

phase 2

bismarck center



PHASE TWO: PRELIMINARY DESIGN FOR:

BISMARCK CENTER

architects/planners/environmentalists

the
hodne/stageberg
partners
inc.

116 east 22nd street 339-0561 (612)
minneapolis, minnesota 55404

URBAN RENEWAL PROJECT: N. D. /R-5
BISMARCK URBAN RENEWAL AGENCY
BISMARCK, NORTH DAKOTA

SEPTEMBER 1973

TRANSMITTAL

26 September 1973

Bismarck Urban Renewal Agency
107 East Bowen Avenue
Bismarck, North Dakota 58501

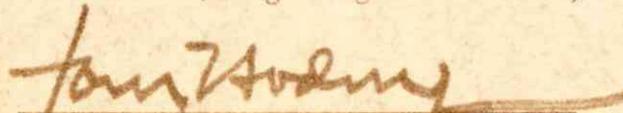
Ladies and Gentlemen:

It is our pleasure to present to you this report, Bismarck Center: Phase Two, which represents completion of our Preliminary Design work.

Your personal commitment, confidence and cooperation has been greatly appreciated, and no doubt has been instrumental in the positive community involvement, response, and approval. Hopefully, that community spirit can be marshalled further to effect implementation of this initial stage, and to extend in the future the physical downtown development to approach the objectives of the Concept Plan. As you know, the quest for a quality environment is a continual process, requiring dedication of effort toward a common goal. We stand ready to aid you in that effort to create a new and exciting "Bismarck Center".

Respectfully submitted,

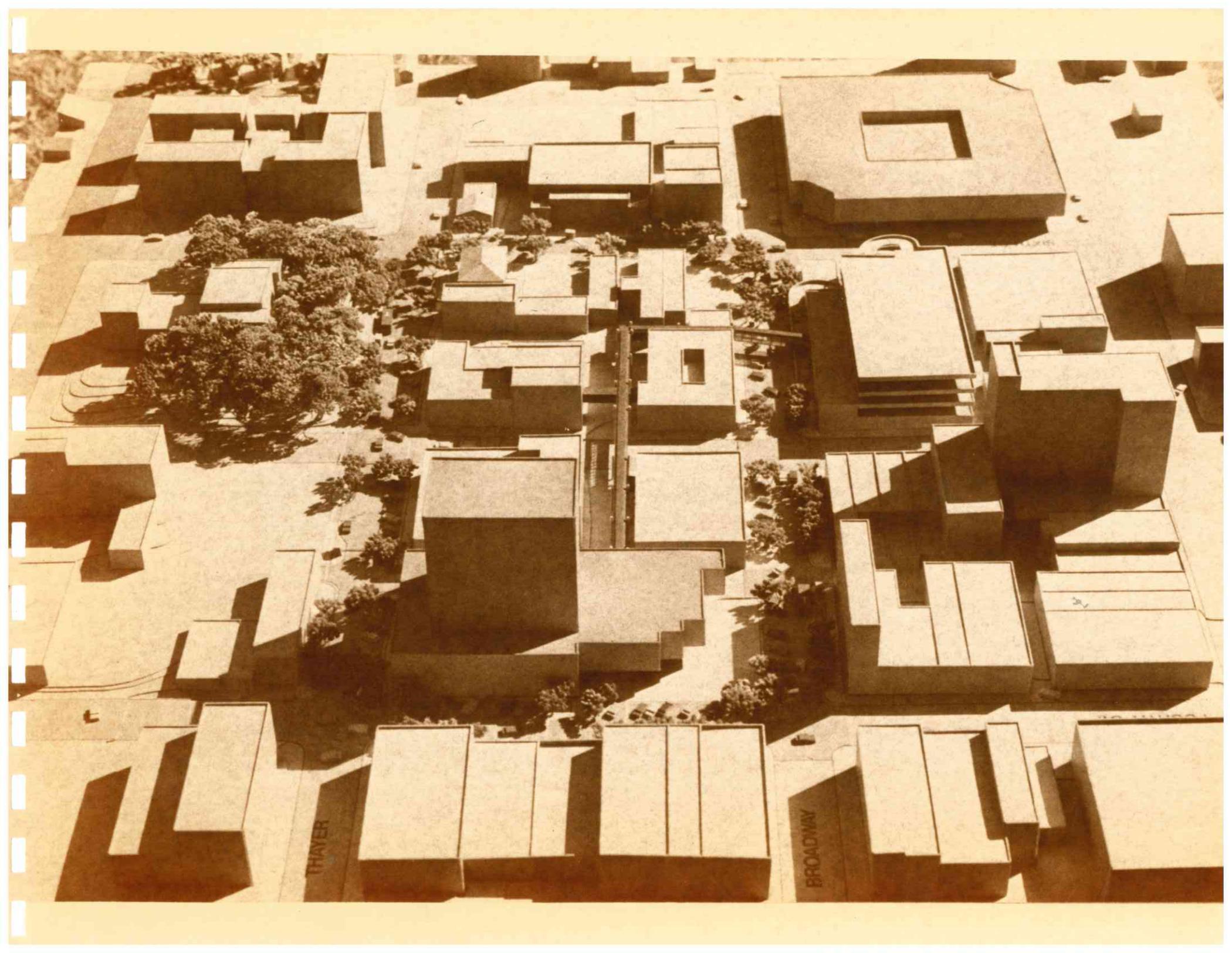
THE Hodne/Stageberg PARTNERS, Inc.


Thomas H. Hodne, Jr., AIA, AIP


Jon E. Gravender, AIA

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INTRODUCTION

This report represents the completion of Phase II: Preliminary Design, in a continuing planning and design process for the development of "Bismarck Center." The project refers directly to the construction of public improvements within a three and one-half square block area of downtown Bismarck, as defined and sponsored by the Bismarck Urban Renewal Agency and the federal Department of Housing and Urban Development. Phase I work conducted a Reconnaissance/Feasibility Study and concluded with the recommendation that before specific physical improvements be made, a more comprehensive, long-range concept plan should be developed to establish planning goals and objectives, and assure that such incremental improvements contribute to a rational whole.

The first portion of Phase II dealt with the generation of such a Concept Plan, which was presented to the Urban Renewal Agency and approved in principle by the City Commission. The concept plan went beyond the project boundary and "downtown" limits to include the new civic arena and shopping center areas south of the railroad tracks, or generally described within Third and Seventh Streets from Rosser to Bismarck Avenue. Important elements of that concept included suggestion of a new commercial "spine" linking the shopping center with downtown both economically and physically; a concentrated transit service loop; a central park surrounding the railroad depot; a "mini-mall" street system; and an enclosed pedestrian "skywalk" system.

With the Concept Plan as the reference base, this Preliminary Design is intended to illustrate a plan for initiating as much of the above elements as appropriate and as allowed by the present project definition and budget. As the name implies, it is a preliminary plan - requiring further development and detail, and subject to modification as the design process continues.

