes multiple alternatives for any given street type, and supports the rebuild, as is, of the Chancellor Square area and continuation of elements such as raised planters at intersections that should not occur.

As primarily a guideline document there is little direction for the comprehensive set of street and public area design standards that will be required to ensure high quality and compatible design and construction of the public right-of-way and public areas. Standards elements beyond lighting, benches, monuments, and fencing such as bike parking/corrals, bollards, sidewalk paving materials and scoring, intersection design and crosswalks are not provided for in the existing Streetscape Design Plan.

Where standards do exist for lighting, benches, monuments, and fencing , these should not become the predominate standards for future improvements. A comprehensive set of street standards is required that may or may not utilize some of these existing street standard elements.

Intent

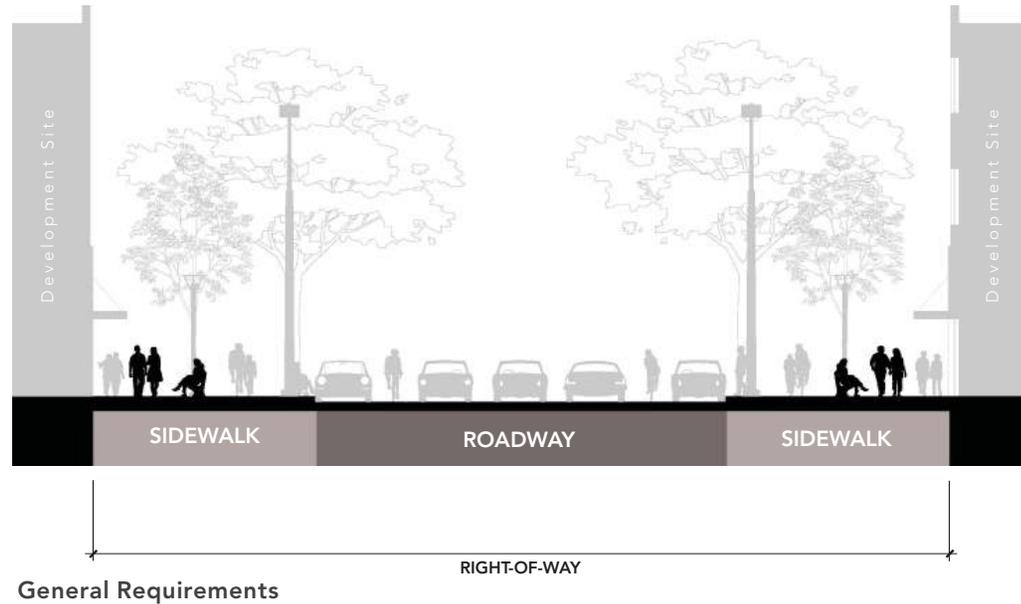
Downtown Street Standards will need to be developed for streets within the Downtown Subarea and would replace the Streetscape Design Guidelines for Downtown Bismarck (1995). Implementation of Downtown Street Design Standards will ensure that a complete-streets approach to circulation design is built as envisioned in the Downtown Bismarck Subarea Plan. Additionally, the Standards would ensure that a unified, consistently applied design of streetscape improvements are constructed throughout the Downtown Subarea. The City, other public agencies, private utility companies and private developers would be required to adhere to the Standards when constructing all new street improvements and any major replacement or repairs.

The Standards document to be developed should include general requirements and design elements. The following is an outline for the Downtown Street Design Standards and includes suggested street types and guidance for the general requirements for Main Avenue, Fifth Street, the Sixth Street Cultural Trail, and the Rail Trail.

1. GENERAL REQUIREMENTS

The General Requirements should classify and locate downtown 'street types', describe the required street type plan and section, and dimension all essential design elements of each street type for all public rights-of-way. The General Requirements should describe the:

- **Roadway**— measured from face-of-curb to face-of-curb within the right-of-way; the roadway includes travel and turn lanes, curbside parking and protected bikeways, bike lanes, landscaped medians, etc.
- **Sidewalk**— measured from development property line to face-of-curb within the right-of-way; in some instances sidewalks may extend into development easements or setbacks



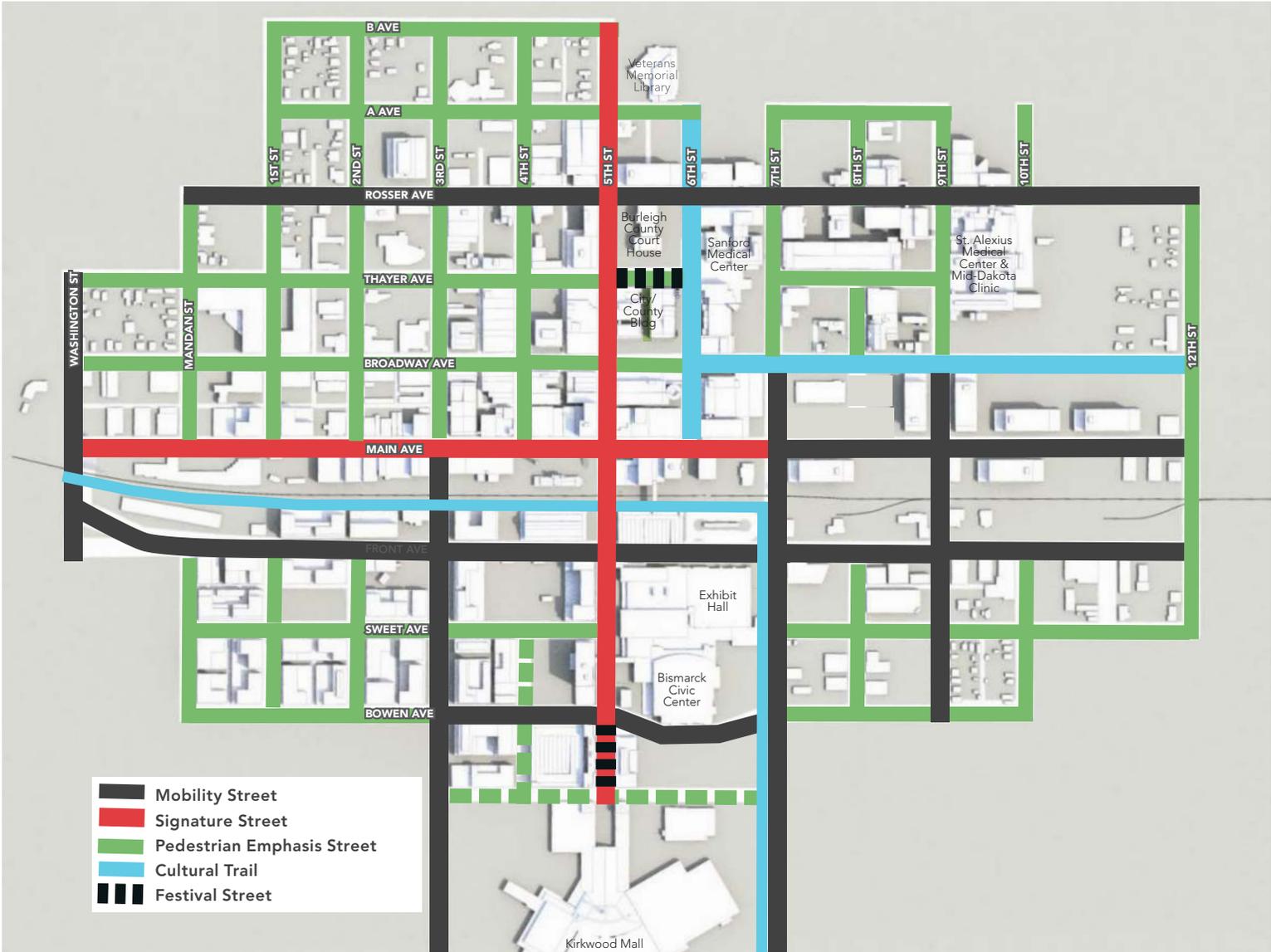
STREET TYPES

Each street within the Downtown Subarea should be assigned a standard street type. Each type should reflect the streets' intended character, transportation function, and supporting adjacent land uses. Below are recommended street types to be incorporated in a Downtown Street Design Standards document.

The street types include:

- **Mobility Street**—Rosser Avenue, Main Avenue (East of Seventh Street), Washington Street, Third Street (South of Main Avenue), Seventh and Ninth Streets (South of Broadway), Front Avenue, and Bowen Avenue function as the primary auto and truck mobility routes into the downtown and connecting to regional destinations
- **Signature Street**—Main Avenue (West of Seventh Street) and Fifth Street (Avenue A to Kirkwood Mall) establishes the 'front door' to downtown and functions as a destination for shopping, entertainment, events and government functions
- **Festival Street**—Thayer Avenue (5th to 6th Streets) and 5th Street (Bowen Ave to Ingals Ave) function as special use areas for temporary street closure during community activities and events associated with the Mehus Commons along Thayer Ave and the street oriented retail and entertainment uses along 5th Street south of Bowen Ave
- **Pedestrian Emphasis Street**—East/West Avenues' A, B, Thayer, Broadway (West of Sixth Street), Sweet, Bowen (West of Third Street and East of Seventh Street), and a New Ingals (Between Third street and Seventh Street) and North/South Streets' Mandan, First, Second, Third (North of Main Avenue), Fourth, Seventh (North of Broadway Avenue), Eighth, Ninth, Tenth (North of Broadway), and Twelfth function as essential primary pedestrian linkages within the study area supporting residential, and commercial uses
- **Cultural Trail**—Sixth Street (Main Avenue to the Capitol), Broadway Avenue (East of Sixth Street), Adjacent to the BNSF rail line (Seventh Street to the Missouri River), Seventh Street (Front Avenue to the University of Mary) function as the primary active transportation routes for biking and walking within a mile of the downtown

Street Types

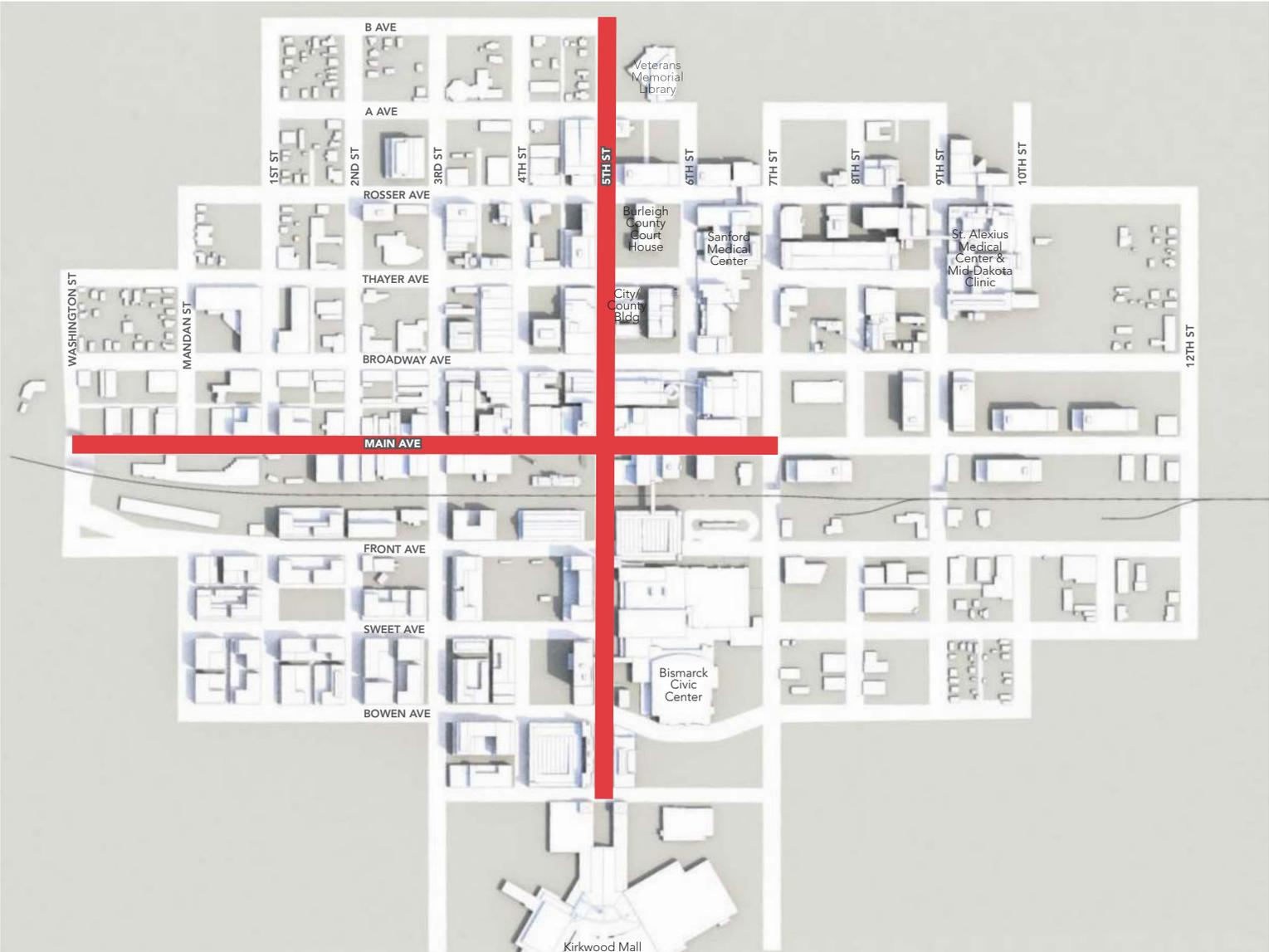


SIGNATURE STREET

The Signature Street– Main Avenue and Fifth Street– are the two most important streets in the Downtown and a destination for downtown retail, arts and entertainment. These streets are designed to be memorable and timeless. Not just a place to drive through, this unique and pedestrian oriented environment is the active 18-hour heart and focus of the community.

- **Main Avenue**–the heart of Downtown’s historic retail center includes a traffic calmed street with parking, widened sidewalks and intersection enhancements that support a high quality shopping environment
- **Fifth Street**–as Downtown’s ‘spine’ Fifth Street is the primary connector between the historic retail and employment center north of the rail line and the Civic Center and Kirkwood Mall to the south. The street provides a gracious pedestrian environment with an emphasis on landscaping and pedestrian amenities that support retail, office, entertainment and Civic Center uses

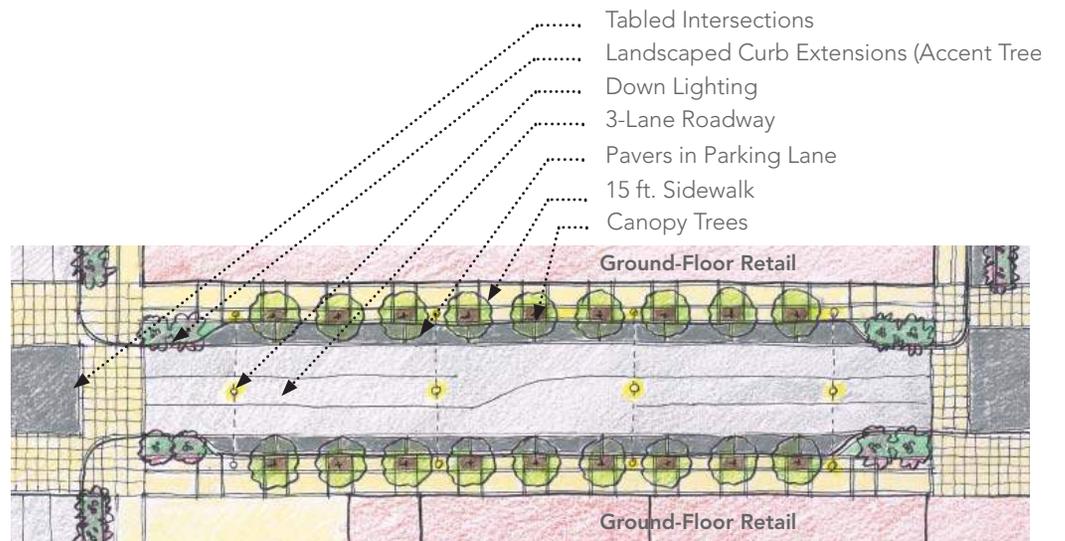
Signature Street



MAIN AVENUE SIGNATURE STREET REQUIREMENTS

The standard Main Avenue Signature Street section represents the preferred design for the intended character and function of Main Avenue as a retail emphasis street west of Seventh Street.

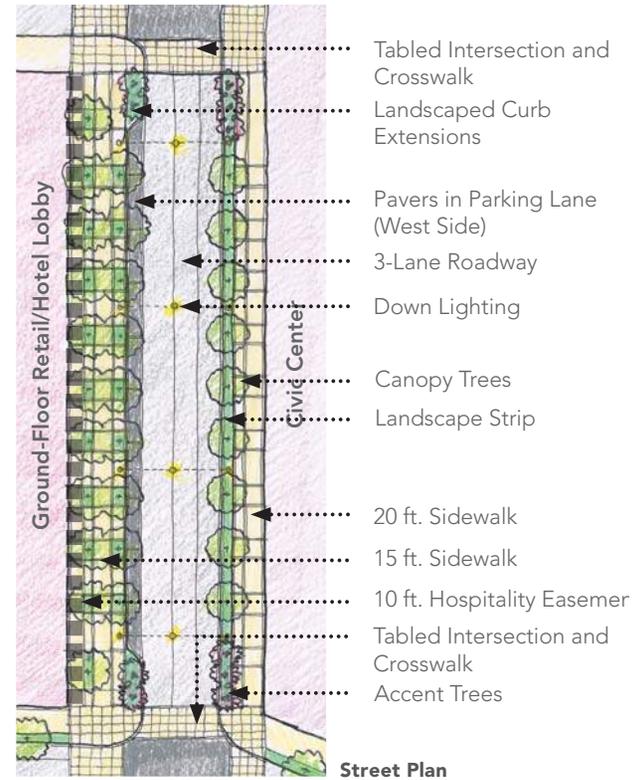
STANDARD SECTION ELEMENTS	
Traffic:	Two-way; two 12-ft. travel lanes, one 10' turn lane
Parking:	8-ft. parallel lane, each side of street
Sidewalk:	15 ft. wide, each side
Bicycle:	None
Intersections:	Tabled intersection or laid-down corners, & crosswalks (15' Wide Min.)
Curbs:	1-ft. concrete
Paving:	Scored concrete, tinted concrete, accent pavers and asphalt
Trees:	Canopy and accent
Landscape:	Shrubs and groundcover in 4-ft. tree wells and curb extensions
Lighting:	Roadway and sidewalk
Furnishings:	Benches, trash receptacles, bike racks, & tree grates



FIFTH STREET SIGNATURE STREET REQUIREMENTS

The standard Fifth Street Signature Street section represents the preferred design for the intended character and function of Fifth Street as a retail, entertainment, and Civic Center emphasis street south of Main Avenue and a well greened pedestrian emphasis street north of Main Avenue.

STANDARD SECTION ELEMENTS	
Traffic:	Two-way; two 12-ft. travel lanes, one 13' turn lane
Parking:	8-ft. parallel lane, west side of street
Sidewalk:	15 ft. wide, west side 20' east side (Includes 15' sidewalk and 5' furniture/landscape zone) 10' Hospitality Easement, west side
Bicycle:	None
Intersections:	Tabled intersection or laid-down corners, & crosswalks (Min 15')
Curbs:	1-ft. concrete
Paving:	Scored concrete, tinted concrete, accent pavers and asphalt
Trees:	Canopy trees and accent
Landscape:	Shrubs and groundcover in 4-ft. tree wells, landscape strip & curb extensions
Lighting:	Roadway and sidewalk
Furnishings:	Benches, trash receptacles, bike racks, & tree grates



FIFTH STREET SIGNATURE STREET VARIATIONS

Fifth Street Signature Street variations from the Standard Section south of Front Avenue include the following.

- Festival Street- South of Bowen Avenue to Kirkwood Mall
- Fifth Street Promenade- North of Main Avenue to the Library

Festival Street—Bowen Avenue to Ingals Avenue

The Festival Street provides a distinct character to the nightlife zone by incorporating a flexible street design that allows for daily auto access to shops but is also dedicated for street events with the ability for temporary street closures.