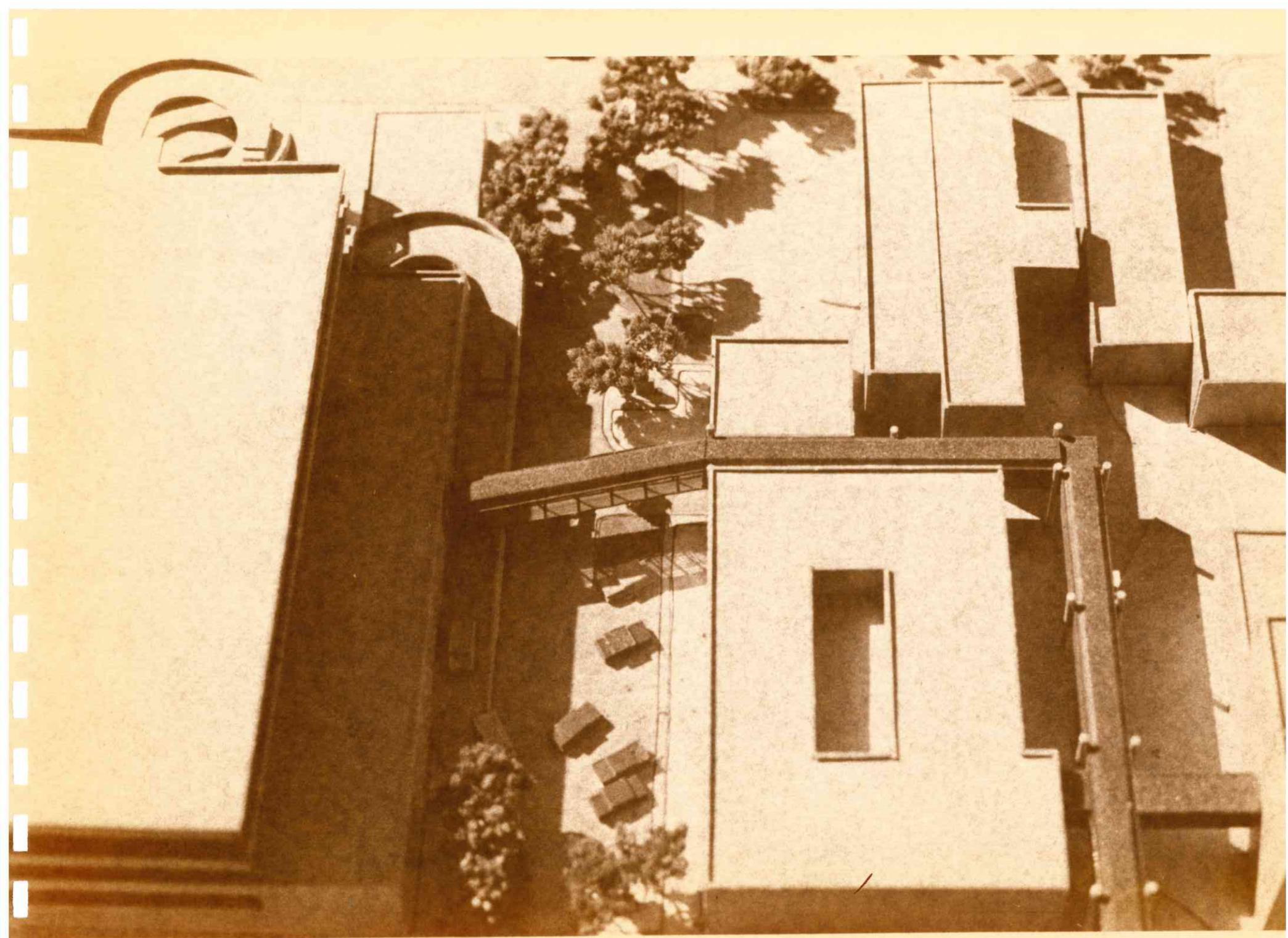
DESIGN SUMMARY

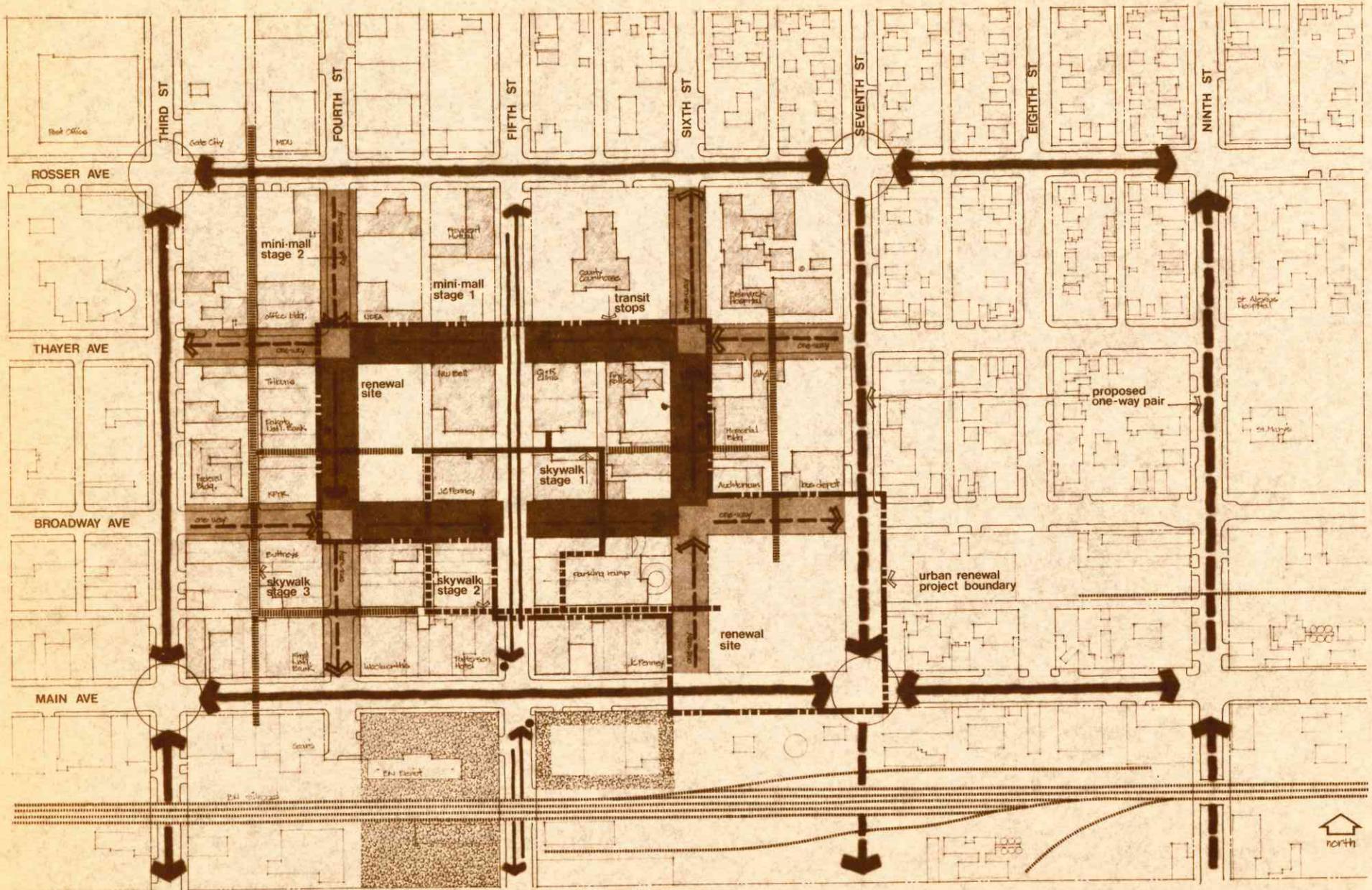
The preliminary design plan includes two separate but closely inter-related elements; a "mini-mall" system and a "skywalk" system, which offer circulation options to downtown users, while serving to unify and identify the downtown area.

The "mini-mall" system converts six blocks of existing streets into a core loop of one-way traffic, angled parking, landscaped malls which follows the proposed transit route, and serves the maximum number of downtown blocks. The plan provides two lanes of one-way traffic (counter-clockwise) and slightly increases the number of on-street parking spaces while introducing significant pedestrian amenities such as trees, plantings, street furniture, and coordinated lighting and signage systems.

The "skywalk" system is designed to offer an enclosed, continuous pedestrian pathway through several downtown blocks initially, as a counter, in part, to the indoor environment appeal of Kirkwood Plaza. The free-standing system follows public right-of-way, (alleys and easements) where possible for two basic reasons: 1) the project definition allows funding only for improvements upon public property, and 2) many existing downtown buildings are not considered to remain over the long term. The skywalk route links the Fourth Street renewal site with at least J. C. Penney's store, Q & R medical clinic, and the Park-ade, with possible extension to the Sixth Street renewal site. Where applicable, the skywalk roof element will enclose the present overhead utility equipment and feature a continuous skylight along mid-block portions for visual relief and interest along the interior pathway.

Together, the "mini-malls" and the "skywalk" offer a bold and unique solution to the special conditions of downtown Bismarck.





BISMARCK CENTER
 URBAN RENOVATION PROJECT: ND/R-5
 BISMARCK URBAN RENOVATION AGENCY

SEPTEMBER, 1973

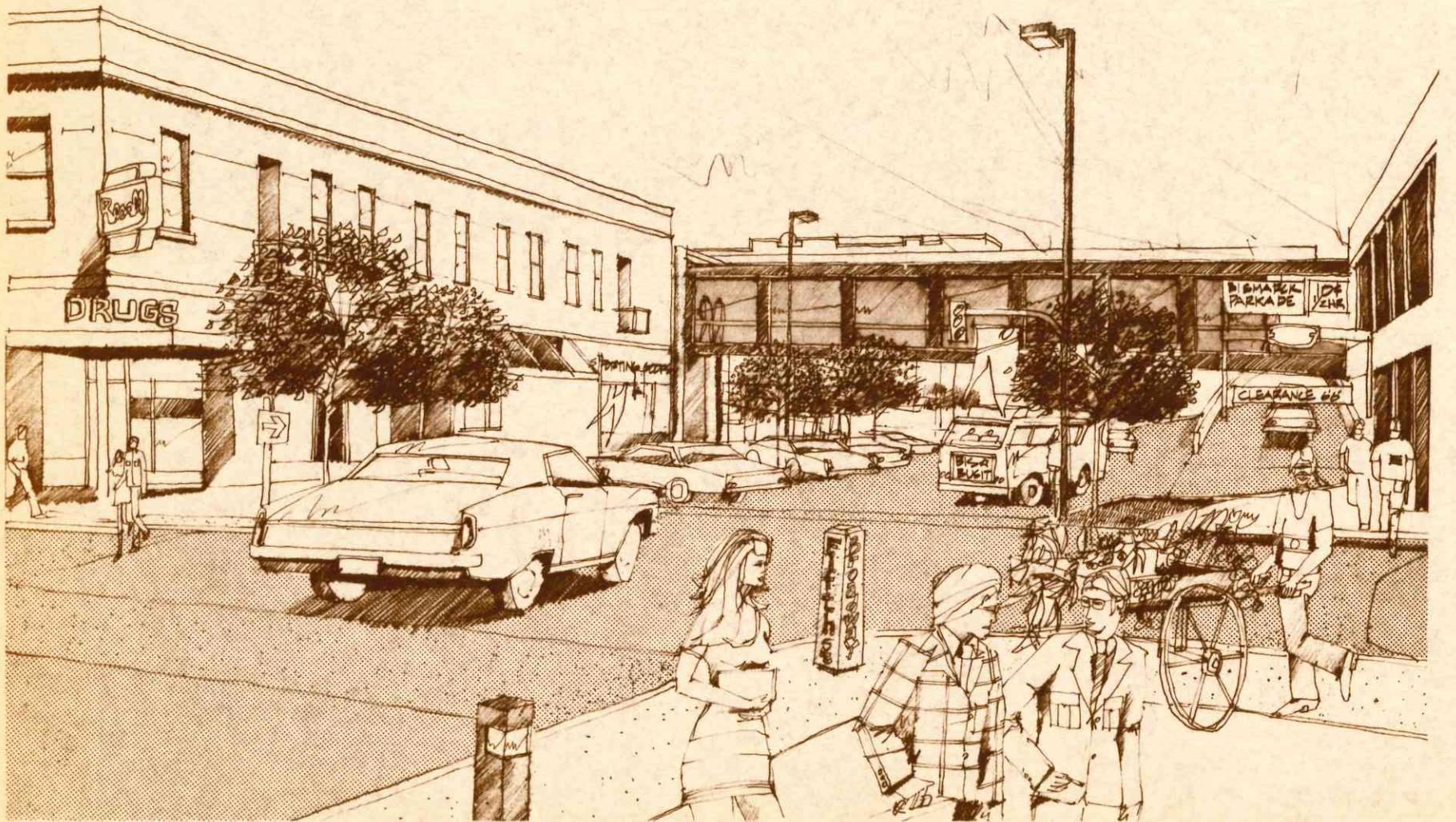
DOWNTOWN PLAN

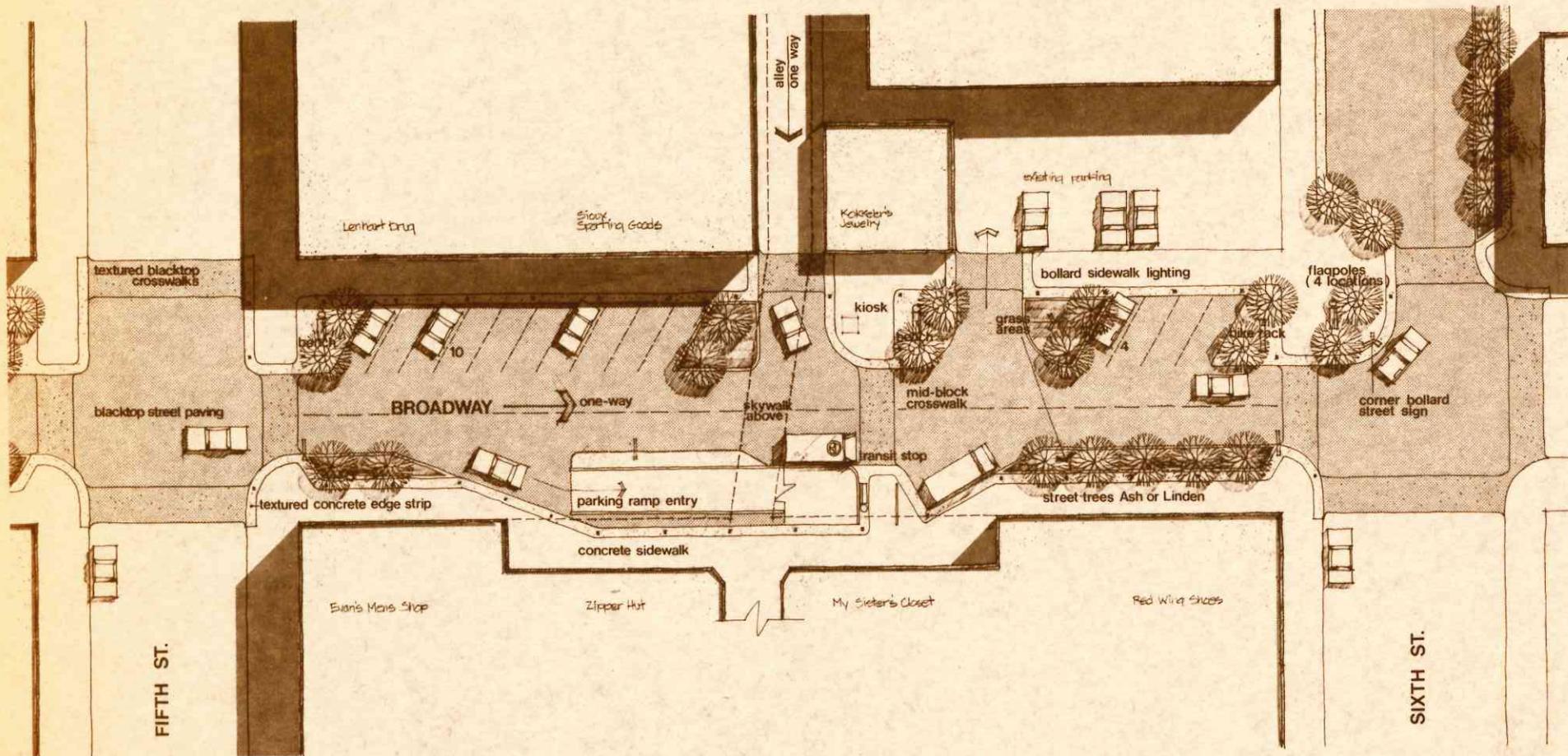
SCHEMATIC PLAN / STAGING DIAGRAM

MINI-MALL SYSTEM DESIGN CRITERIA

The "mini-mall" system should:

- Enhance the ground-level outdoor environment and provide a downtown identity.
- Improve the vehicular traffic flow.
- Encourage pedestrian use and circulation.
- Provide on-street parking capacity equal to or greater than existing.
- Accommodate the route and stops for the proposed bus transit system.
- Introduce trees, plantings, furnishings and related public amenities to the street landscape.
- Provide opportunity to coordinate work with needed utility repair or replacement.
- Meet all public requirements of safety, maintenance, and access.
- Relate design and layout to special conditions of each block.
- Function at the interim stage of development as well as being capable of expanding to the completed stage and beyond.





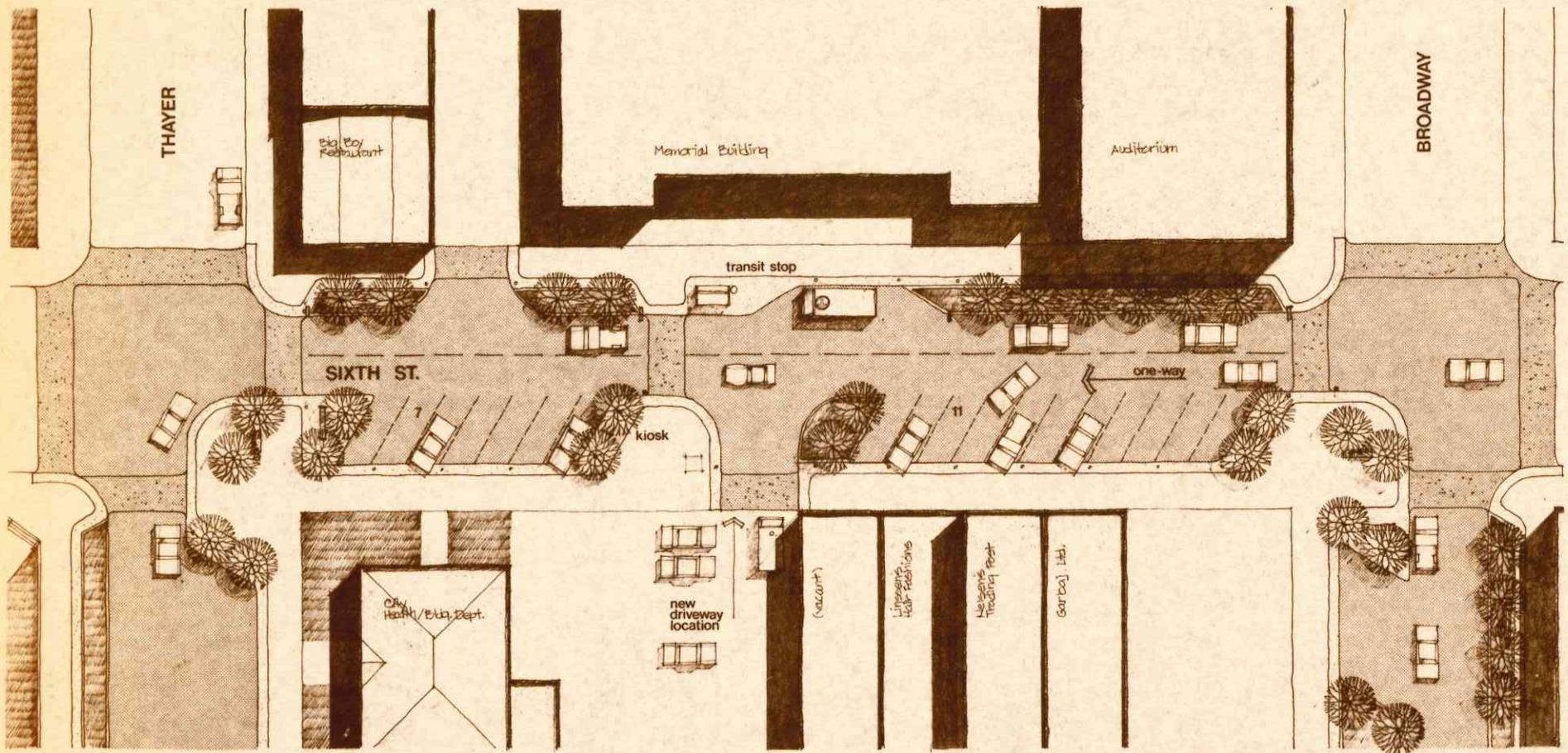
MINI-MALL: BLOCK "A"

BROADWAY AVE. BETWEEN FIFTH & SIXTH ST.