Street elements:

- A curbless, brick paved surface from building edge to building edge
- Wide sidewalks (Minimum 18' each side)
- A two-lane roadway, one lane in each direction (12' lanes)
- Curbside parking both sides of street (8' parking lane)
- Curbside landscaping and pedestrian scaled lighting
- Curb extensions and enhanced crossings at intersections
- Decorative string lighting over the sidewalk and roadway

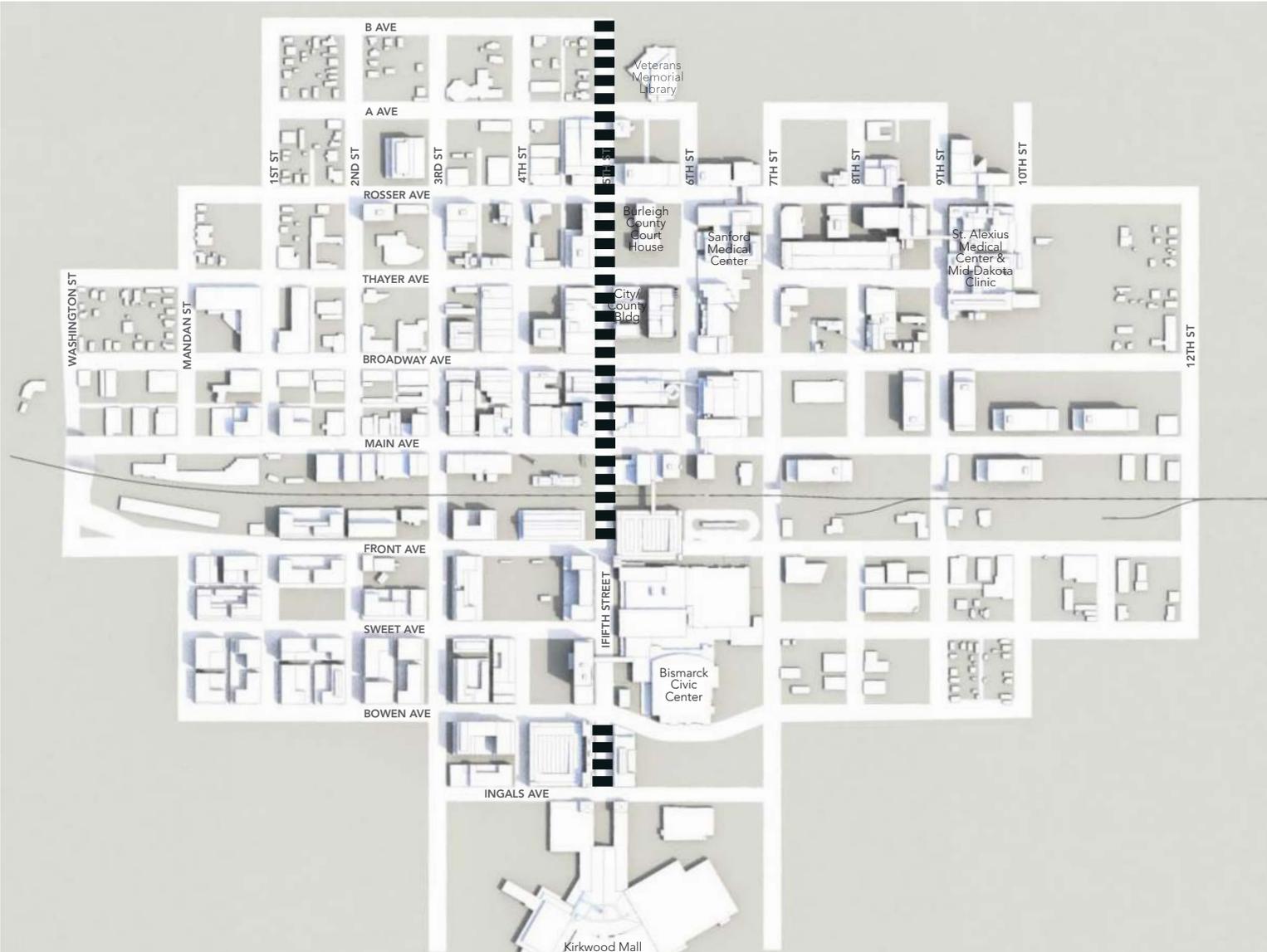
Fifth Street—Front Avenue to Avenue A

Fifth Street north of Front Avenue is intended to incorporate a continuous pedestrian promenade with enhanced landscaping along the east side of the street and to maintain street trees and curbside parking on the west side. In areas where curbside parking serves retail uses on both sides of the street curbside parking would be provided.

Street elements:

- Wide sidewalks (Minimum 15' each side)
- Curbside landscaping (8' wide- east side)
- Street trees in large curbside tree wells (west side)
- A two-lane roadway, one lane in each direction (11' lanes)
- A continuous turn lane (12' lane)
- Curb extensions and enhanced crossings at intersections

Fifth Street Variations

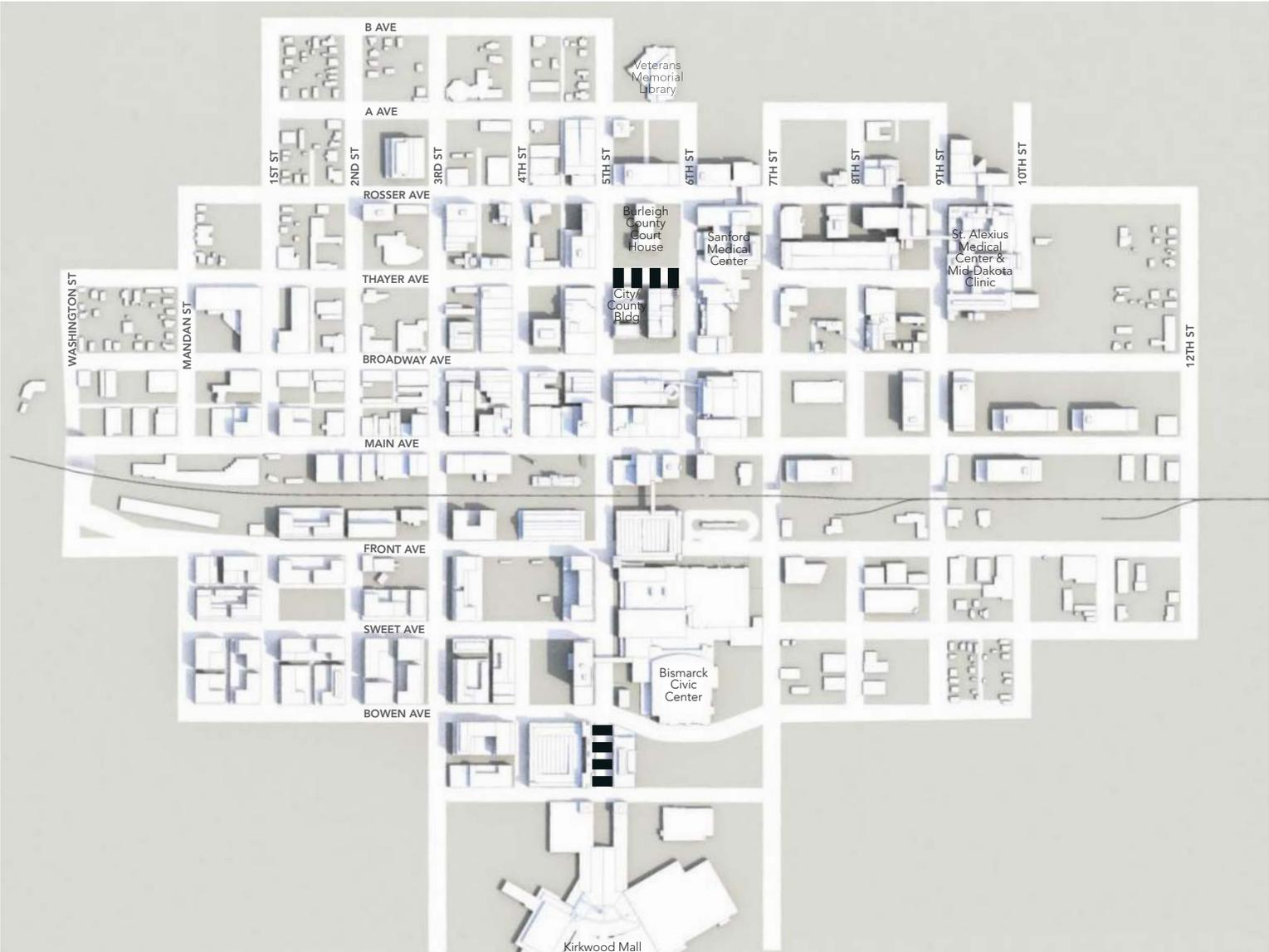


FESTIVAL STREET

These streets are designed to promote a sense of community in that they provide an approved public right-of-way location that can be closed to traffic on multiple occasions during the year for pedestrian-focused special events. They are strategically located to serve the Mehus Commons area and the retail, entertainment, and Civic Center uses along Fifth Street.

- **Thayer Avenue (5th to 6th Streets)**– A curbless street design accommodating temporary street closure for events and providing a complementary and supporting space to the Mehus Commons and Burleigh Mall (Alleyway)
- **Fifth Street (Bowen Avenue to Ingals Avenue)**– A curbless street design accommodating temporary street closure for Fifth Street retail business/ community events or activities associated with the Civic Center

Festival Street

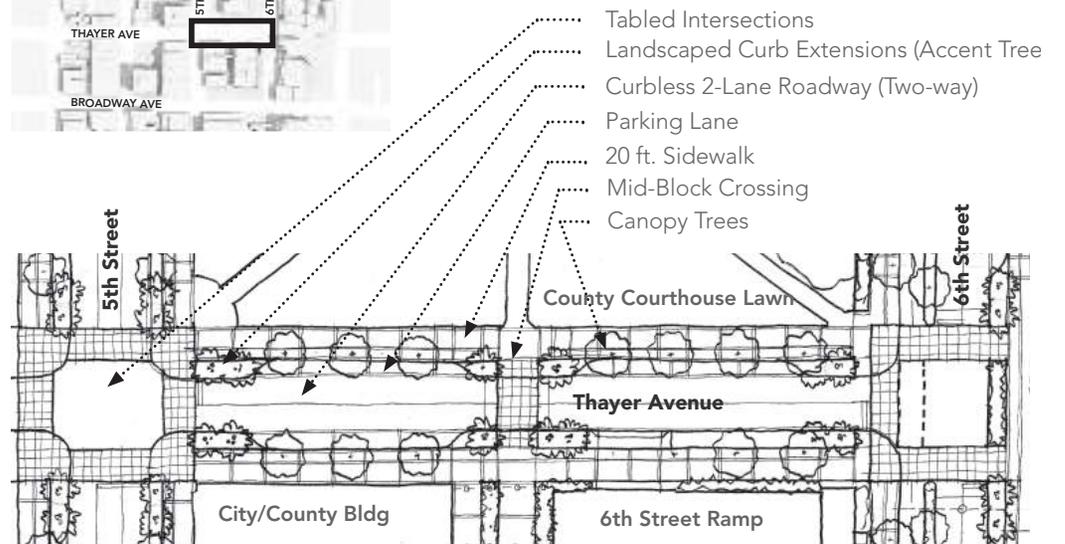


THAYER AVENUE FESTIVAL STREET REQUIREMENTS

The standard Thayer Avenue Festival Street section represents the preferred design for the intended character and function of Thayer Avenue between 5th and 6th Streets as a complementary space to the Mehus Commons and Burleigh Mall (Alleyway) that supports temporary street closure for events and provides a high quality pedestrian environment linking major employment and government uses in the downtown.



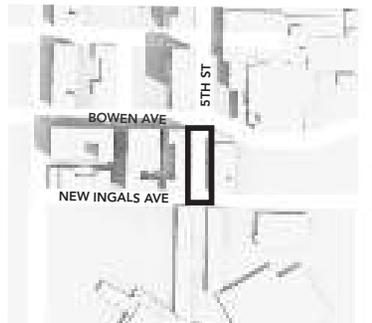
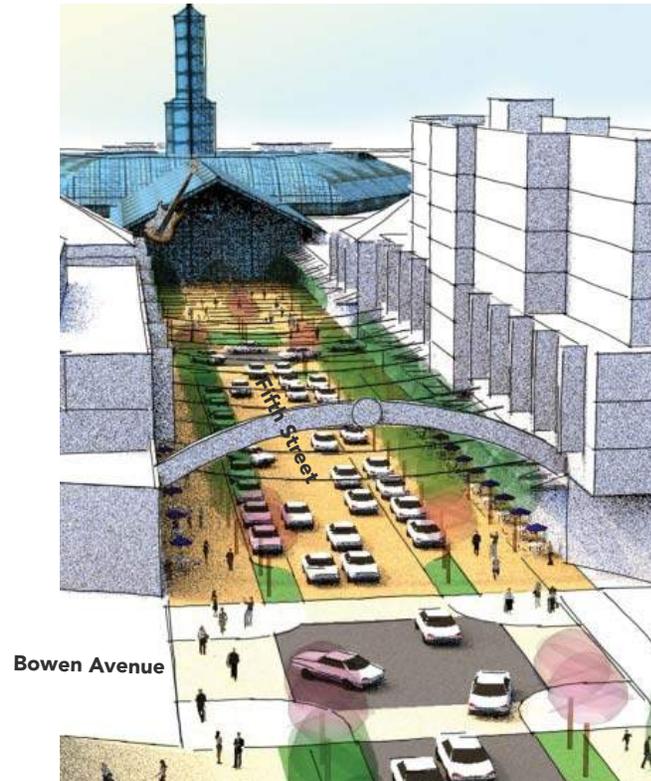
STANDARD SECTION ELEMENTS	
Traffic:	Two-way; two 12-ft. travel lanes.
Parking:	8-ft. curbside lane
Sidewalk:	20 ft. wide
Intersections:	Tabled intersection or laid-down corners, crosswalks
Curbs:	Curbless w/ bollards
Paving:	Brick, Scored concrete, or accent pavers
Trees:	Canopy and accent
Landscape:	Shrubs and groundcover in 5-ft. tree wells and curb extensions
Lighting:	Roadway, sidewalk, overhead string lights
Furnishings:	Benches, trash receptacles, bike racks



FIFTH STREET FESTIVAL STREET REQUIREMENTS

The standard Fifth Street Festival Street section represents the preferred design for the intended character and function of Fifth Street between Bowen Avenue and Ingals Avenue to incorporate a curbless street design to support temporary street closure in association with adjacent business, community or Civic Center events.

STANDARD SECTION ELEMENTS	
Traffic:	Two-way; two 11-ft. travel lanes. 12-ft turn lane
Parking:	8-ft. curbside lane
Sidewalk:	15 ft. wide
Intersections:	Tabled intersection or laid-down corners, crosswalks
Curbs:	Curbless w/ bollards
Paving:	Brick, Scored concrete, or accent pavers
Trees:	Canopy and accent
Landscape:	Shrubs and groundcover in 5-ft. tree wells and curb extensions
Lighting:	Roadway, sidewalk, overhead string lights
Furnishings:	Benches, trash receptacles, bike racks



PEDESTRIAN EMPHASIS STREET

The Pedestrian Emphasis Streets essentially function as every day routes for pedestrians and local traffic within the downtown. The design of the street is specific to the adjacent land uses and two types exist today that include:

- **Residential Character**—routes outside of the core commercial area of downtown and between commercial and residential use transition areas within predominately residential neighborhoods.
- **Commercial Character**— routes within the core commercial area of downtown and between commercial and residential use transition areas

City street standards exist for these streets and should continue to provide guidance for future improvements or extensions of existing streets or new streets within the Subarea.

Pedestrian Emphasis Street

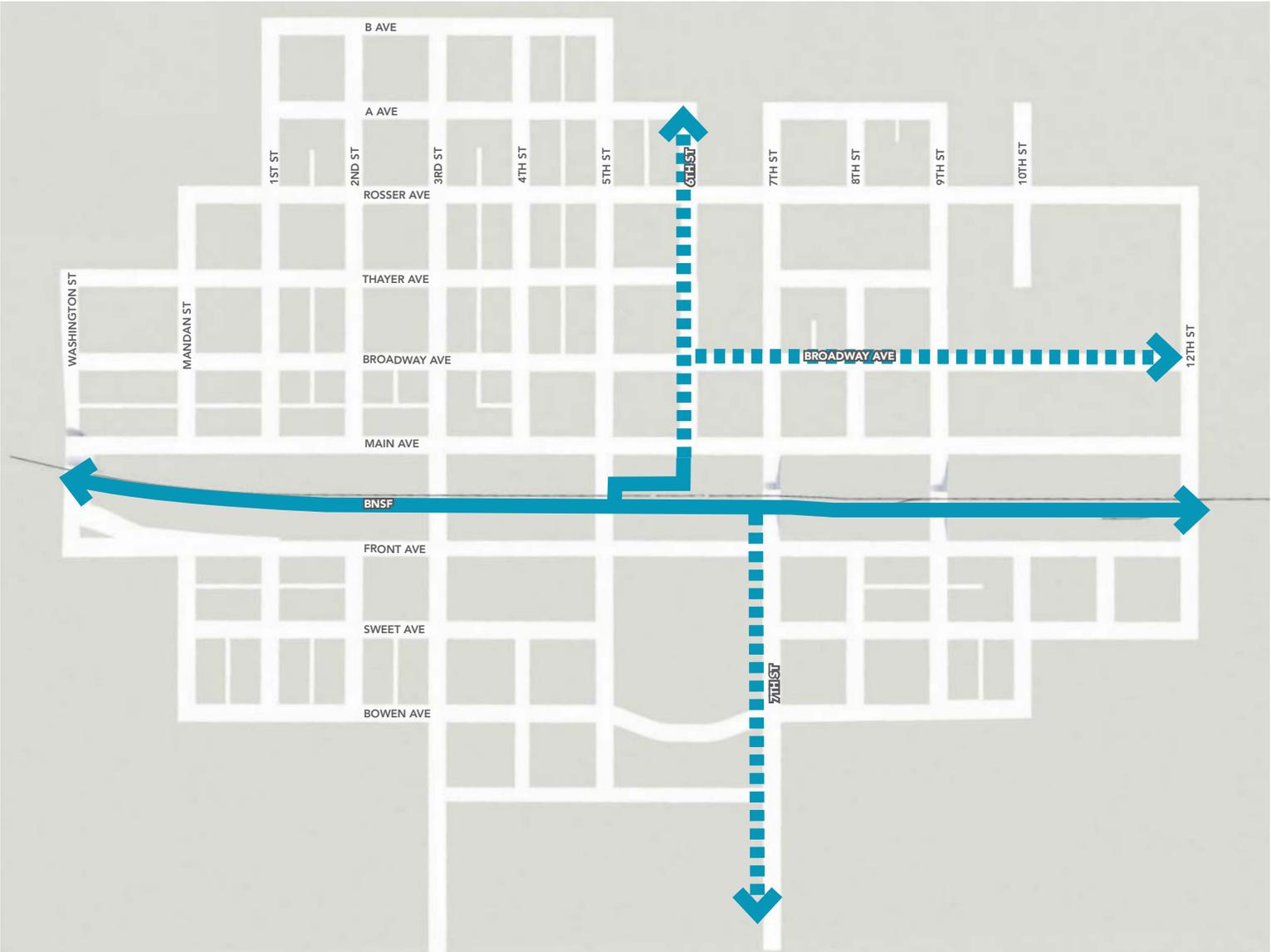


CULTURAL TRAIL

The Cultural Trail—6th Street, Broadway Avenue, adjacent to the BNSF rail line and Seventh Street— is the backbone of an active transportation network that connects neighborhoods and attractions to downtown beyond a five minute walk by way of a five minute bike ride.