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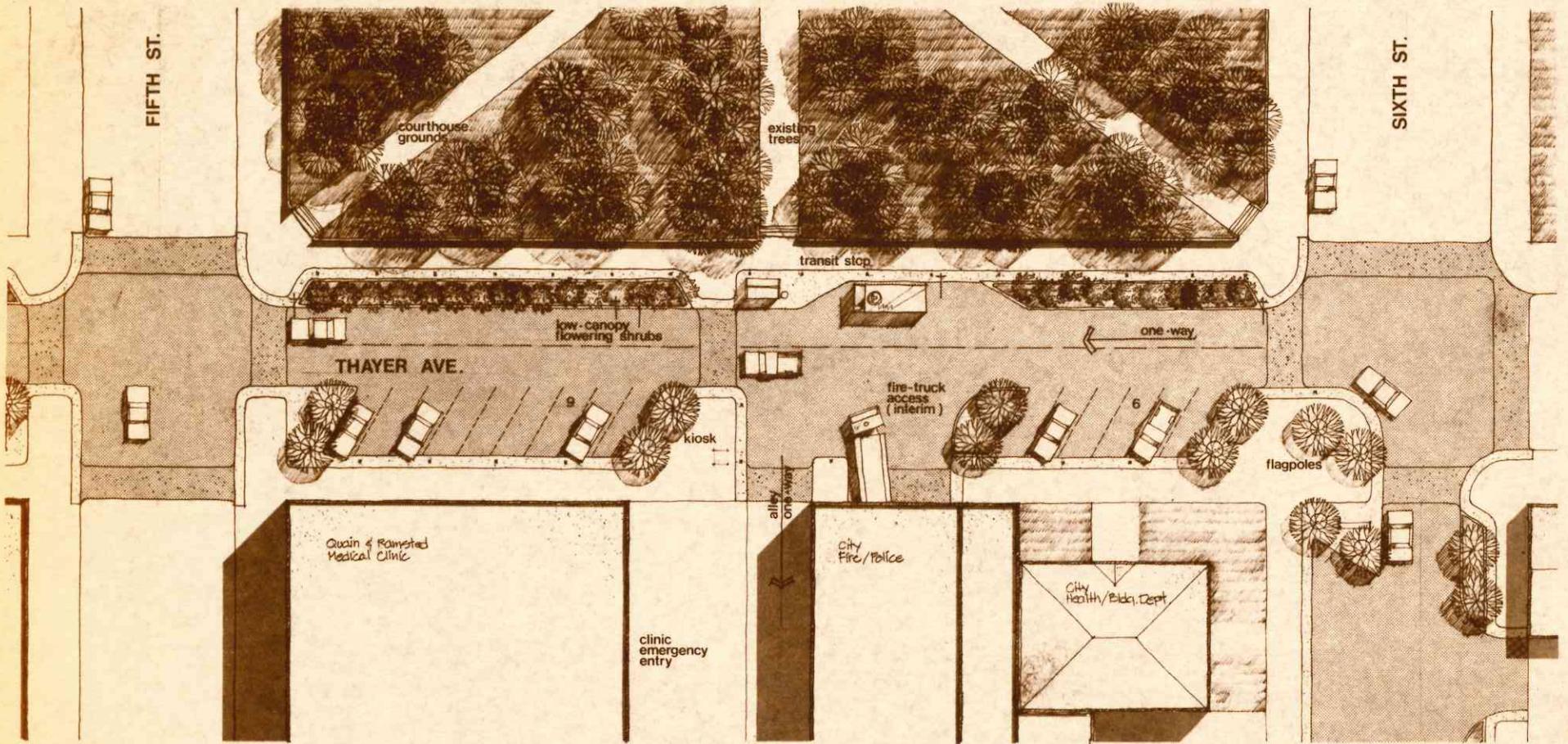


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MINI-MALL: BLOCK "B"

SIXTH STREET BETWEEN BROADWAY & THAYER

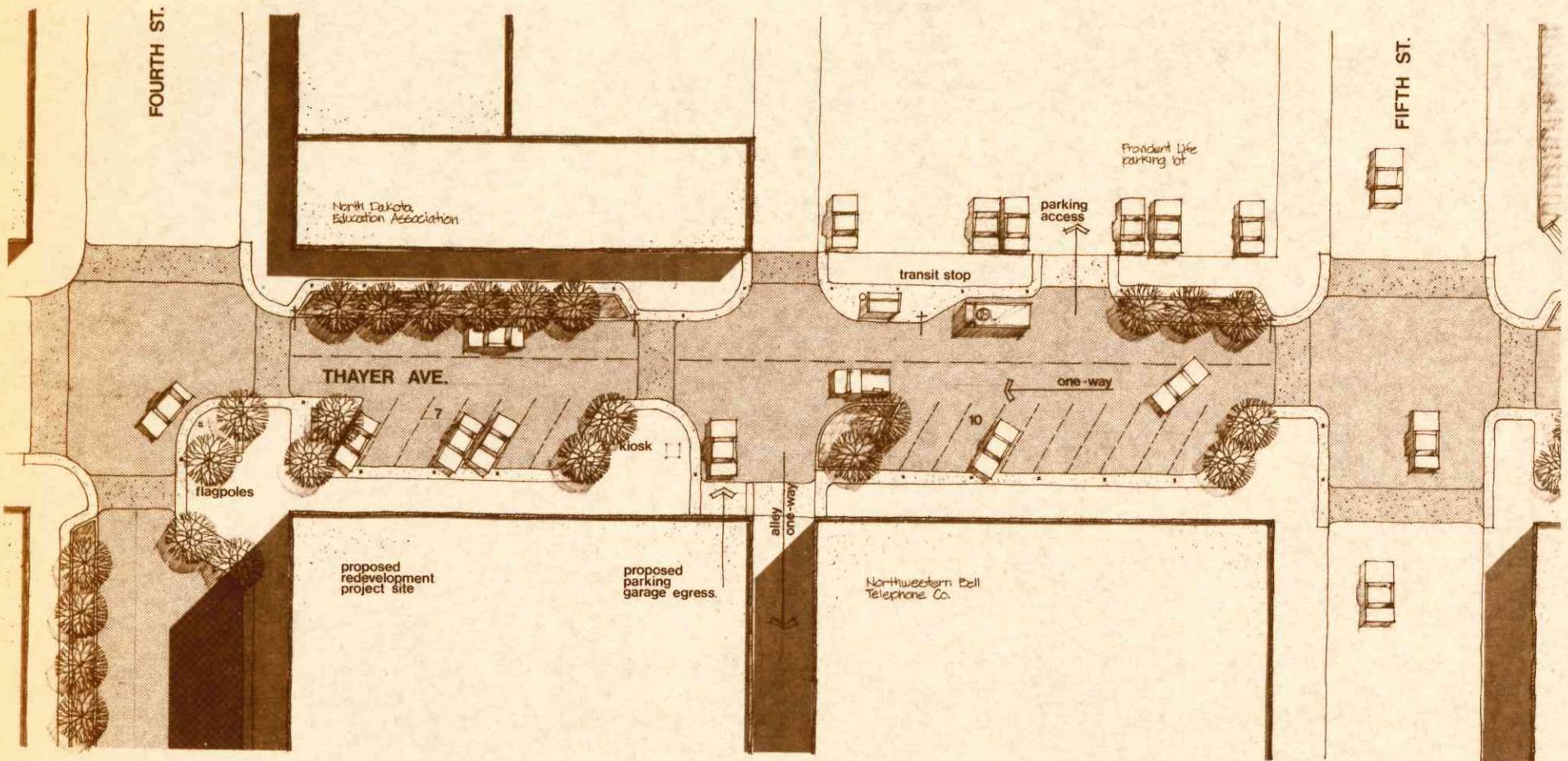


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SEPTEMBER, 1975

MINI-MALL: BLOCK "C"

THAYER AVENUE BETWEEN FIFTH & SIXTH ST.