- **Protected Bikeway**—the standard for progressive bike communities, this facility has a physical separation from bicycle and traffic by way of landscaping, extruded curbs or parked cars along 6th Street connecting the Downtown to the State Capitol, and Broadway Avenue east of Sixth Street
- **Multi-Purpose Trail**— a mixed pedestrian and bicycles facility adjacent to the BNSF rail line and connecting to existing off-street bike facilities along Front Avenue to the Missouri River and located off-street on the west side of Seventh Street linking to the Bismarck Expressway, and University Drive/Airport Expressway trails south to adjacent neighborhoods, the airport and the University of Mary

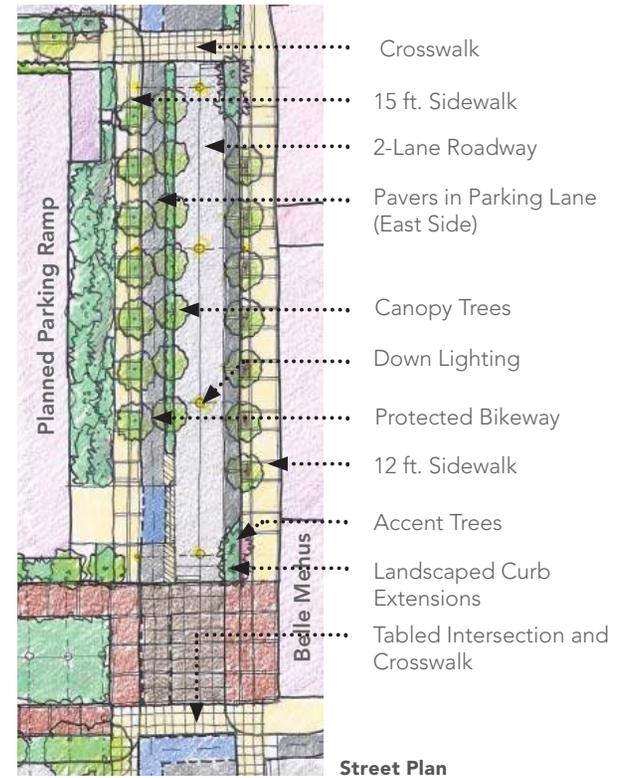
Cultural Trail



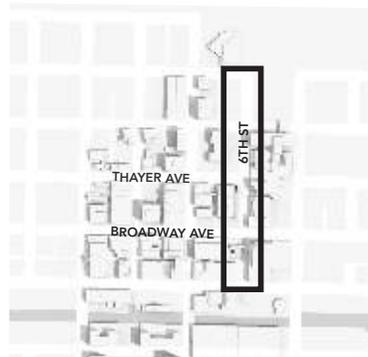
SIXTH STREET CULTURAL TRAIL REQUIREMENTS

The standard Sixth Street Cultural Trail section represents the preferred design for the intended character and function of Sixth Street as a protected bikeway route linking the Downtown to the State Capitol. This design would also be applicable to Broadway Avenue east of Sixth Street linking neighborhoods and the Sanford and St Alexius medical centers.

STANDARD SECTION ELEMENTS	
Traffic:	Two-way; two 12-ft. travel lanes
Parking:	8-ft. parallel lane, east side of street
Sidewalk:	12 ft. wide, west side 19 ft. wide, east side (15-ft. sidewalk & 4' furniture zone),
Bicycle:	Two-way 12-ft. protected bike lanes, west side
Intersections:	Tabled intersection or laid-down corners, crosswalks (15' Wide Min.), & bike boxes
Curbs:	1-ft. concrete
Paving:	Scored concrete, tinted concrete, accent pavers and asphalt
Trees:	Canopy and accent
Landscape:	Shrubs and groundcover in 5-ft. tree wells and curb extensions
Lighting:	Roadway and sidewalk
Furnishings:	Benches, trash receptacles, bike racks



Street Plan



RAIL TRAIL REQUIREMENTS

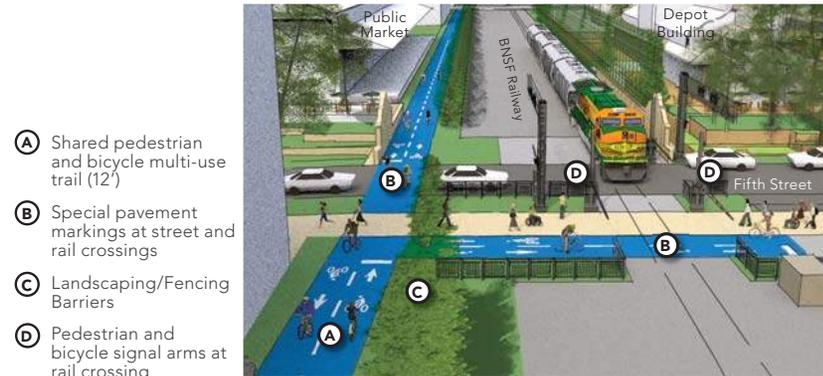
The standard Rail Trail section represents the preferred design for the intended character and function of the rail trail linking the Downtown to the Sertoma Park Trail Loop, Riverfront Trail and the Missouri River. The rail trail is located adjacent to the southerly edge of the BNSF right of way. The trail is envisioned to extend from Seventh Street to the existing paved trail west of Washington Street.

STANDARD SECTION ELEMENTS	
Multi-Purpose Trail	Pedestrian and Bicycle-Min. 15'
Intersections:	Ladder Crossing
Paving:	Scored concrete, or asphalt
Trees:	Columnar evergreen and accent
Landscape:	Shrubs and groundcover in buffer between trail and buildings and/or railway
Lighting:	Roadway and sidewalk
Furnishings:	Benches, trash receptacles, & fencing

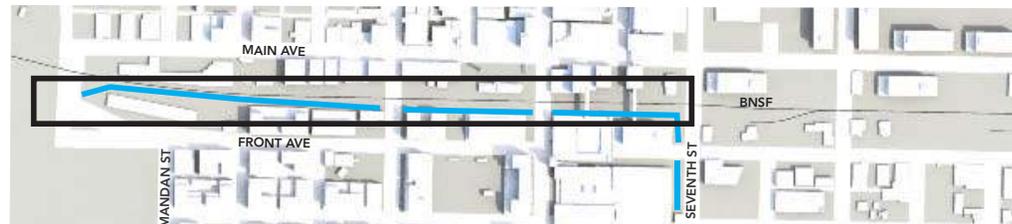
RAIL TRAIL EXAMPLE



RAIL TRAIL- VIEW LOOKING



- (A) Shared pedestrian and bicycle multi-use trail (12')
- (B) Special pavement markings at street and rail crossings
- (C) Landscaping/Fencing Barriers
- (D) Pedestrian and bicycle signal arms at rail crossing



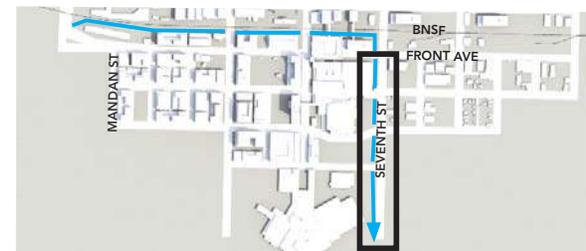
TYPICAL MULTI-PURPOSE REQUIREMENTS

The typical Multi-Purpose Trail section represents the preferred design for the intended character and function of the Cultural Trail (along the west side of Seventh Street from Front Ave to the Bismarck Expressway Trail) linking the Downtown to the University Drive Trail and destinations further south such as the airport and University of Mary. This design includes much of the elements of the existing trails along University Drive and the Bismarck Expressway.

STANDARD SECTION ELEMENTS	
Multi-Purpose Trail	Pedestrian and Bicycle-Min. 15'
Intersections:	Ladder Crossing
Paving:	Scored concrete, or asphalt
Trees:	Canopy and accent
Landscape:	Shrubs and groundcover in buffer between trail and buildings and/or roadway
Lighting:	Roadway and sidewalk
Furnishings:	Benches, trash receptacles, & fencing



- (A) Shared pedestrian and bicycle multi-use trail (15' min.)
- (B) Special pavement markings at street and rail crossings
- (C) Landscaped Buffer between Trail and Roadway

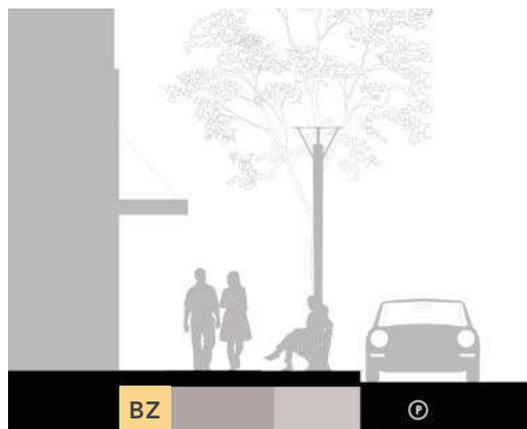


2. DESIGN ELEMENTS

The design elements prescribe specific design and specifications of all roadways and sidewalks for each street type. The design elements include the following:

- Sidewalk and Roadway Zones— identify the character and arrangement of ‘functional zones’ within sidewalks and roadway lanes. Provide detailed dimensioned layouts (plans and sections) for all elements within each sidewalk zone and roadway lane
- Sidewalk and Roadway Details— provide plans and construction design details for all key elements within the right-of-way
- Street Furniture Elements—identify the type, location, and material specifications for lighting, benches, bicycle racks, landscape planting lists, etc.

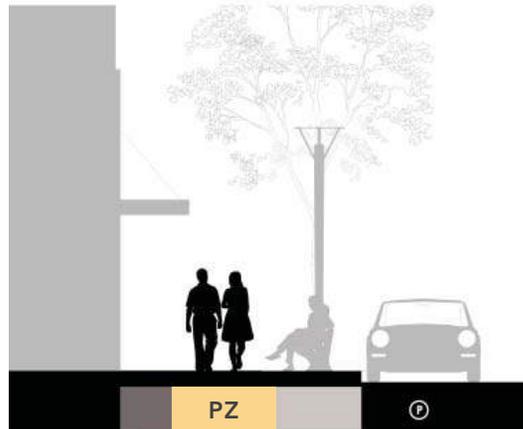
Sidewalk and Roadway Zones Example



Building Zone (BZ)

The area immediately adjacent to the building facade. Due to its limited area, this zone accommodates retail displays, small two-person cafe tables/seating, and minor building projections such as downspouts. This zone is interrupted at doorways, corners and driveways.