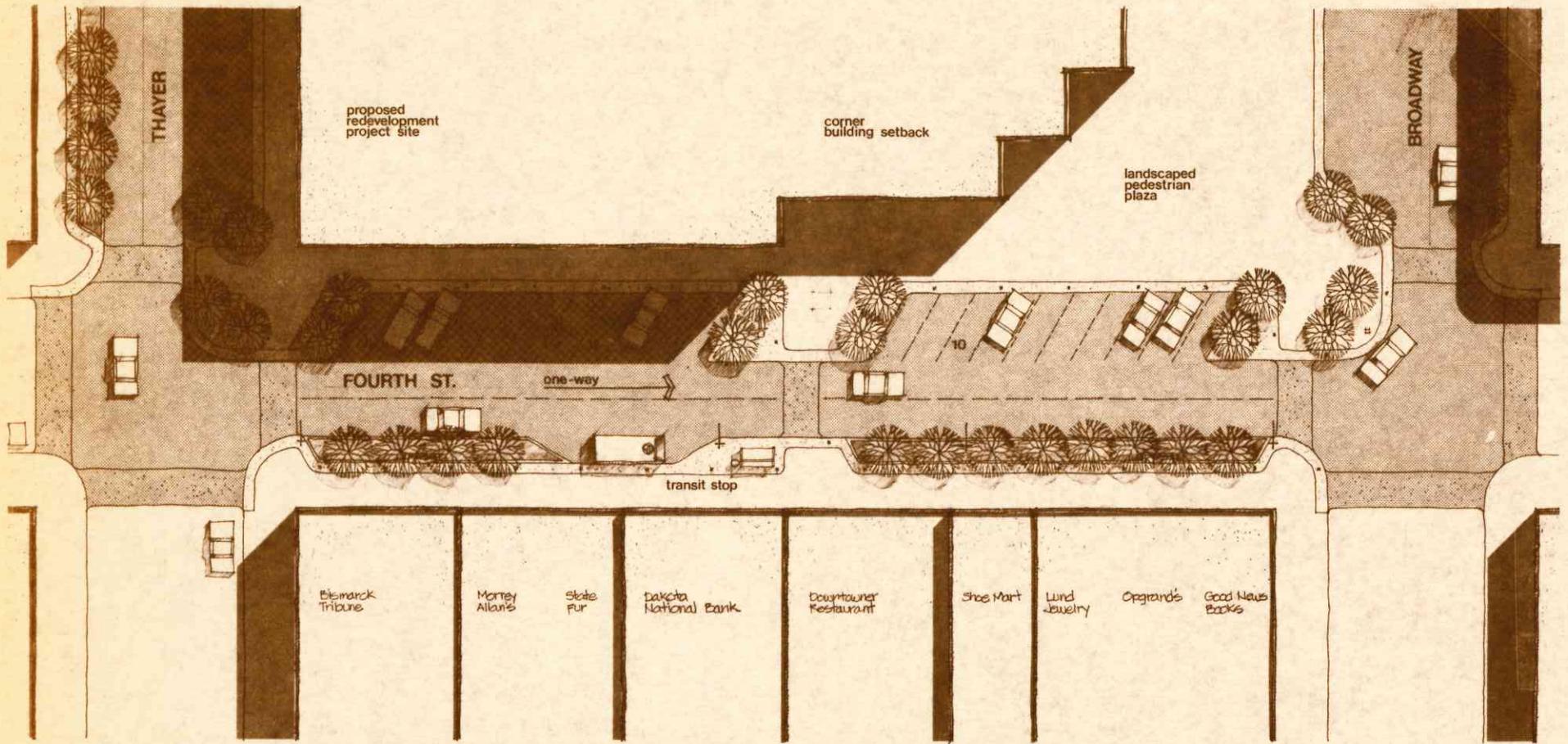


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 BISMARCK URBAN RENEWAL AGENCY

SEPTEMBER, 1978

MINI-MALL: BLOCK "D"

THAYER AVENUE BETWEEN FOURTH & FIFTH ST.

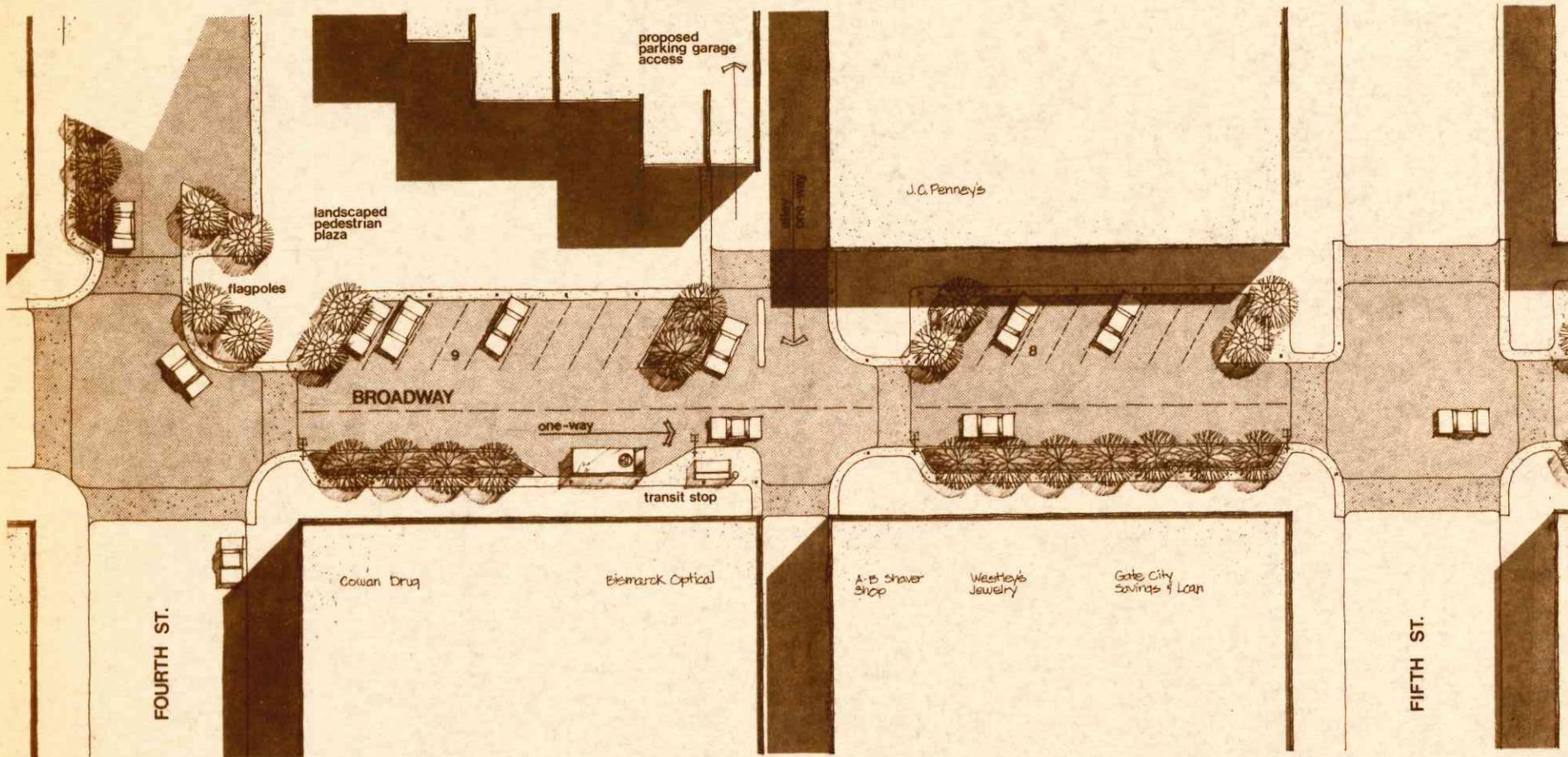


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MINI-MALL: BLOCK "E"

FOURTH STREET BETWEEN THAYER & BROADWAY

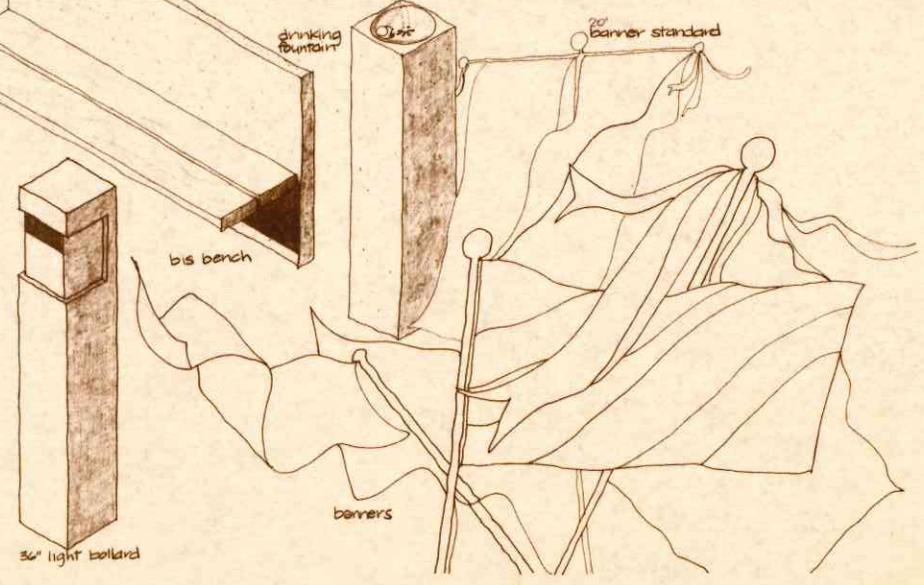
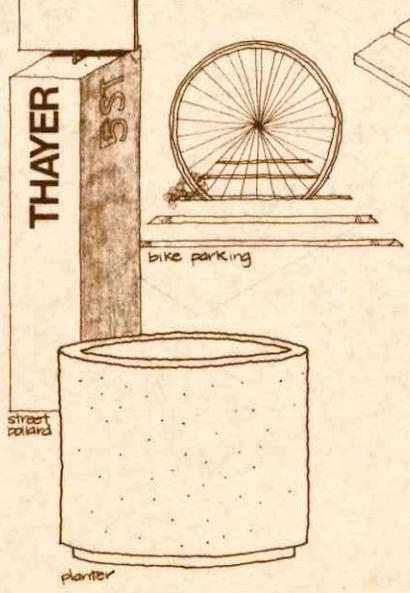
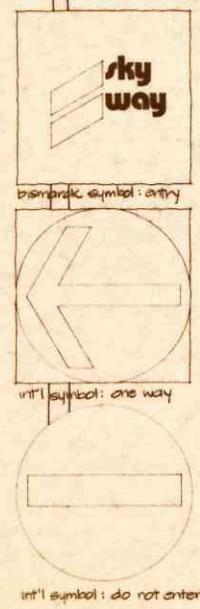
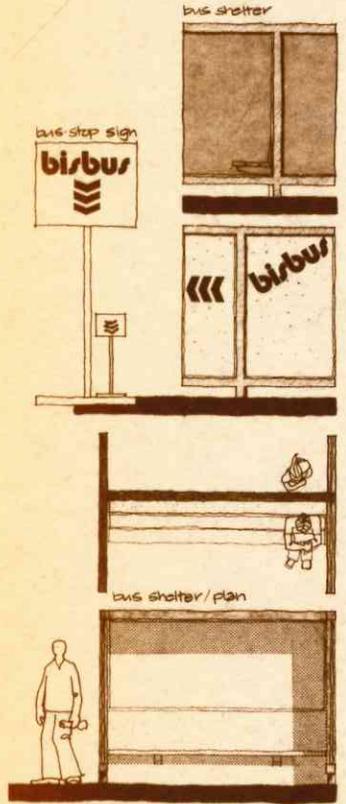
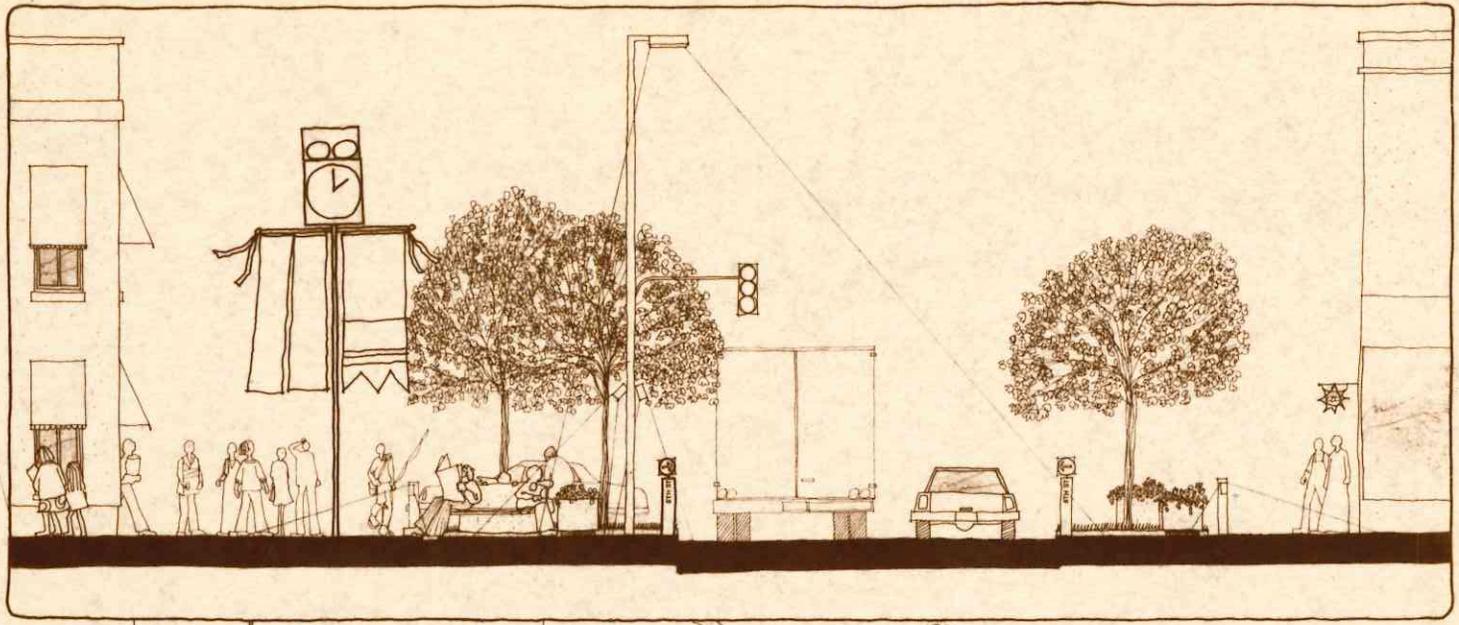