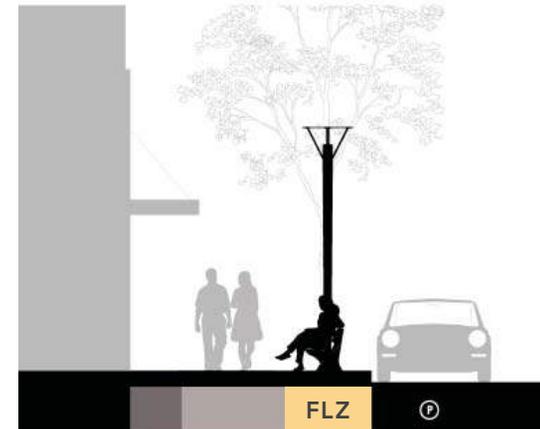
Size: 3 feet



Pedestrian Zone (PZ)

An unobstructed area for pedestrian through-movement, free of cafe seating, sandwich boards, signal poles, utility poles, etc. This zone should not be interrupted by driveways on the Retail Street.

Size: 6 to 7 feet

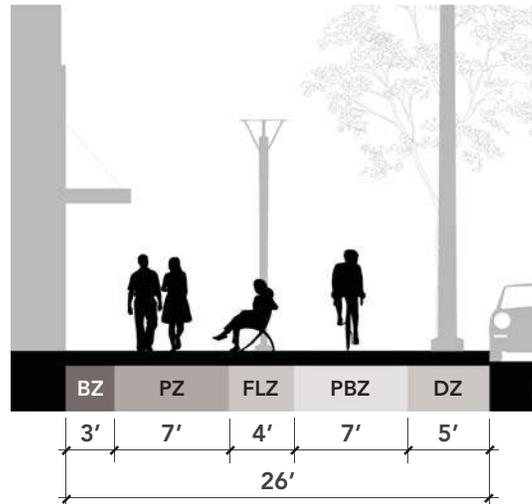


Furnishings/Landscape Zone (FLZ)

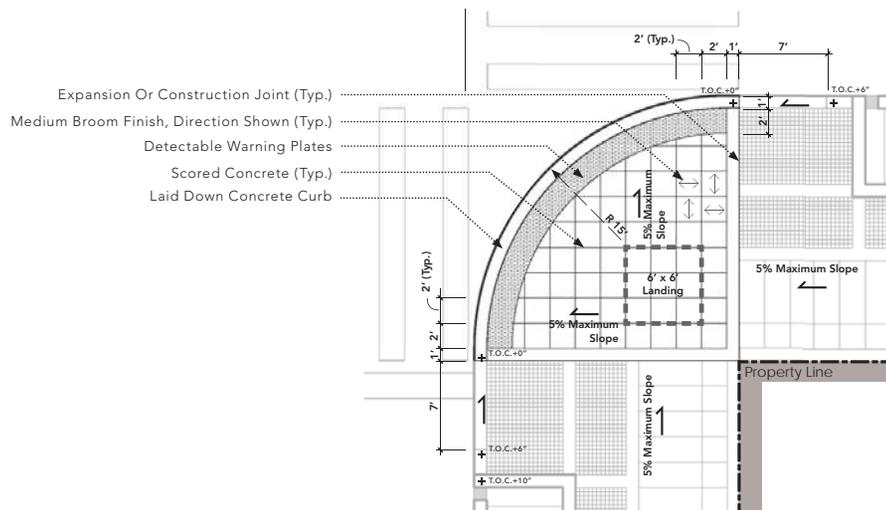
This is the location of all elements supporting pedestrian, vehicular and bicycle use of the right-of-way, including sign poles, lighting, street furniture, seating corrals, landscape, rain gardens and bicycle parking facilities. This zone is interrupted at corners and driveways.

Size: 4 to 8 feet (Min. 5 feet at curb side)

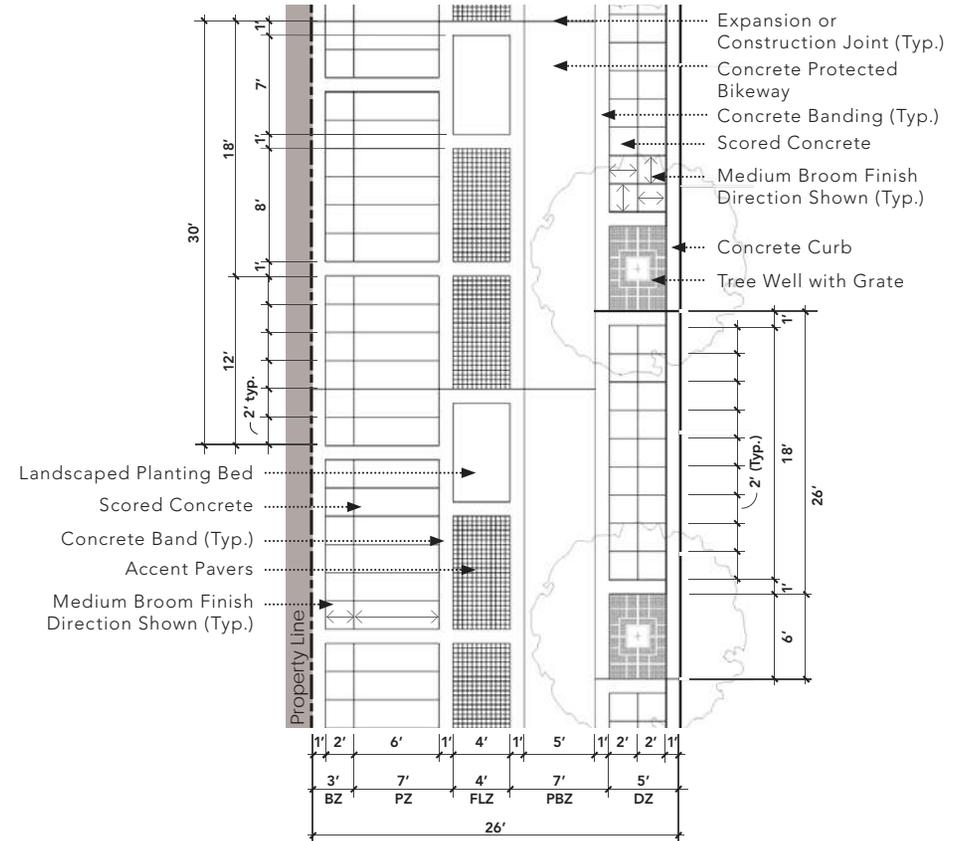
Sidewalk and Roadway Details Example



Zones

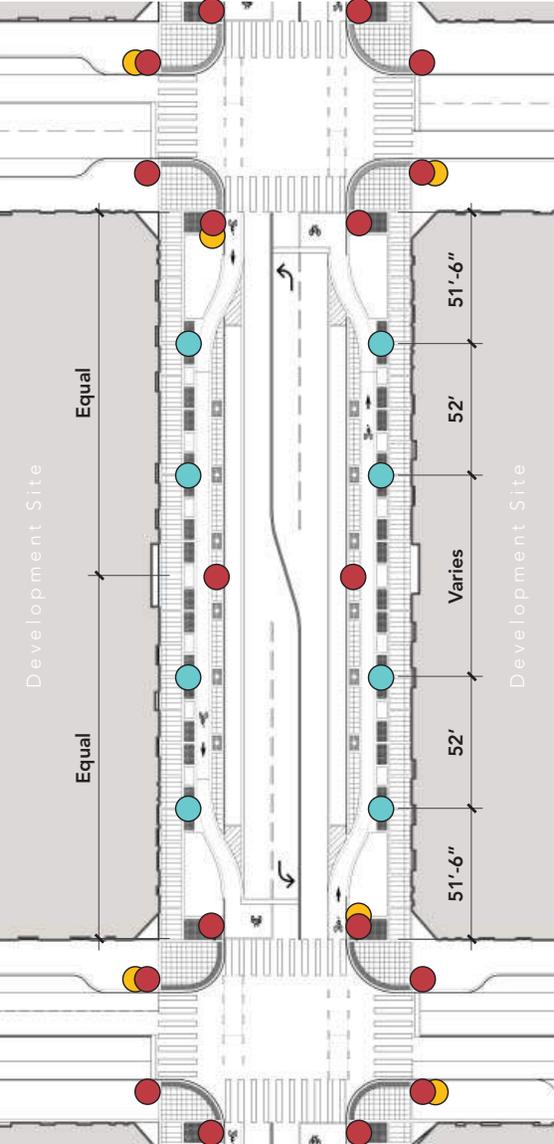


Typical Corner Plan



Paving Plan

Street Furniture Elements Example



Sidewalk Lighting

- **Manufacturer**–Kim Lighting
- **Model**–Bounce, single luminaire, 10-ft. pole height
- **Finish**–Platinum Silver

- Roadway Light Fixture
- Sidewalk Light Fixture
- Potential Traffic Signal Fixture

VI. MANAGING IMPLEMENTATION

The implementation of the Downtown Subarea Plan can occur through passive measures- those that rely on the regulatory parameters guiding Plan development, or active measures- those using a systematic approach that includes the regulatory framework, city departments and agencies coordination, public and private partnerships, integrated funding strategies and a central managing entity to coordinate implementation efforts.

While planning within the context of the Downtown Subarea Plan is considered a longer term effort, implementation will not occur without some short-term strategic actions that lay the foundation for future success. The short term strategic actions include plan and implementation strategy adoption and the update to existing codes and ordinances that establish the Plan’s regulatory framework as discussed in the previous section.

City Commission adoption of the plan and implementation strategy provides the authority to create the regulatory framework needed to ensure that the intent of the Plan is realized and establishes the directive to City departments for integrating the Plan’s recommendations into departmental work plans.

A Recommended Comprehensive Implementation Approach

By taking a more active approach, the City can maximize the development potential in a coordinated fashion benefitting both existing and future residents and businesses and ensuring the efficient use of public dollars to leverage significant private development.