BISMARCK CENTER
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 BISMARCK URBAN RENOVATION AGENCY

SEPTEMBER, 1978

MINI-MALL: BLOCK "F"

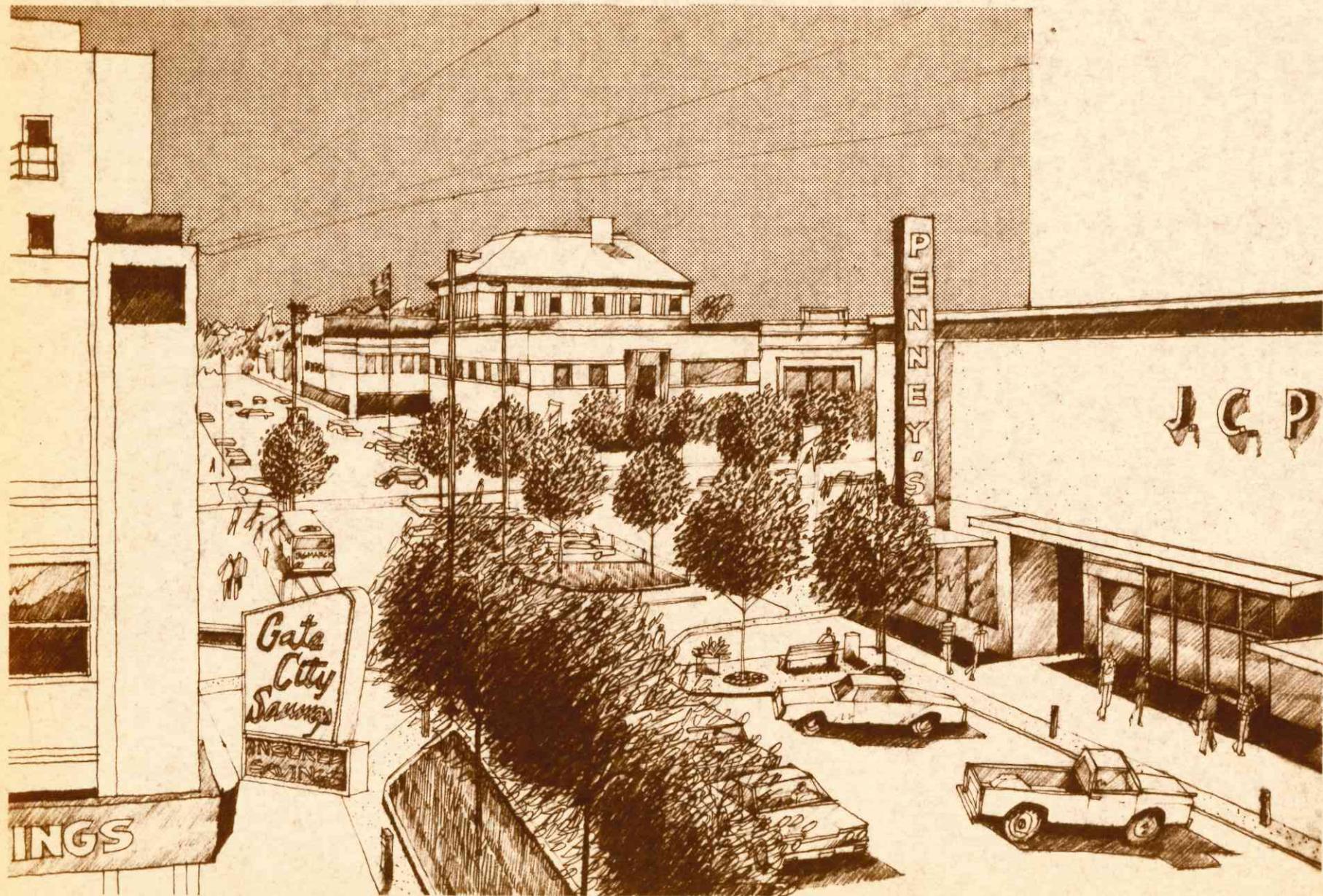
BROADWAY AVE. BETWEEN FOURTH & FIFTH ST.



BISMARCK CENTER
 URBAN RENOVATION PROJECT: ND/RS
 BISMARCK URBAN RENOVATION AGENCY

SEPTEMBER, 1978

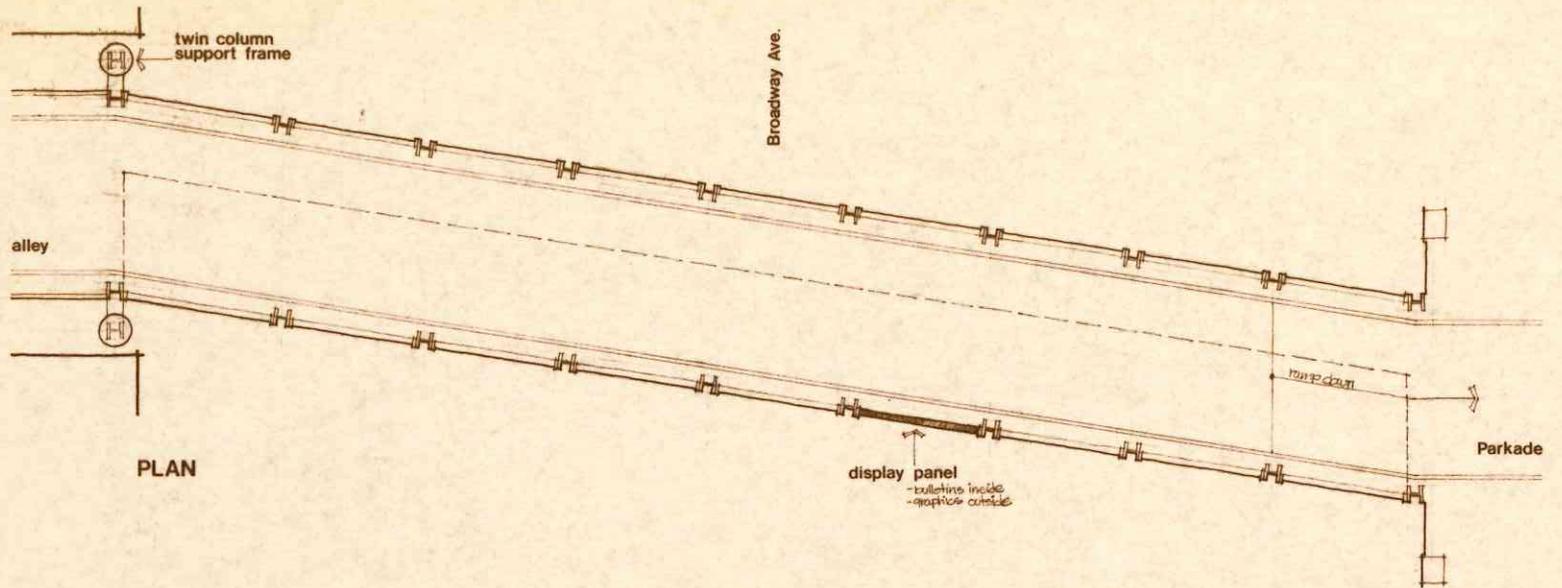
STREET FURNITURE



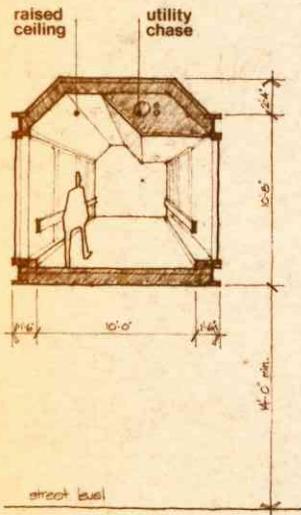
SKYWALK SYSTEM DESIGN CRITERIA

The "skywalk" system should:

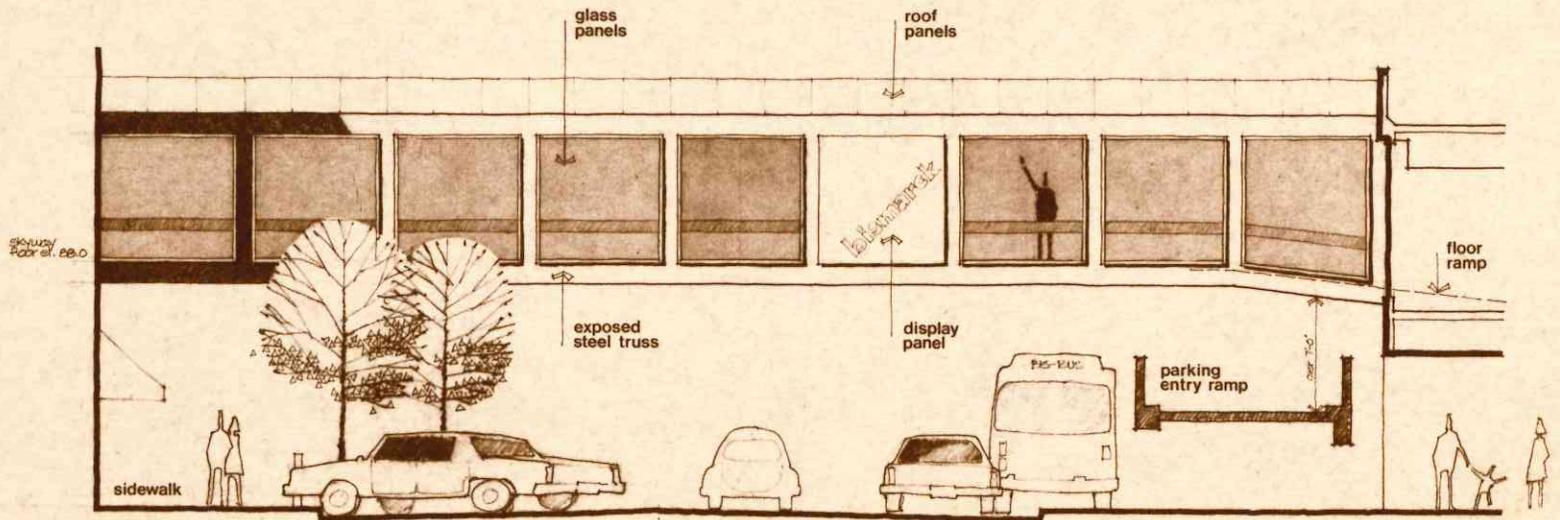
- Provide a new second-level indoor pathway option connecting downtown blocks without crossing traffic or going outside.
- Connect the two urban renewal development sites, with intermediate connections to destinations between.
- Follow public right-of-way (alley or easement) wherever possible.
- Be structurally independent and freestanding of adjacent structures where existing conditions may change.
- Provide a flexible, adjustable system capable of accommodating changes in elevation, direction, and exposure; and allowing future extension as demand and funding allow.
- Relate directly to points of convenient vertical access (elevators and escalators).
- Utilize an efficient, economical structural system minimizing ground-level interference.
- Offer opportunity to replace and incorporate overhead electrical service along alleys.
- Meet all public requirements of codes, vertical clearance, fire access, etc.
- Reflect its primary function in design, and set the standard for design quality downtown.



PLAN



SECTION



ELEVATION

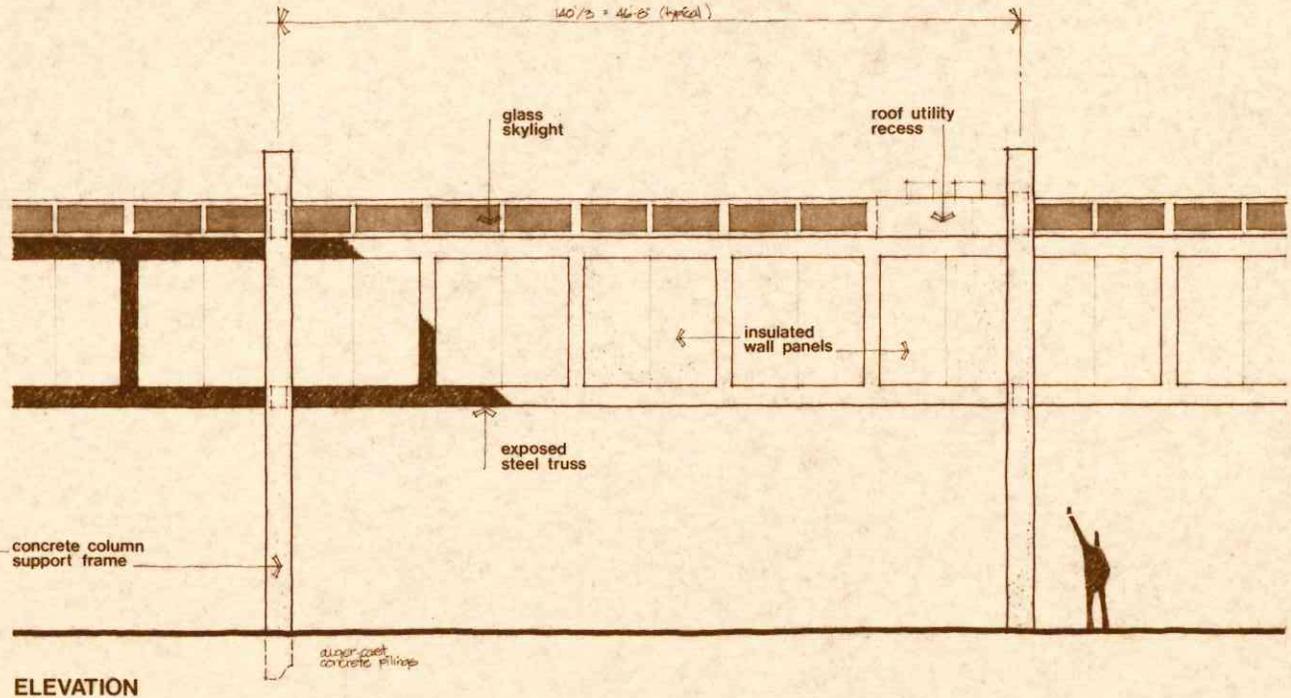
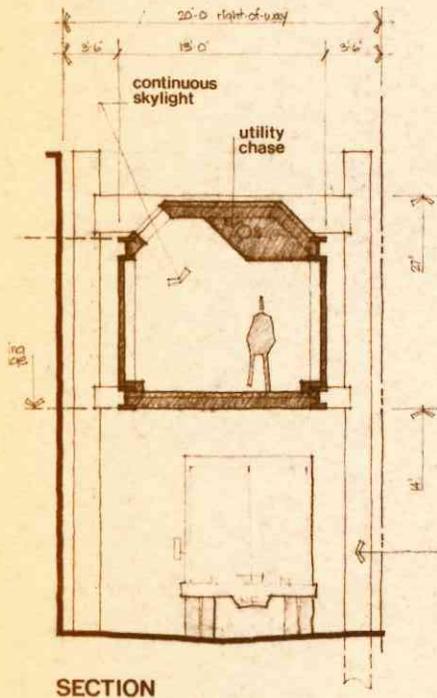