It is recommended that the city consider the following active implementing measures that include:

The Regulatory Framework

Establishing the regulatory framework in the form of codes and ordinances is the most time sensitive measure to be instituted to ensure that future development meets the intent of the Plan and creates a level playing field by providing certainty to the development community. The first task in terms of implementing the Plan will be to establish the regulatory framework which consists of updating zoning codes, plans and standards.

Interagency Coordination

Interagency coordination will be required to ensure that citywide planning across agencies and departments are working together to implement the Downtown Bismarck Subarea Plan. For instance, the timing of this planning effort will allow for the Plan to inform the final update of the growth management plan as well as the upcoming update to the long range transportation plan..

The implications for coordinating with the LRTP update provide the opportunity to guide the future location and design of streets and identify priorities and funding for public investment in citywide transportation projects that support Plan implementation. In addition, interagency coordination should look for opportunities to include the Plan’s implementing measures and goals within existing city department work plans.

Partnerships

Partnerships provide the relationship building that expand the capacity of a local community to effectively fund and implement planning efforts and are critical for bridging the gap in funding public and private projects. Partnerships are likely to be in the form of:

- Public—The city, MPO and NDDOT are currently involved as partners in the planning for the Downtown Subarea Plan and LRTP
- Public/Private—The majority of specific area planning projects within the Plan introduce development products that have not been readily assembled in the local community, such as mixed-used housing development, and within the restricted current economic environment will require a coordinated effort between developers and the public sector to navigate the process to mutually benefit all parties involved.
- Public/Non-Profit— Affordable housing, services for the elderly, and public spaces, which add to the livability of downtown and ensure safe and equitable access to housing, services and open spaces are often leveraged and implemented through partnerships with non-profits.

Development Incentives

Development incentives are intended to reduce the barriers to private development and local business creation and are a tool for ensuring high quality development.

Development incentives are most successful when directed toward specific community goals such as the creation of affordable housing, encouraging mixed-use development, improving community access and mobility, and job creation. Incentives encourage private development and shared contributions for public infrastructure such as streets, utilities, affordable housing and parks. Incentives may come in the form of:

- Development bonus programs such as height or density bonuses
- Reduced fees for system development charges
- Streamlined development approvals to reduce the amount of time and cost associated with the plan approval process
- Tax abatement
- Direct loan programs

Implementation Oversight

Coordination of public and private sector efforts is absolutely essential for navigating the complexities of Plan implementation and for ensuring that implementation is effective and timely. A central agency or point person provides the institutional capacity to manage and coordinate, funding, partnerships and marketing for downtown development.

It is recommended that a single point-of-contact staff position be created to coordinate and manage these projects through implementation in conjunction with an Implementation Oversight Committee. As an oversight body, it is recommended that willing members of the Downtown Subarea Steering Committee and Technical Advisory Committee be transformed into an 'Implementation Oversight Committee'. This committee would review proposals and provide recommendations to the Bismarck City Commission for any expenditure of public resources throughout the life of the implementation plan.

The Implementation Oversight Committee should:

- Be formally appointed by the Bismarck Board of City Commissioners
- Receive support from City staff regarding necessary data and information needs, scheduling of regular meetings and preparation of an annual report and briefing materials
- Meet at regular intervals throughout the life of the Subarea Plan implementation to review and provide recommendations to the City and Commissioners on implementation progress
- Prepare an annual report and briefing to City Commissioners

The make-up, and purpose of an implementation committee is as follows:

Committee Members

The Implementation Oversight Committee should include a diverse and representative group of downtown members including property and business owners, representatives of downtown business associations and advocacy groups, and City departments, agencies, and commissions. Oversight Committee members might be representatives of:

- Downtown Business Improvement District
- Renaissance Zone Authority
- Downtown Business Association
- City Transportation, Planning, Public Works, and Engineering
- Housing Authority and Parking Authority
- Transit Agency
- City Commission and Planning and Zoning Commission
- Downtown Business Owner
- Property Owner

Committee Purpose

The Implementation Oversight Committee should meet regularly to review implementation progress, and identify opportunities for advancing implementation efforts. Tasks of the Oversight Committee should include:

- Meeting monthly
- Keeping the Master Plan ‘Alive and Breathing’
- Tackling topics and components of the Master Plan, setting goals, finding community members to engage in goals, advocating for goals, and creating committees to achieve goals
- Seeing this plan through
- Recognizing that City support is essential for plan success
- Recognizing that accomplishments of the Master Plan are the result of a community-wide effort

Committee Annual Report

The Implementation Oversight Committee should prepare an annual report. An annual report and briefing to City Commission identifies implementation progress and provides a basis for establishing partnerships and prioritization of City funds for implementation projects. The annual report should include:

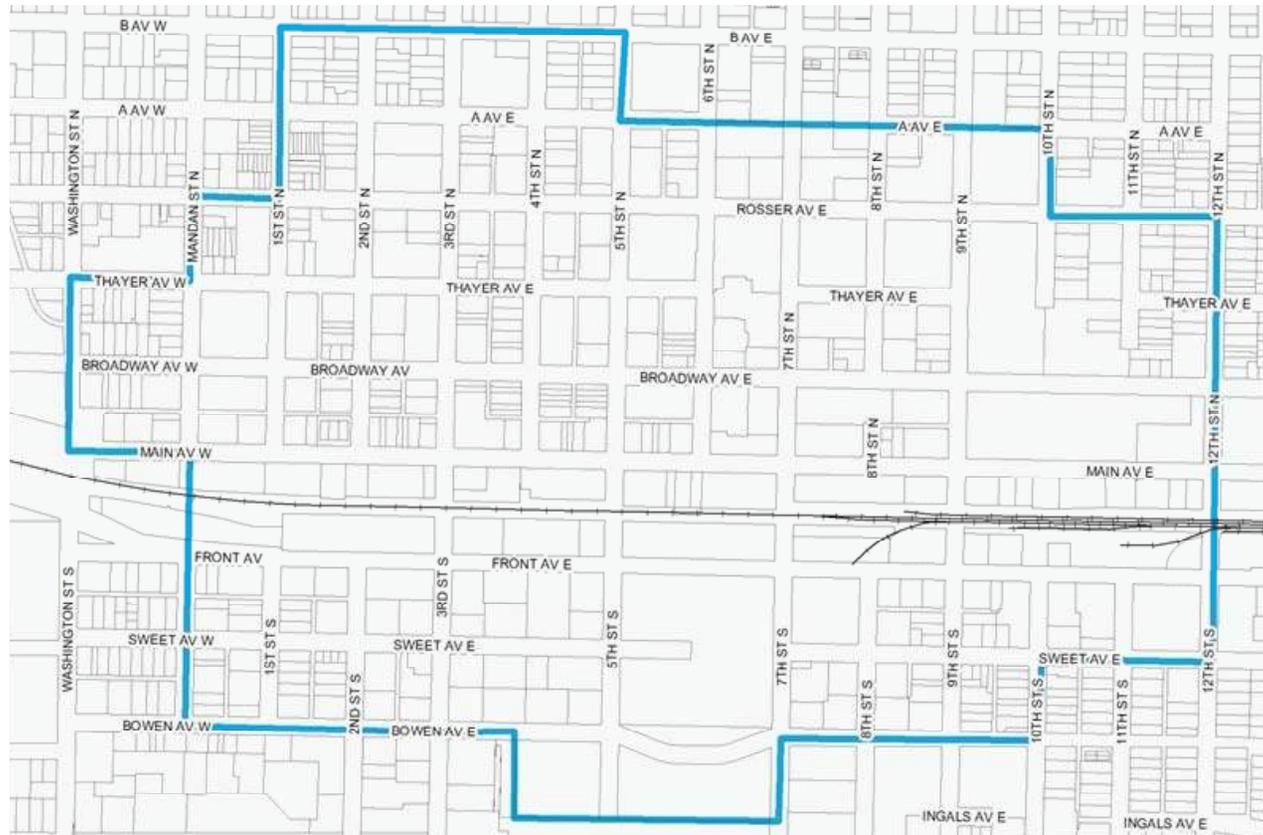
The annual report should include:

- An update on accomplishments for the last year
- An updated list of Master Plan accomplishments since the start of the plan
- The committee’s immediate goals for the near future

VII. FINANCING

A primary source of funding for downtown projects is tax increment financing available throughout the study area. The current reserve of TIF funds are estimated at approximately \$9 million. Two committed downtown projects- the 6th Street Ramp and Quiet Rail at-grade crossing improvements will require an estimated \$17 million. These projects have the potential to consume all or a portion of the available TIF funds. TIF still provides a viable source for leveraging funding for the Bismarck Subarea Plan game changing and essential projects. These projects should be identified as priority projects for financing either fully or in part through tax increment financing. Additional funding sources available to the city are identified on the following pages.

Urban Renewal District (Tax Increment Finance District and CORE) Boundary



FUNDING SOURCES

Downtown revitalization is a multi-year task, and success is enhanced by a multi-year commitment of funding and incentive programs. There are many financing tools which will be available to downtown Bismarck to implement the projects identified in the Downtown Bismarck Subarea Plan.

The following are a list of possible federal, state, and local funding sources for transportation and infrastructure, housing, and economic development projects.

I. STATE and LOCAL PROGRAMS

The City of Bismarck has a variety of fiscal resources to assist in the implementation of downtown projects. The methods used to finance infrastructure and other public improvements can influence how development and growth occur within the City. Property taxes, special assessments, sales tax, enterprise funds, community development block grants, and state and federal aid programs are some revenue resources available. These resources can be applied on a case-by-case basis to help achieve the goals and objectives of the Subarea Study.

Renaissance Zone Program

A state initiative that began in 1999, the Renaissance Zone program provides a five year property tax exemption and a five year state business income tax exemption. Business and property owners can qualify for the tax exemptions by making capital improvements to their lease space, buildings or by constructing new buildings within the Zone. There are no cash incentives or grants available with this program.