BISMARCK CENTER
URBAN RENAISSANCE PROJECT: N.D./R.S
BISMARCK URBAN RENAISSANCE AGENCY

SEPTEMBER, 1973

SKYWALK SYSTEM

TYPICAL STREET BRIDGE CONDITION

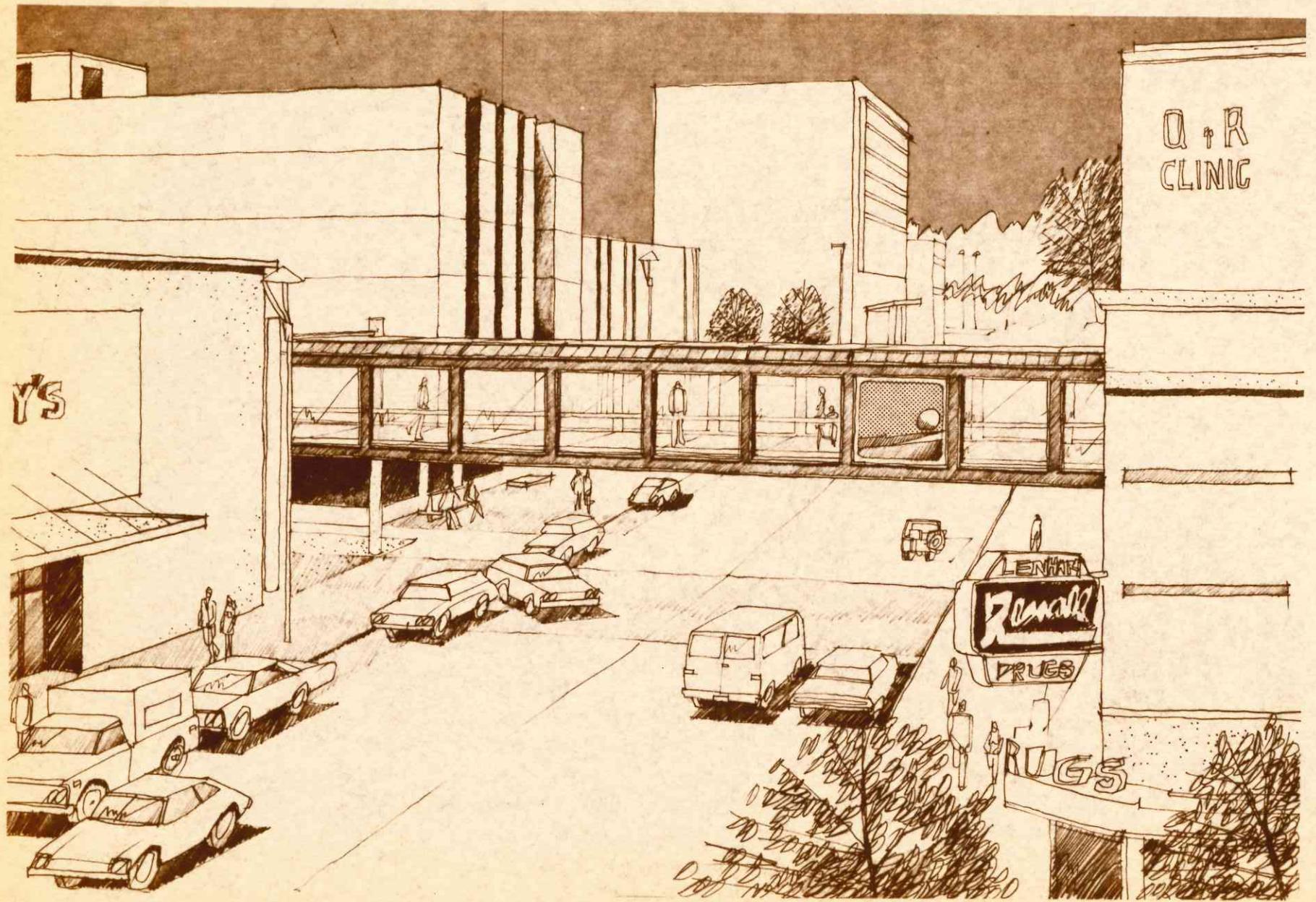


BIEMARCK CENTER
 URBAN RENEWAL PROJECT: N.D./R-5
 BIEMARCK URBAN RENEWAL AGENCY

SEPTEMBER, 1973

SKYWALK SYSTEM

TYPICAL ALLEY CONDITION



MATERIAL OUTLINE

MINI-MALL SYSTEM:

- Demolition: Plan includes removal of all existing sidewalks, curbs, gutters, street paving and street fixtures.
- Paving: Traffic and parking areas to be standard bituminous paving, with textured crosswalks; sidewalks to be standard finish concrete with textured finish edge strips and accent areas.
- Landscaping: Primary street trees to be 4"-6" caliper Green Ash or American Linden, with accent plantings of Flowering Crab. Grass areas indicated to be from cultured sod over 18" topsoil. Flush metal tree-well grates at sidewalk locations.
- Lighting: 30' high square steel street light standards, four per block; 42" high square bollard sidewalk lighting spaced at approximate 20' intervals.
- Furnishings: Design to include benches, kiosks, bicycle racks, trash receptacles, signposts, bus shelters (alternate) and street signage system.

SKYWALK SYSTEM:

- Structure: "Verendeel" truss system with exposed steel framing (painted dark), suspended from twin column support frames spaced approximately 50' apart along and within 20' wide alley or easements. Clear span of street crossings.
- Walls/Roof: Insulated metal or fiberglass panels. Tinted plate glass panels at street bridges; plexiglass skylight units continuous along mid-block locations.
- Interior: Ceiling to be suspended acoustic plaster; floors carpeted throughout.
- Mechanical: Continuous baseboard radiation heating; ceiling ducts for air-conditioning and ventilation; ceiling chase for electrical service.

PRELIMINARY COST ESTIMATE OUTLINE SUMMARY :

MINI-MALL SYSTEM :

● 152,700 sq. ft. @ \$3.45/s.f. =	\$526,800
● 20% contingency for inflation, unknow conditions: =	105,400
	<hr/>
(or \$105,400 per block)	\$ 632,200

SKYWALK SYSTEM :

● Skybridges: 160' @ \$1,000/l.f. =	160,000
● Skywalks: 420' @ \$650/l.f. =	273,000
	<hr/>
	\$433,000
● 20% contingency =	86,600
	<hr/>
	\$ 519,600

TOTAL SYSTEM · \$ 1,151,800



ACKNOWLEDGMENTS

The individuals and groups below provided special assistance during the course of this preliminary design:

Bismarck Urban Renewal Agency

John R. Sakariassen, Chairman
Myron H. Atkinson
Mrs. Ronald Haas
William O. Shirley
R. L. Jacobsen

E. N. Sandwick
Executive Director
W. C. Wocken
Administrative Assistant

City of Bismarck

E. V. Lahr, Mayor
Tom Baker, City Auditor
Edward J. Booth, City Engineer
Will Walton, City/County Planner

The many individual business, public, and community representatives who offered advice and comment during personal interviews.

Special Consultants

Egil Wefald & Associates, Inc.
Minneapolis, Minnesota
(traffic engineering)

Meyer, Borgman & Johnson, Inc.
Minneapolis, Minnesota
(structural engineering)

Swenson & Hagen
Bismarck, North Dakota
(civil engineering)

Herb Baldwin Associates
Jordan, Minnesota
(landscape architecture)

"MINI-MALL" DESIGN STANDARDS SUPPLEMENT
CITY OF BISMARCK, NORTH DAKOTA

A. TRAFFIC / PARKING

1. Mall streets shall be limited to two lanes of one-way traffic; each lane 12' minimum width.
2. Vehicles over 5 tons gross weight shall be prohibited, except for emergency access or local service on approval.
3. Angled parking (60 degrees) shall be provided by special bays (20' perpendicular depth) off the left-hand lane. The total number of spaces shall average at least 16 per block.
4. A separate bus stop or drop-off bay shall be provided off the right-hand lane near the middle of each block.
5. Access shall be maintained to all existing alleys and driveways along the mall, public or private, unless removed or relocated with the specific approval of the property owner.
6. Traffic signals and sign controls shall be provided at each intersection with the review and approval of the North Dakota State Highway Department.

B. PEDESTRIAN CIRCULATION

1. Sidewalks shall be provided on both sides of the street: typical width 14' each, including a 30" to 36" decorative strip at the curb edge.
2. Sidewalk elevations shall match existing conditions at street property lines and frontage entryways.
3. Crosswalks shall be provided at each intersection connecting all four corners, at a mid-block location on each block, and across all alley or driveway openings. Such crosswalks shall have a 10' minimum width and be defined by a change of material or texture, or by striping, or any combination thereof.

C. PAVING MATERIALS

1. Sidewalks: 4" concrete (refer to CS: Section 501 and 601).
2. Sidewalk edge strips: 4" textured finish or exposed aggregate concrete (refer to CS: Section 501 and 601).

3. Curb and gutter: 6" concrete combined curb and gutter section (refer to CS: Section 603), unless otherwise approved.
4. Street paving: 3" minimum bituminous surface course over prepared base course in accordance with CS: Sections 301-403.
5. Crosswalks paving: similar to above but with integrally colored or textured surface finish approved by City Engineer.

D. LANDSCAPING

1. Primary street trees shall be Green Ash or American Linden (Minimum 4" caliper) averaging at least 16 per block in number.
2. Other special plantings shall be in areas designated by the architect and selected to be compatible with the design and with the local growing conditions.
3. Sodding shall be done in accordance with CS: Section 1203.

E. LIGHTING

(More detailed lighting criteria and standards shall be developed and established as part of the design development stage, and applied to this project and future mall extensions).

1. Street lighting: provide four 1,000 watt mercury vapor lamps on 30' metal standards per block (or average spacing of 100') along the right-hand side of the street, unless otherwise approved.
2. Sidewalk lighting: Provide 100 watt directed mercury vapor lamps within 42" high metal bollards spaced approximately 20' apart along the sidewalk edge strip on both sides of the street.
3. Special or supplemental lighting shall be provided as appropriate and compatible with the above lighting system and as approved by the City.

F. STREET FURNITURE

(More detailed criteria and standards for street furniture shall be developed and established as part of the design development stage, and applied to this project and future mall extensions. Materials shall be coordinated for all elements).

1. Bus / waiting shelters: provide one per block located adjacent to the drop-off bay. Shelter shall provide seating for at least three persons, and protection from wind (partial) and rain.
2. Flagpole clusters: provide one cluster of 2 - 4 poles for flags or banners at each of the four corners of the "mini-mall" loop.
3. Benches: provide at least three permanent benches per block for public pedestrian use, seating at least 3 persons each.
4. Kiosks: provide one informational or poster-type kiosk per block located near the mid-block crosswalk.
5. Trash receptacles: provide at least two per block, with removable containers.
6. Miscellaneous items: signposts, phone booths, bicycle racks, and other similar items shall be designed and located as appropriate and approved by the City.