CORE Incentive Program

The CORE Program was created to stimulate new development in the downtown area and fund projects that will serve as the foundation for future downtown development. These incentive programs are funded by the Downtown Tax Increment District and are available to property owners, developers and tenants for projects within that District. All CORE Incentive Program grants and awards are discretionary and will be considered on a case-by-case basis. The funds include:

1. Façade Grant program provides 50% matching funds made to building facades
2. Housing Incentive Grant program provides 20% matching grants for the creation or rehabilitation of housing units within the downtown area
3. Technical Assistance Bank offers financial assistance for professional design services from licensed architects and engineers
4. Sidewalk Subsurface Infill program offers grants for work within the public right-of-way needed to fill subsurface vaults under sidewalks.

Special Assessments

Special assessments are used to levy the costs of specific improvements against the properties that directly benefit from the improvements. Special assessments are traditionally used for infrastructure improvements, including roadways, sanitary sewer, storm sewer, and water mains.

In new subdivisions, the developer is responsible for paying trunk line charges, right-of-way grading, basic water and sanitary sewer main costs up front, with the remainder of the improvements generally being assessed to the individual lots within the subdivision. The North Dakota Century Code also allows the use of special assessments to develop public parks.

Property Taxes

Property taxes are a primary revenue source for local government. Tax rates apply uniformly to all property within the City regardless of what the property creates in terms of costs or benefits within the community. For this reason, property taxes are usually used to fund City operations.

Sales Tax

As a home rule municipality, Bismarck has the authority to utilize sales tax to fund specific projects and reduce property taxes. This 1% sales tax is currently used to fund roadway construction and maintenance of City facilities, economic development activities, and to offset property taxes by an equivalent of 25 mills. Sales tax is also used to fund community betterment projects, which are voted on by the residents of Bismarck. The most recent vote in 2002 authorized funding for several project that would not otherwise have funding, such as the construction of a new fire station, construction of the new 911 emergency center, development of new recreational trails, and improvements to the Community Bowl.

Enterprise Funds

The City also has several enterprise funds, which are self-supporting and function much like a business in that fees are charged for services in order to cover the cost of operation and equipment replacement. Enterprise funds within the City include the airport and flightline operations, the solid waste utility (collection and landfill), the water and sewer utility (water treatment & distribution, wastewater collection & treatment, and stormwater management facilities), and the parking authority operation.

Special Revenue Funds

Several operations of the City also operate as special funds, including the arena and exhibit functions of the Civic Center, the public library, specific function of the roads and streets department (snow gates and street lighting/traffic signals), and specific law enforcement activities. The lodging and restaurant tax used for capital construction and government grants are also budgeted as special revenue funds. The City of Bismarck is an entitlement community for Community Development Block Grants. These funds must be used for projects that benefit low and moderate income households, eliminate slums and blight, or mitigate a life-threatening situation. In Bismarck, CDBG funds may be utilized for infrastructure improvements in low and moderate income neighborhoods. The City is also eligible to apply for a variety of state and federal aid programs including funding for transportation plans, airport improvements, stormwater management studies, and qualified capital improvements.

Development Impact Fees

Although development impact fees are not utilized extensively in Bismarck, they should be considered. Developers would set aside funds to be used as a portion of required future infrastructure (such as roadways, traffic signals, stormwater management facilities, etc) benefiting the property being developed. This would help eliminate the need for general taxpayer subsidy of improvements that primarily benefit specific properties.

Legislative Direct Appropriations

State appropriation bills may include funding for particular local or state transportation projects.

Special Districts for Business Improvement, Parking & Other Infrastructure

Neighborhood residents and downtown property owners can elect to levy special taxes on themselves for special activities and capital improvements within an established special district. Business Improvement Districts, for example, can fund downtown revitalization activities, promotions and events. Parking Districts can assist development efforts in locating parking facilities in a manner consistent with good community design and respectful of the historic streetscape. Cities and counties may also create special improvement districts to pay for projects with bond repayment to be made by the adjoining landowners receiving the benefit of the improvement.

General Obligation Bonds

The sale of general obligation bonds used to finance specific public infrastructure and facilities improvements. A G.O. bond sale, subject to voter approval, can provide the financing initially required for major projects.

North Dakota Department of Transportation (NDDOT)

The NDDOT distributes a variety of federal funds and provides state matching funds requirements for a variety of auto, pedestrian, bicycle and rail infrastructure projects.

North Dakota Department of Transportation- Transportation Alternatives Program (TAP)

The current transportation bill, Moving Ahead for Progress in the 21st Century Act (MAP-21) establishes a new program to provide for a variety of alternative transportation projects, including many that were previously eligible activities under separately funded programs. TAP provides funding for programs and projects defined as transportation alternatives. They include pedestrian and bicycle facilities; safe routes to school projects; safe routes for non-drivers; community improvement activities, and environmental mitigation projects. TAP funds are administered through the NDDOT to local jurisdictions. A 10-member NDDOT director's task force rates the applications and attaches dollar values. The projects are usually reviewed in December with notification of award coming in January.

City of Bismarck General Fund

This fund provides revenue for most major city functions like the administration of local government, and the departments of public services including police, fire, and public works.

Downtown Development Incentives Fund

Creation of a 501-C3 to receive private donations and philanthropic support for public projects and economic development initiatives can provide a significant amount of funds over a sufficiently long period of time to capitalize financing for private investment through such means as:

1. Revolving loan fund offering interest rate write downs
2. Matching funds for contributions to downtown projects
3. Direct incentives for targeted retailers - would provide a “negotiation fund” for target retailers or categories

The Bismarck Vision Fund

The Bismarck Vision Fund is a city sales tax supported fund that provides assistance to businesses that wish to locate or expand in Bismarck. The Bismarck Vision Fund can offer a flexible assortment of financing opportunities, such as interest buy downs, reduced interest loans, equity positions, grants, and other individually tailored financial incentives and exemptions.

In addition, North Dakota is the only state in the nation to own, control, and operate its own development bank. The capacity to arrange financing was a benefit when the bank was established in 1919, and it remains so today. The Bank of North Dakota (BND) and other state and local programs can offer an array of financing opportunities, such as interest buy downs, reduced interest loans, equity positions, and other individually tailored financial incentives and exemptions.

MATCH Program

The MATCH Program is designed to encourage and attract financially strong companies to North Dakota. The program is targeted to manufacturing, processing and value-added industries. Through this program, the BND will participate in loans to financially strong companies and provide interest rates at some of the lowest in the nation.

PACE Fund

The PACE Fund is designed to assist North Dakota communities in expanding their economic base by providing for new job development. This program has two major elements: (1) the participation by the BND with a local lender in a community based loan, and (2) the participation by the PACE Fund with the local community in reducing the borrower’s overall interest rate.

Business Development Loan Programs

The Business Development Loan Program through the BND is designed to assist new and existing businesses in obtaining loans that would have a higher degree of risk than would normally be acceptable to a lending institution.

North Dakota Development Fund

A North Dakota Economic Development and Finance fund designed to provide flexible “gap” financing through loan and equity investments to new or expanding primary-sector businesses in North Dakota.

Tax and permitting incentives

Property tax exemptions can be granted to new and expanding businesses; payment in lieu of property tax may be negotiated; sales and use tax exemption for building materials used to construct an agricultural commodity processing facility; no sales and use tax on machinery or equipment used in processing and manufacturing; no sales tax on computer and telecommunications equipment for a new primary sector business; 5-year corporate income tax exemption; no personal property or inventory taxes.

Strategic Partners

The Bismarck Mandan Development Association (BMDA) has relationships with local service providers. Partnering with the BMDA, many of these utility service providers will negotiate favorable terms, grants, and conditions for new or expanding companies.

Other Funding Types

Another method that may be available to Bismarck for implementation of projects is to structure a range of public and private incentives for downtown retail, business development, and affordable housing. These “indirect funding” mechanisms might typically include:

1. Granting bonus densities under the zoning code in exchange for provision of a public amenity or benefit
2. Special permitting for uses such as sidewalk cafes to animate street life
3. Allowing sponsorships of public space programming to encourage pedestrian activity
4. Below-market-rate land sales or ground leases
5. Fast track approval processes for downtown housing development to lower developer costs while at the same time providing a benefit at little or no cost to the city
6. Below-market-rate public bond financing to reduce the cost of capitol for designated development projects
7. Property tax deferrals in exchange for provision of public amenities

II. FEDERAL PROGRAMS

Community Development Block Grants (CDBG)

Federally-funded through the Department of Housing and Urban Development this grant program provides funding for housing, infrastructure improvements, and economic development and must serve the interests of low and moderate-income populations.

New Market Tax Credit Program (NMTC)

This federal program is intended to attract private-sector investment into qualifying low-income communities to help finance community development projects, stimulate economic opportunity and create jobs. The program offers federal tax credits for making private investments in qualified Community Development Entities (CDEs).

Federal Rehabilitation Tax Credits for Certified Historic Structures

Federal program in which a portion of the renovation investment in an historic building is credited back against federal income taxes, in exchange for certain federal (Department of Interior) renovation standards being followed.

Moving Ahead for Progress in the 21st Century Act (MAP-21)

Moving Ahead for Progress in the 21st Century Act (MAP-21) MAP-21 is a milestone for the U.S. economy and the Nation's surface transportation program. By transforming the policy and programmatic framework for investments to guide the system's growth and development, MAP-21 creates a streamlined and performance-based surface transportation program and builds on many of the highway, transit, bike, and pedestrian programs and policies established in 1991.

TIGER Grants

The Transportation Investment Generating Economic Recovery, or TIGER Discretionary Grant program, provides a unique opportunity for the U.S. Department of Transportation to invest in road, rail, transit and port projects that promise to achieve critical national objectives. Congress dedicated \$1.5 billion for TIGER I, \$600 million for TIGER II, \$526.944 million for FY 2011 and \$500 million for the FY 2012 round of TIGER Grants to fund projects that have a significant impact on the Nation, a region or a metropolitan area.

Low-Income Housing Tax Credits (LIHTC) and HOME Investment Partnership Program

Federal tax credits program created in 1986 under Section 42 of the IRS code to encourage the development of affordable multifamily rental housing.

Economic Development Administration- U.S Department of Commerce (EDA)

The EDA provides funds for technical assistance, planning and the development of projects that result in the creation of new employment. Technical assistance grants usually average about \$25,000 and require a small cash match. Capital grants and revolving loan funds are available on a 50/50 matching basis and vary in size from hundreds of thousands to millions of dollars.

Crandall Arambula PC



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